



# Planning Commission

Tuesday, February 25, 2025

6:30 PM

Elk River City Hall

## Regular Meeting & Work Session Agenda

- Regular meeting in Council Chambers
  - Work Session meeting in Council Chambers immediately following regular meeting
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### 1. CALL MEETING TO ORDER

### 2. CONSIDER AGENDA

### 3. CONSIDER MINUTES

#### 3.1 Draft Minutes - January 28, 2025

### 4. PUBLIC HEARINGS

An opportunity for the public to express their opinions and raise questions pertaining to the agenda item. All comments become part of the official public record. For this reason, all comments must be made at the podium so they can be heard and recorded. Comments may also be provided in writing. There will not be deliberations, discussions, or answers to questions until the hearing is closed. It is important to be courteous and allow each presenter to comment before adding additional testimony.

4.1 Conditional Use Permit Amendment: Outdoor Storage of Semis (tractor and/or trailer), motor vehicles, recreational vehicles, trailers, boats, light and heavy equipment, and vehicle storage, and shipping containers, PID 75-00013-4405, Northstar Trucking & RV Parking LLC

4.2 Conditional Use Permit Amendment: Outdoor Storage, 12777 Meadowvale Rd. NW, Elk River WinLectric

4.3 Preliminary Plat and Ordinance Amendment (PUD): The Villas at Fillmore Third Addition, PID 75-00931-0030, Riverside Development

4.4 Ordinance Amendment (PUD): Signage Requirements and Codification, Elk River Crossing Planned Unit Development, Tractor Supply

### 5. GENERAL BUSINESS

Items in which the information is presented by city staff or consultants, then deliberation and action occur. General Business items are not opportunities to receive or provide public input. However, the presiding officer may, at its sole discretion, solicit public feedback.

5.1 Land Use Amendment, Zone Change, and Ordinance Amendment: Specht Family Farm Residential Development (PUD), 16330 US Highway 10, Capstone Homes

### 6. COUNCIL LIAISON UPDATES

### 7. MOTION TO ADJOURN REGULAR MEETING



**Meeting of the Planning Commission  
Held at the Elk River City Hall  
Tuesday, January 28, 2025**

**Members Present:** Commissioner Dennis Booth, Commissioner James Zahler, Commissioner Dornan Bland, Commissioner Anthony Kaba, Commissioner Eric Johnson, Chair Perry Beise

**Members Absent:** Commissioner Robert Rydberg

**Staff Present:** Senior Planner Chris Leeseberg, and Sr. Administrative Assistant/Recording Secretary Jennifer Green

**1. CALL MEETING TO ORDER**

Pursuant to due call and notice thereof, the meeting was called to order at 6:30 PM.

**2. PLEDGE OF ALLEGIANCE**

The Pledge of Allegiance was recited.

**3. CONSIDER AGENDA**

**Moved by Commissioner Johnson and seconded by Commissioner Booth to approve the agenda. Motion Carried 6-0.**

**4. CONSIDER MINUTES**

**Moved by Commissioner Johnson and seconded by Commissioner Booth to approve the following consent items as presented. Motion carried 6-0.**

4.1 Draft Minutes - December 17, 2024

**5. PUBLIC HEARINGS**

5.1 Zone Change: General Industrial (I-3) to Planned Unit Development (PUD), PID 75-00013-4405, Northstar Trucking & RV Parking LLC

Mr. Leeseberg presented the staff report and indicated the application for a zone change is no longer required to be considered, since the use the applicant is proposing is already allowed in the I-3 zoning district.

5.2 Conditional Use Permit Amendment: Bus, Motor Vehicle, and Implement Storage, Maintenance and Repair, PID 75-00013-4405 - Northstar Trucking & RV Parking LLC

Mr. Leeseberg presented the staff report. He distributed a handout of updated conditions to consider. He requested the Planning Commission provide staff input on a new request by the applicant for a

temporary guard shack. He also requested the commission provide input to staff on businesses that are already operating and not in compliance with city ordinances, and to recommend a timeline to complete all required conditions.

Chair Beise opened the public hearing.

**Nikolya Vladyka**, no address given, stated he felt this project was dragging on too long but did understand there were two cities involved. He stated due to the city of Ramsey changing their mind and their lack of communication, he had not received the requested paperwork. He stated his concern regarding Condition 12, which states no repairs or maintenance on site, as they would now like to build a drive-thru building/repair shop on the property, but they were not sure where to locate the building, and unsure if the building could straddle both properties/cities.

Chair Beise stated the request for tonight's meeting doesn't indicate there will be a building on the Elk River parcel. He stated if they recommend approval tonight, it will not include a building.

Commissioner Johnson asked the applicant if his intent was to construct a building to repair vehicles.

Mr. Vladyka stated that was their intent but unsure where it would be located. He stated there were questions about who gets paid the taxes on the building, which public safety agency would respond to emergencies on the properties, electricity, etc. He stated he wants to meet with the city of Ramsey to either move the building entirely onto the Ramsey parcel or partially on both. He stated he is still waiting for paperwork from the city of Ramsey.

Commissioner Johnson noted Condition 2, which states all vehicles stored on the property need to be licensed and operable, which conflicts with this new intention of having a shop to repair vehicles.

Commissioner Booth asked Mr. Vladyka if the Planning Commission should delay action on their request.

Mr. Vladyka requested to continue this application for two more weeks to figure out what is going on with the city of Ramsey and determine the best location for the proposed building.

Chair Beise stated the Planning Commission would continue the request to the next Planning Commission meeting, which is February 25.

Commissioner Booth asked Mr. Vladyka if they have a building design and plans to submit to the city.

Mr. Vladyka stated they do have preliminary blueprints and where they want the building positioned on the properties.

There being no one else available to speak, Chair Beise closed the public hearing.

Mr. Leeseberg indicated the applicant could waive the 60-Day Rule to allow the timeline to be extended. Mr. Vladyka agreed on the record to waive the 60-Day Rule. Staff will follow up with this request in writing from the applicant.

Mr. Leeseberg stated Condition 2 – unlicensed and inoperable vehicles being allowed on the property - is allowed but was not part of the initial request and staff will have to review this additional request. Mr.

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Leeseberg indicated the building straddling two properties will be a logistical nightmare and access to the building can only come from the Elk River side if the building is located on the Ramsey side.

Commissioner Johnson stated he is comfortable with the conditions as outlined as long as there is no junk stored and they remove the condition of inoperable vehicles if a building is built.

Mr. Leeseberg asked for the Planning Commission's preference regarding Condition 7, driving and parking areas. He stated Elk River's ordinance allows Class 5, but Ramsey requires bituminous pavement. The commission discussed having the more restrictive requirement to maintain consistency and be cooperative with the city of Ramsey's requirements.

Commissioner Booth asked if the commission should discuss the temporary guard shack.

Mr. Leeseberg explained the applicant is requesting a guard shack to aid in operating the entrance gate. The Planning Commission was supportive of a temporary guard shack until a building is constructed.

Commissioner Dornan suggested a sunset date for the guard shack.

**Moved by Commissioner Johnson and seconded by Commissioner Zahler to postpone the Conditional Use Permit Amendment request by Northstar Trucking & RV Parking, LLC, to February 25, 2025.**

**Motion Carried 6-0.**

5.3 Conditional Use Permit Amendment: Outdoor Storage, 12777 Meadowvale Road NW, Elk River WinLectric

Mr. Leeseberg presented the staff report.

Chair Beise opened the public hearing. There was no one present to speak.

**Moved by Commissioner Johnson and seconded by Commissioner Booth to postpone the public hearing to the February 25, 2025, Planning Commission meeting. Motion Carried 6-0.**

5.4 Conditional Use Permit Amendment: Recycling Processing Center, 16501 Highway 10, B&E Recycling Station

Mr. Leeseberg presented the staff report.

Chair Beise opened the public hearing. There being no one to speak, Chair Beise closed the public hearing.

Commissioner Dornan asked questions about the storage trailers and wondered if those would be used temporarily while a structure was built.

Mr. Leeseberg stated that at the staff level, there are no concerns with the storage containers as they are allowed by ordinance, but he was not sure of their intended use.

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Commissioner Johnson thought he understood that whatever was being recycled was being held in the storage containers until it was processed and recycled.

**Adam Eastman**, the applicant, at 16501 Highway 10 – stated the storage containers will hold items until they are recycled. A 3-sided building might be utilized for boats, etc. as Sherburne County doesn't want things kept on the ground, so it would be to prevent fluids from soaking into the ground. He stated they are close to outgrowing the building but couldn't answer if it was a benefit to using a connex as cold storage to keep costs down or to avoid the cost of constructing a new building.

**Moved by Commissioner Johnson and seconded by Commissioner Booth to recommend approval of the Conditional Use Permit amending the previous approvals for a Recycling Processing Center (CU 95-02) for B&E Recycling Station with the following conditions to satisfy the standards set forth in Section 30-654:**

1. **The applicant must apply for and receive all county required permits and licenses prior to expanding the requested scope of operations.**
2. **The area identified as a holding pond must be repaired as the site has been re-graded as a parking area. The repair work must be designed by a licensed professional.**
3. **There shall be no storage of recyclable materials or waste outside of the building or fenced in area.**
4. **All non-recyclable materials must be removed from the site by May 1, 2025.**
5. **All vehicles or materials stored on site, and not approved as part of CU 03-26, must be removed by May 1, 2025.**
6. **The disassembling or salvaging of parts from vehicles or machinery is prohibited.**
7. **Any new structures must be designed by a licensed architect and permitted through the building department.**
8. **Business owner must submit an annual waste activity report to the MPCA by March 1<sup>st</sup> of every year.**
9. **The applicant shall submit an updated drainage plan, prepared by a qualified professional, showing that the facility is designed to prevent surface water drainage through recyclable materials, to control dispersion of the recyclable materials and residuals by wind, to contain any spills or releases that could harm human health or the environment, to provide for the storage of recyclable materials so as to protect the recyclability of the materials, and to provide for the storage and removal of residuals.**
10. **Storage of all waste and recyclable materials must comply with all county, state, and federal regulations.**
11. **All recyclable materials must be removed from the facility within three years after the date of receipt.**
12. **The owner of the facility must inspect the site at least every 30-days for malfunctions, deterioration, or discharges that may result in the release of pollutants to the environment. Records of these inspections may be requested by city or county staff.**
13. **The owner or operator must prepare and maintain a contingency action plan addressing actions for a potential fire, spill, or release.**

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**Motion Carried 6-0.**

5.5 Land Use Amendment, Zone Change, and Ordinance Amendment: Specht Family Farm Residential Development (PUD), 16330 US Highway 10, Capstone Homes

Mr. Leeseberg presented the staff report.

Commissioner Booth asked if the city ever authorized 29' wide streets in any prior plats.

Mr. Leeseberg stated the city has allowed narrower streets, but those streets are privately maintained.

Commissioner Johnson asked if this Capstone project would be association maintained.

Mr. Leeseberg stated it will not be association maintained and noted the city engineer stated city streets need to meet city standards of 34'.

**Matt Barker** and **Stephen Bona** of Capstone Homes, along with **Brian Krystofiak** from Carlson McCain, provided a presentation outlining the proposed development. Mr. Barker discussed a traffic study that they completed, the Mississippi River Trail, creating a buffer between the development and Kelley Farm, and the berm at the entrance of the development. He also discussed the proposed 29' street width and explained how they felt it works in other cities such as Otsego and Ramsey. He pointed out the curving streets and boulevard trees can have traffic-calming benefits.

Chair Beise opened the public hearing.

**David Kelliher**, Vice President of the Minnesota Historical Society, spoke and introduced **Alyssa Olson**, Site Manager of the Oliver H Kelley Farm, and **Ben Leonard**, Senior Vice President of Historic Sites Network and Operations for the Minnesota Historical Society, and stated they met with Capstone reps to learn more about the project and to share the importance of the farm with them. Mr. Kelliher posed the following questions and concerns:

- 1) They appreciated the efforts by the city and Capstone to improve access to Highway 10, noting how dangerous it was taking a left turn into and out of the farm. They were excited about a future interchange and asked if there was anything they could do to join that coalition to let them know.
- 2) They are interested in knowing more about the trail and what it looks like.
- 3) Regarding the boundary between the farm and Capstone they look forward to vegetative screening on both wooded and non-wooded.
- 4) They asked for visual simulations to see what this development might look like from all corners of the property.

Mr. Kelliher stated they are looking forward to collaborating on the EAW and appreciate the opportunity to partner with the city and Capstone and also make sure their needs are met.

**Nathan Krakau**, Otsego, stated he has friends that live in Capstone homes and feels they are good builders. He asked about having 9 or 10 overflow parking lots scattered in the development to prevent cars parking in the street, and if that would allow emergency trucks and ambulances to travel along the roadway to get to residents.

There being no one else to speak, Chair Beise closed the public hearing.

Commissioner Booth asked if there were any other development projects where the developer has

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built 29' streets.

Mr. Krystofiak stated he has seen a fair number of 29' street widths being constructed by Capstone and others, and that is the standard for the city of Blaine. He stated it slows traffic down and lessens impervious surfaces. He stated a number of communities are going to this standard.

Chair Beise commented that it seems fairly tight to get emergency vehicles down the road with cars parked on both sides.

Commissioner Booth commented, especially in a cul-de-sac. He stated he had concerns about multiple emergency vehicles in a cul-de-sac.

Commissioner Johnson asked if, in other private developments with 29' street widths, has the city restricted parking on one side?

Mr. Leeseberg indicated the city has. Quite often they were in townhome developments with narrower roads, privately maintained and not built to city standards. He reiterated the city engineer's recommendation to remain with the current street width standard of 34', for various reasons such as vehicle and public safety traffic, snow removal, etc.

Commissioner Johnson would like to know the city engineer's perspective in supporting 34' versus 29'. He feels it's cluttered and does suggest parking is restricted to one side of the street.

Mr. Barker stated Capstone was fine for allowing additional time to review these applications to allow the Planning Commission to continue the street width discussion. He stated the city of Otsego has a 29-foot standard and they can show them how that works with EMS and snow removal. He stated Blaine also has a 29' standard and Ramsey just approved a development with 29' standard road width and almost 550 homes in the Riverstone communities.

Commissioner Johnson asked which developments have 29' roads with narrower lot sizes and Liberty (50' wide lots) homes.

Mr. Barker stated the Harvest Run community in Otsego is probably the closest to how this proposal feels.

Commissioner Dornan asked which development would be a fair replication with trees in the boulevard.

Mr. Barker stated the boulevard trees in Ramsey's Riverstone development would be a fair replication and offered to meet on site if commissioners would so choose.

Commissioner Booth had concerns with cul-de-sac standards.

Mr. Krystofiak stated the cul-de-sac would be sized to city standards.

Commissioner Dornan asked questions about density in the Liberty area and concerns with parking. He stated the narrower driveways seems counter-intuitive and by taking away parking on the home's driveway forces more street parking.

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Mr. Barker stated the Liberty product parking area width of 16' is their standard. He stated this wasn't a reduction in parking in the driveway.

Commissioner Dornan felt the smaller road and driveway width could create more street parking concerns.

Mr. Barker stated the Liberty design provided for 1500-2800 sq. ft. homes with 3-4 bedrooms.

Mr. Leeseberg explained parking stall standards in public parking lots in Elk River are 9 feet, to give an idea for driveway parking widths.

Commissioner Zahler stated he likes the idea of the Liberty homes due to the housing market being tight especially on the low end of pricing, offering another housing stock, so maybe putting up with smaller streets makes for a trade-off.

Chair Beise and Commissioner Johnson stated they like the proposed development design and layout, but do have concerns with the 29' road width.

Commissioner Johnson stated the city has approved narrower roads only when they are private/association maintained. He wondered if by approving these, will those developments come back and ask the city to take over maintenance. He stated he is in favor of coming up with creative solutions.

Commissioner Dornan wondered if we would be setting precedence for future developments and the volume of traffic would be higher in the Liberty housing area. He stated they are getting away from the standard without having an explanation and justification for changing the standards.

It was the consensus of the Planning Commission to get a better feel for the lot and road widths by visiting area developments by Capstone. It was the consensus of the Planning Commission to request reasoning directly from the city engineer for supporting city standards of 34' width streets and opposing 29' width streets.

**Steve Bono** - Capstone - explained they have seen over the years that many cities did have the larger street widths and now are slowly changing to a narrower street, and a big part of that is the safety aspect. He feels the wider street creates a bigger impervious surface and faster traffic on a street. He stated for example, the city of Ramsey had the wider streets, and they approved the 29' street width after a lot of discussion about the safety aspects. After a few years, they felt it worked and phase 2 is now being constructed. He stated the city of Otsego has now changed their standard and it works for them also. The cost of a home is also a factor and the Liberty model is as affordable as Capstone can get at \$360,000 - \$440,000 which is still an expensive home.

Mr. Barker stated he would continue discussions with the city engineer. He noted the main roadway arteries are 34' wide and the residential home arteries are 29' wide.

David Kelliher requested a fence between their property and the homes right up against the forested section.

**Marilyn Nathe Specht**, 9180 Collins Drive, Ramsey, shared her concerns about the number of homes being proposed and discussed her experiences and beauty on the farm. She wanted the city and

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the developer to understand the importance of preserving the heritage of the land for its habitat and create a park for all to enjoy.

**Moved by Commissioner Johnson and seconded by Commissioner Booth to postpone action of the request by Capstone Homes for a Land Use Amendment updating the city's Comprehensive Plan to align with the proposed residential and commercial areas until the February 25, 2025, Planning Commission meeting.**

**Motion carried 6-0.**

**Moved by Commissioner Johnson and seconded by Commissioner Kaba to postpone action of the request by Capstone Homes for an ordinance amendment to establish a residential Planned Unit Development until the February 25, 2025, Planning Commission meeting.**

**Motion Carried 6-0.**

**Moved by Commissioner Johnson and seconded by Commissioner Kaba to postpone action of the request by Capstone Homes for a zone change amending the city's zoning map to designate parcels for Highway Commercial and Planned Unit Development (PUD) uses until the February 25, 2025, Planning Commission meeting.**

**Motion Carried 6-0.**

6. GENERAL BUSINESS

There was no general business to discuss.

7. COUNCIL LIAISON UPDATES

Councilmember Jennifer Wagner introduced herself and stated she would share each Planning Commission meeting dialog to allow the City Council to make informed decisions, and stated she appreciated their conversations.

8. MOTION TO ADJOURN REGULAR MEETING

**Peter Specht** stated he is in support of the Capstone project.

**Moved by Commissioner Booth and seconded by Commissioner Kaba to adjourn the meeting. Motion Carried 6-0.**

The meeting adjourned at 8:12 p.m.

Minutes prepared by Jennifer Green.

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Perry Beise, Chair

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Tina Allard, City Clerk



# Request for Action

**To**  
Planning Commission

**Item Number**  
4.1

**Meeting Date**  
February 25, 2025

**Prepared By**  
Chris Leeseberg, Senior Planner

**Item Description**  
Conditional Use Permit Amendment: Outdoor Storage of Semis (tractor and/or trailer), motor vehicles, recreational vehicles, trailers, boats, light and heavy equipment, and vehicle storage, and shipping containers, PID 75-00013-4405, Northstar Trucking & RV Parking LLC

**Reviewed by**  
Zack Carlton

## Action Requested

Recommend, by motion, approval of the Conditional Use Permit with the following conditions to satisfy the standards set forth in Section 30-654:

1. Schedule a site visit with the Building Official and Fire Marshal to review and complete any required code updates.
2. The outdoor storage shall be allowed for only the following vehicles:
  - a. Properly licensed, registered, and operable.
  - b. Semi-trucks, semi-trailers, and other delivery trucks;
  - c. Recreational vehicles, campers, motorhomes, and fifth-wheel trailers;
  - d. Boats;
  - e. Enclosed shipping containers; and
  - f. Cars and trucks.
3. Vehicle Wrecking Yards and Salvage Yards, as defined by the City of Ramsey, and Salvage Yards, as defined by the City of Elk River shall not be permitted.
4. Outdoor storage of any products, equipment, materials, supplies, or debris is not permitted, except for snow removal equipment and supplies for the site in conformance with the applicable municipality's regulations.
5. Motor vehicle, trailer, and boat sales is prohibited.
6. Car dealer overflow parking is prohibited.
7. All driving and parking areas shall be paved with an approved surface (bituminous, Class 5, or concrete).
8. Drive lanes need to maintain a minimum 24-foot width throughout the site.
9. All driving areas and outdoor storage/parking areas shall be bounded by B612 concrete curb and gutter.
10. The B612 concrete curb and gutter shall be installed on the east side of the fence along Jarvis Street and 10 feet from the remaining property lines.
11. Site lighting shall comply with Section 30-937.

The Elk River Vision

*A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity*

12. There shall be no repair or maintenance of any vehicles, trailers, or boats on the site.
13. The idling of a truck tractor or other business equipment in excess of 15 minutes is prohibited from April 16 through October 31, and idling in excess of 30 minutes is prohibited from November 1 through April 15.
14. A guard shack no larger than 25 sq.ft. shall be allowed near the entrance gate. Location to be approved by city staff.
15. In the event a gate is installed, access to the property must include key boxes for both Elk River and Ramsey Fire Departments.
16. A reinforced concrete strip, a minimum one (1) foot wide and centered on the property line, must be maintained along the city/county border in order to clarify any relevant jurisdictional issues.
17. In the event either parcel is owned separately, or one municipality revokes their approval, each parcel will need to conform to each jurisdiction's ordinance and codes individually, including, but not limited to, stormwater ponds, setbacks, screening, curbing, and landscaping.
18. Update submitted plans as needed to demonstrate compliance with state and local stormwater management requirements across both parcels/jurisdictions.
19. Submit documentation showing compliance with all Minnesota Pollution Control standards.
20. An amendment to this Conditional Use Permit will be required for future expansions/additions not shown on the city site plan dated February 25, 2025.
21. Conditional Use Permit, case number CU 23-16, shall become void.
22. Approvals from the city of Ramsey shall be recorded and copies provided.
23. Conditions #1, 7, 9, 10, 11, 16, 18, and 19 shall be completed by .
24. A 6-foot tall, 100% opaque privacy fence shall be installed along the north property lines.

### **Background/Discussion**

The subject parcel was granted a Conditional Use Permit (CUP) in 2023 but has yet to be recorded. The property owner has applied for a new CUP to facilitate a joint operation of their current use on two parcels, one in Elk River and the other in Ramsey. The business has been, and continues to operate, on both parcels and staff have not received any complaints as it relates to their Elk River operation.

Through the CUP, a master plan can be reviewed and approved to accommodate their use while having a uniform set of uses, rules, regulations, and site amenities that work for both municipalities and conforms to each city's Comprehensive Plans.

The property is 3.83 acres in size and is zoned I-3 General Industrial, where bus, motor vehicle, and implement storage require a Conditional Use Permit . NorthStar Truck & RV Parking LLC is seeking the CUP to provide outdoor storage of Semis (tractor and/or trailer), motor vehicles, recreational vehicles, trailers, boats, light and heavy equipment, and vehicle storage, and shipping containers.

The subject parcel is part of a larger parcel divided between two cities with approximately 6.75 acres of additional land in Ramsey.

The site is currently fenced, but no security gate has been installed, and clients have 24-hour access to the property. The owner will be the sole employee working on-site. The applicant asked about the possibility of installing a temporary guard shack, such as a job site trailer, to house hardware in a heated structure for operating a security gate. Staff expressed concerns about the size of such a structure and the potential for it to be used as a sleeping area. Therefore, staff recommends that the guard shack be limited to a maximum size of 25 square feet and be positioned near the entrance gate.

### **Parking**

The applicant stated in their narrative that 80 storage stalls are available, and the surface consists of Class 5 material. The submitted site plan shows how they plan to park vehicles on site. Drive lanes need to maintain a minimum 24-foot width throughout the site.

The closest regulation in the ordinance is self-storage facilities stating one parking stall for each 400 square feet of office space or four stalls, whichever is greater. As there is no office on site, four stalls should be provided.

Section 30-901 states all driving areas and parking areas shall be bounded by B612 concrete curb and gutter, but the city council can grant an exemption where installing concrete curb and gutter presents a hardship due to the uniqueness of the business or other conditions. An exemption was not granted by the council for the first CUP request.

### Fencing

There is existing decorative fencing along the railroad tracks and Jarvis Street. The applicant is also proposing a six (6) foot tall wooden privacy fence along the northern property line on the Ramsey side.

### Landscaping

The number of trees the ordinance requires is 32; 16 overstory, 8 ornamental, and 8 coniferous. This is determined by taking the perimeter of the property (1,300 feet) divided by 40. Staff are only using the property lines facing Elk River, as the use does encroach across the property line into Ramsey. The ordinance does allow for credit of existing trees.

### Lighting

The applicant is proposing site lights, which are shown on the site plan. Portions of Section 30-937: Lighting, state: "All sources of artificial lights shall be so fixed, directed, designed or sized that the minimum subtotal of their illumination will not increase the level of illumination on any nearby residential property by more than 0.1 footcandle in or within 25 feet of a dwelling or by more than 0.5 footcandle on any part of the property. Glare, whether direct or reflected, as different from general illumination, shall not be visible from beyond the limits of the immediate site from which it originates." Site lighting shall comply with Section 30-937.

### **Applicable Regulations**

In approving a Conditional Use Permit, the city council may impose such specific conditions with regard to the development or operation of the proposed use as it considers necessary to satisfy the seven standards set forth in Section 30-654 and the requirements contained in this article and to promote compatibility with and minimize any potentially adverse effects upon adjacent properties, the neighborhood or the city.

The issuance of a Conditional Use Permit can be ordered only if the use at the proposed location:

- I. *Will not endanger, injure or detrimentally affect the use and enjoyment of other property in the immediate vicinity or the public health, safety, morals, comfort, convenience or general welfare of the neighborhood or the city.*

The site currently operates in a similar fashion and will not detrimentally affect the use and enjoyment of the properties in the immediate vicinity. However, the addition of motor vehicles and the associated potentially toxic chemicals will require additional oversight from the MPCA, Sherburne County, and the city.

To ensure any concerns for public health and groundwater contamination are addressed, staff have included conditions requiring compliance with all state and local standards related to stormwater management. Additionally, to minimize visibility from surrounding properties, potentially impacting the use and enjoyment of those parcels, staff recommends the existing fencing along the railroad tracks and Jarvis Street have opaque screening incorporated into it. It is understood that semi-trailers and taller box vehicles will still be visible.

2. *Will be consistent with the comprehensive plan.*

The property is guided as Industrial and the Industrial Use category includes both heavy and light industrial businesses such as manufacturing, warehousing, and excavation. These uses are primarily located off of Highway 169, Highway 10, and the BNSF Railroad which provide easy access for the transferring of goods throughout the region.

Staff do not see a need to impose specific conditions to satisfy this standard.

3. *Will not impede the normal and orderly development and improvement of surrounding vacant property.*

The surrounding area is developed. The use will not impede the normal and orderly development and improvement of surrounding vacant property.

Staff do not see a need to impose specific conditions to satisfy this standard.

4. *Will be served adequately by and will not adversely affect essential public facilities and services including streets, police and fire protection, drainage, refuse disposal, water and sewer systems, parks and schools; and will not, in particular, create traffic congestion or interference with traffic on adjacent and neighboring public thoroughfares.*

The property will be served adequately by and will not adversely affect essential public facilities and services including streets, police and fire protection, drainage, refuse disposal, water and sewer systems, parks and schools; and will not, in particular, create traffic congestion or interference with traffic on adjacent and neighboring public thoroughfares.

While the stormwater holding pond is in Ramsey, staff continues to work with Ramsey to ensure stormwater compliance across both parcels.

5. *Will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons or property because of excessive traffic, noise, smoke, fumes, glare, odors, dust or vibrations.*

If dust can be controlled, the proposed storage use will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons or property because of excessive traffic, noise, smoke, fumes, glare, odors, dust, or vibrations.

To prevent dust becoming a problem, dust control measures shall be utilized in the fenced yard area.

6. *Will not result in the destruction, loss or damage of a natural, scenic or historic feature of major importance.*

The site is an open, relatively flat area, and already used for the outdoor storage of materials. The use will not result in the destruction, loss or damage of a natural, scenic or historic feature of major importance.

Staff do not see a need to impose specific conditions to satisfy this standard.

7. *Will fully comply with all other requirements of this Code, including any applicable requirements and standards for the issuance of a license or permit to establish and operate the proposed use in the city.*

Staff included conditions requiring compliance with state, county, and city ordinances related to salvage yards.

Included conditions address compliance with this standard.

If denial of such a permit should occur, it shall accompany recommendations or determinations by findings or a report stating how the proposed use does not comply with the standards set forth in Section 30-654.

#### Ramsey Planning Commission

The Ramsey Planning Commission reviewed this at their regular meeting on January 23, 2025. To ensure both municipalities are reviewing the same information, please refer to the site exhibits in the attached Ramsey Planning Commission memo dated January 23, 2025.

Elk River staff attended the meeting, where the Ramsey Planning Commission expressed appreciation for the collaborative efforts between both cities in addressing the unique application. During the presentation, Ramsey staff shared an image of damaged vehicles and other materials currently on-site, and the commission concurred that such items should not be permitted. Aside from the applicant, no one spoke during the public hearing, and the Ramsey Planning Commission unanimously recommended approval of the request.

#### Elk River Planning Commission (1-28-2025)

The commission reviewed the proposal and discussed the paving of the site, repair of vehicles, and construction of a building. As the applicant brought the desire for vehicle repair and a building to the commission meeting, the commission did not want to make a recommendation until more concrete information was before them and the applicant requested their item be postponed until February 25, 2025..

#### **Financial Impact**

None

#### **Mission/Policy/Goal**

Appropriately govern in an ever-changing environment.

#### **Attachments**

1. Location Map
2. Site Aerials
3. City Plan 2-25-2025
4. Ramsey Memo dated 1-23-2025



# Project Location Map

Northstar Trucking

Zone Change  
Conditional Use Permit

Case No: ZC 24-05, CU 24-21





75-00013-4400

75-00811-0115

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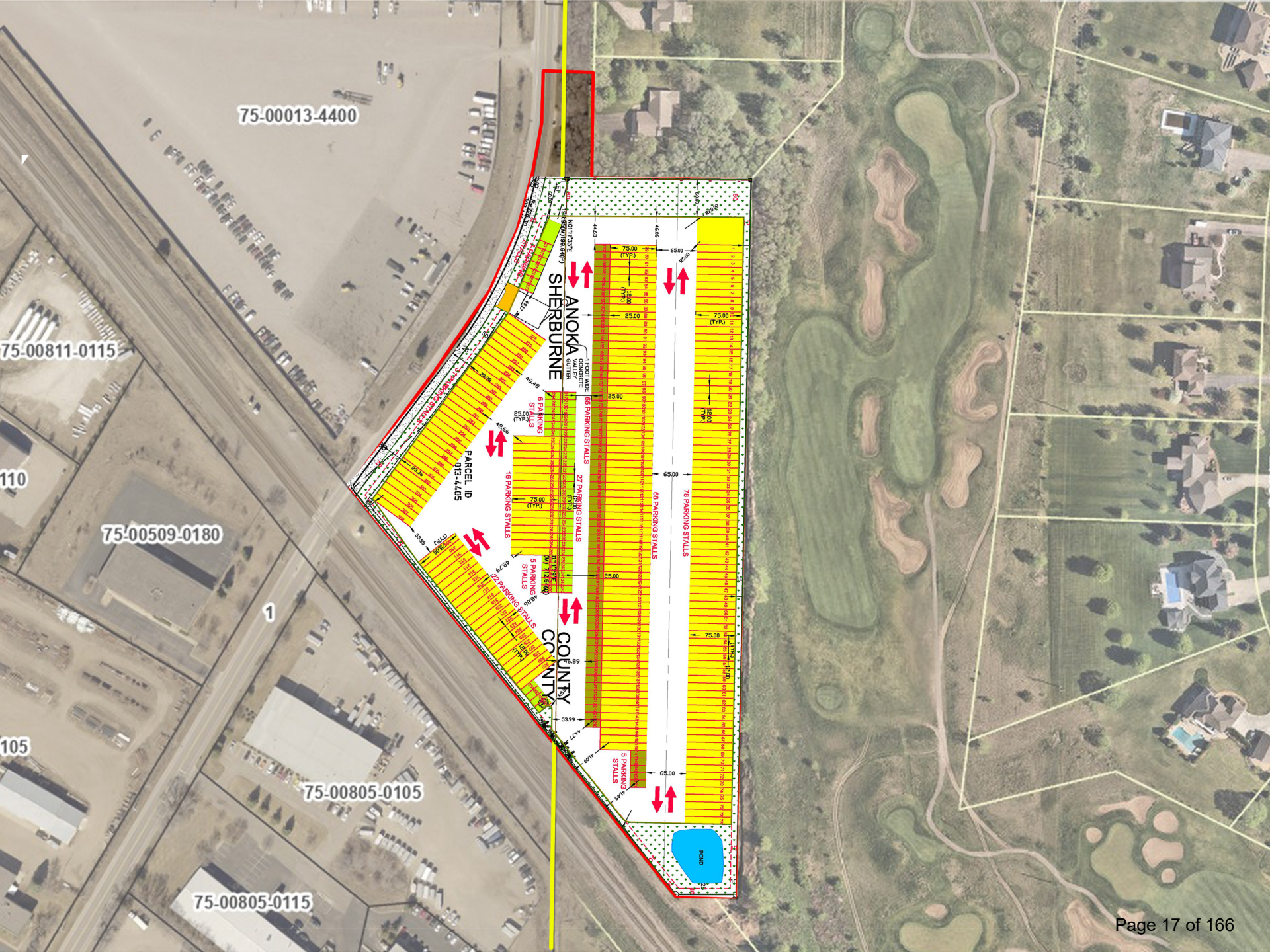
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75-00013-4400

75-00811-0115

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75-00509-0180

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75-00805-0105

75-00805-0115

ANOKA  
OUTLET  
SHERBURNE

COLINTY  
COUNTY

PARCEL ID  
013-4405

83 PARKING STALLS

27 PARKING STALLS

18 PARKING STALLS

22 PARKING STALLS

5 PARKING STALLS

5 PARKING STALLS

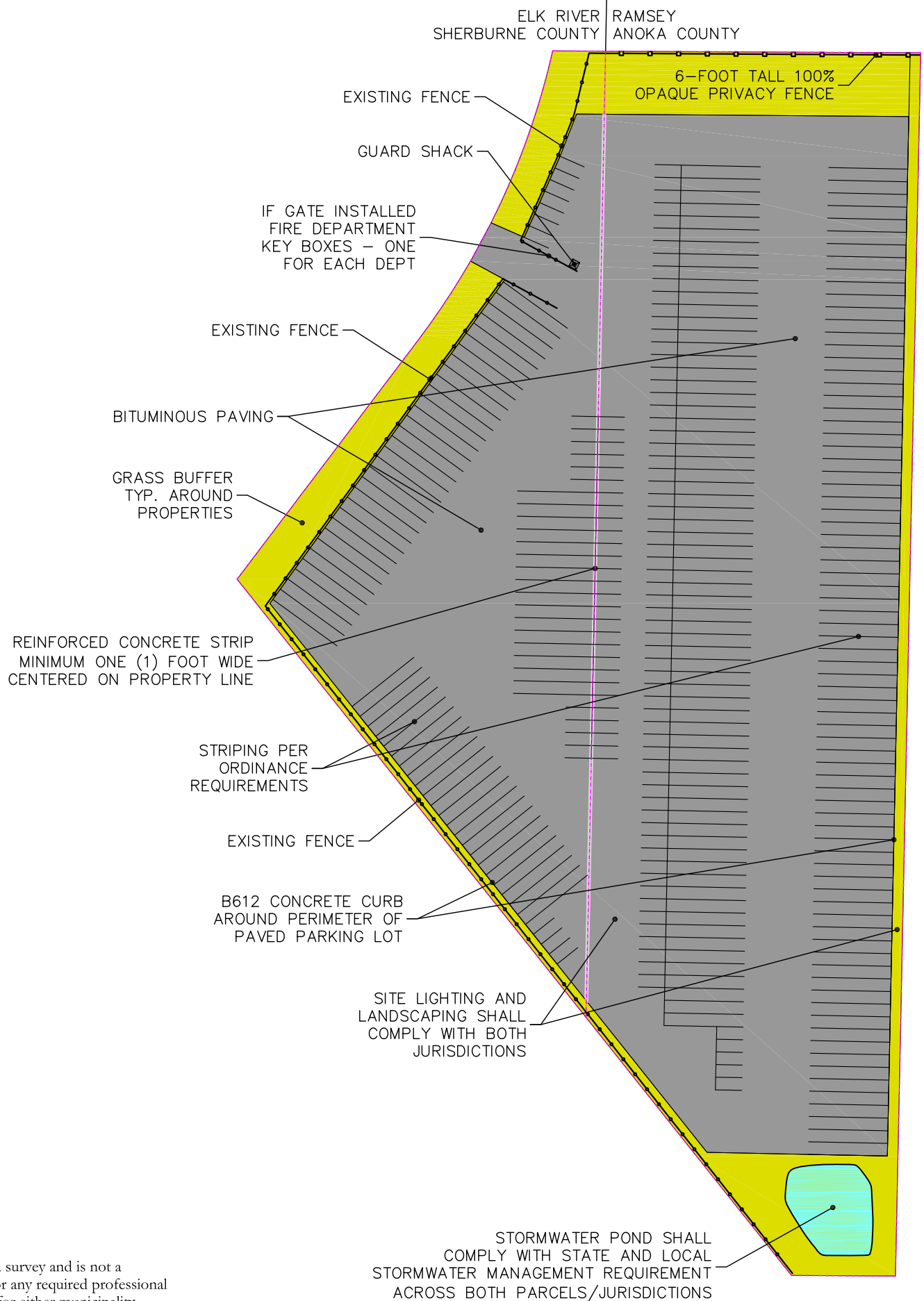
83 PARKING STALLS

78 PARKING STALLS

89 PARKING STALLS

POND

# City Site Plan - February 25, 2025



This is not a survey and is not a substitute for any required professional documents for either municipality.

**Regular Planning Commission****Meeting Date:** 01/23/2025**Primary Strategic Plan Initiative:** Promote economic growth and development.**Information****Title:**

PUBLIC HEARING: Northstar Truck &amp; RV Parking - Rezoning and Site Plan Review - 15861 Jarvis St. NW

**Purpose/Background:**Site Location

The site is located along the east side of Jarvis Street, immediately north of the BNSF railroad. The property is split with the western portion located in Elk River and Sherburne County and the eastern portion in Ramsey and Anoka County. The east side is adjacent to the Links at Northfork golf course with single-family residential to the north. Access to the site is on the Elk River side of the property.

Property Use and Zoning

The applicant's business is leasing out spaces for semi-truck parking and RV parking. Many of his clients are independent truckers who cannot store their trucks at home. Generally, a client drives his or her personal vehicle to the site, parks, and then takes the truck out for work. At the end of the route, the truck driver returns, parks the truck, gets into the personal vehicle, and goes home. Several trucks are also owned by area businesses as overflow parking for their sites. There are no buildings on the site. Parking for recreational vehicles (RVs) and boats would also be allowed, though parking of these vehicles will likely be for longer periods or seasonally.

This type of use would be considered somewhere between a storage yard and a parking lot. Ramsey City Code does not have a category to accurately describe this business use. Elk River does have a zoning district that allows for this type of use. Ramsey staff were not comfortable bringing forward modifications to the existing I-1 Light Industrial district as that has implications for other I-1 properties. Instead, both cities' staff are proposing a joint solution to rezone the property in both communities to the same Planned Unit Development (PUD) to accommodate this use while having a uniform set of rules. A PUD offers custom-tailored regulations for unique sites that cannot otherwise be achieved by standard zoning while conforming to the Comprehensive Plan (which is guided Business Park).

It should be noted that the business is currently operating without the necessary approvals. Staff has been working with the applicant on a path to bring the property into compliance with both Ramsey and Elk River Zoning and Maintenance Codes. Use of the property started over a year ago and was discovered via a code enforcement complaint. There are several damaged and inoperable vehicles on the site, some piles of debris, miscellaneous storage, all of which are not allowed by either City's Codes. Additionally, there are several vehicles that are being stored on the site that are owned by car dealers and salvagers. The applicant has been cleaning up the property, including working with the Fire Department by burning the old farmhouse and outbuildings.

Site Plan

The site plan shows a large bituminous-paved parking lot across the site. Along the city/county border, a concrete stripe will be set so that anything requiring a jurisdictional designation (i.e., prosecution of a vehicle theft) can be clearly determined. The parking lot will have concrete curb and gutter to direct storm water to management basins and infiltration trenches.

The parking lot is enclosed by an existing decorative extruded aluminum fence with a screening mesh along the Jarvis Street and BNSF sides. The plans propose a six-foot privacy fence along the property line with the residential property to the north. No fence is shown along the golf course side of the property nor is there a gate to

the site, so the site is not considered secure. Should the applicant add a gate, the Fire Departments for both communities will require a lock box for their access.

### Grading and Drainage

The site is generally flat. The applicant is proposing to lightly grade the site for drainage with storm water collecting in a basin at the south end of the site or a storm water trench along the southeastern portion of the site. Storm water management of this site is reviewed by the Lower Rum River WMO. Additional refinement of the grading and drainage plans are needed before submittal to the WMO.

Off the eastern property line on Links at Northfork property is a large berm. The applicant has reached out to golf course management to add trees to this berm for additional screening and to fulfill the landscaping requirements of the site. Some of the plans indicate modifying the berm and adding a retaining wall. Golf course management has indicated that they will consent to the landscaping, but not the grading and wall. Additional modifications to the grading plan along the eastern property line are likely necessary, but would not substantially impact the site design.

### Landscaping and Screening

From the EPB's January 15, 2025, meeting, it was noted that the submittal does include a Landscape Plan for the project. While there is a request to rezone the Subject Property to a PUD, which has no specific standards, the proposed use is most similar to that of an industrial use, and therefore, the minimum required plantings are to be determined using the canopy cover formula. As submitted, the Landscape Plan needs to be revised to include at least another 27,370 square feet of canopy cover. If, for example, the Applicant were to add a species such as American Elm, Bur Oak, and/or White Oak, this could be accomplished with as few as seven (7) additional trees. All the plantings along the eastern boundary are shown to be on the adjacent golf course property (on top of the existing berm). Staff has informed the Applicant that they need to provide the city with a copy of the written authorization from the adjacent property owner for the plantings to be on the golf course property.

The Landscape Plan does include a sixty (60) foot wide bufferyard along the northern property boundary, which abuts an existing residential property. Within the bufferyard, the Applicant is proposing a combination of deciduous overstory trees (Red Maple), evergreen trees (Green Giant Arborvitae), and a six (6) foot tall wooden privacy fence. Typically, a bufferyard would need to be landscaped with an additional thirty percent (30%) of the required plantings. However, the inclusion of the privacy fence reduces the amount of required plantings in the bufferyard by fifty percent (50%). Thus, an additional 6,320 square feet of canopy cover is needed and again, depending on the species chosen, could be accomplished with as few as two (2) additional trees.

City Code does provide standards for parking lot landscaping (one tree for every ten parking stalls). However, neither Ramsey nor Elk River Staff consider this to be a typical parking lot. This, in essence, simply serves as a storage area for larger vehicles. The landscape requirements for parking lots are intended to ensure there is some shade available for customers and/or employees (they are parking for shorter periods of time) and to create some visual relief. The users of this site will be parking their vehicle and essentially storing it on the Subject Property for longer periods of time. Additionally, it is worth noting that the plantings along the eastern boundary are directly adjacent to the easternmost row of stalls and thus, will provide some shade for those areas.

### Lighting

The applicant has provided a draft lighting plan. The plan shows areas where additional lighting is possible with the addition of freestanding light poles. The northern portion of the site is limited to a 20-foot mounting height (due to the adjacent residential use) with the majority of the site having a 30-foot height. Given the nature of the use, larger and wider drive aisle and deeper parking spaces for the trucks and trailers make pole placement challenging. With some modification, all of the parking areas should be able to meet Ramsey's 1.0 fc parking lot requirement. Many areas of the large drive aisles will be dimmer due to longer distances from light poles, but should be able to achieve the 0.5 fc exterior storage area requirement.

**Notification:**

Staff mailed public hearing notices to property owners within 350 feet of the side on the Anoka County side. Elk River staff sent notices in their community. A Ramsey "proposed development" sign was placed on the property along Jarvis Street. A public hearing notice was published in the January 17 Anoka Union Herald Newspaper.

**Time Frame/Observations/Alternatives:**

Alternatives to Consider:

1. Recommend to the City Council approval of the PUD Ordinance and Site Plan Review Resolution as presented by staff.
2. Recommend to the City Council approval with any modifications that the Planning Commission sees appropriate.
3. Recommend to the City Council denial of the proposal and resume Code Enforcement actions.

**Funding Source:**

All costs associated with processing this application are the responsibility of the applicant.

**Recommendation:**

Staff recommends approval of the PUD and associated site plan, contingent upon staff's review comments.

**Outcome/Action:**

Motion to recommend approval of the Planned Unit Development and Site Plan Review to the City Council.

**Attachments**

- Location Map
- Narrative
- Plans
- Draft Lighting Plan
- Photos (2025-01-14)
- Draft Ordinance #25-04
- Draft Resolution #25-028

**Form Review**

<b>Inbox</b>	<b>Reviewed By</b>	<b>Date</b>
Brian Hagen	Brian Hagen	01/16/2025 03:54 PM
Form Started By: Todd Larson		Started On: 01/13/2025 01:06 PM
Final Approval Date: 01/16/2025		

# Northstar Truck &RV Parking LLC

18-32-25-33-0002

15861 Jarvis St Ramsey, MN 55303

## Zoning Amendment/ Site Plan Review



Print Date: September 16, 2024

0 0.02 0.04 0.07 0.11 0.14 mi

## **ZC 24-03 Narrative**

NorthStar Truck & RV Parking LLC is seeking a PUD to provide outdoor storage for semi-trucks, trailers, recreational vehicles, buses, boats, containers, container chassis, heavy equipment, light equipment and personal vehicles.

The subject parcel is part of a larger parcel divided between two cities with approximately 6.75 acres of additional land in Ramsey. The city review will only be for the 3.83 portion located in the city limits.

The site is fenced with a gate that will be accessible via a code, providing clients with 24-hour access to the site. The owner will be the only employee working on the property.

### **Parking**

80 storage stalls are available, and the surface currently consists of Class 5 material but will be covered with asphalt in the near future.

### **Fencing**

There is existing decorative fencing along the railroad tracks and Jarvis Street.

There will be no building on the Elk River property.



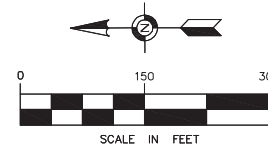
# NORTHSTAR TRUCK AND RV PARKING

15861 JARVIS ST. NW, RAMSEY, MN 55330 - ANOKA COUNTY  
 PID 75-013-4405 SHERBURNE COUNTY

**PROJECT LOCATION**

COUNTIES: ANOKA & SHERBURNE  
 CITY: ELK RIVER & RAMSEY

CIVIL DESIGN PLANS FOR:  
 PROPOSED CONSTRUCTION PARKING LOT,  
 SITE, EROSION, GRADING, DRAINAGE PLANS, STORM WATER MANAGEMENT,



Call 48 Hours before digging  
**GOPHER STATE ONE CALL**  
 Twin Cities Area 651-454-0002  
 MN. Toll Free 1-800-252-1166

**CONTACTS**  
 OWNER  
 NIKOLAY BABKIN  
 NORTHSTAR TRUCK  
 AND RV PARKING LLC  
 (763) 913-6874  
 northstarparkingmn@gmail.com

**GENERAL CONTRACTOR**  
 NIKOLAY BABKIN  
 NORTHSTAR TRUCK  
 AND RV PARKING LLC  
 (763) 913-6874  
 9961 TROY LANE NORTH  
 MAPLE GROVE, MN 55311  
 northstarparkingmn@gmail.com

**PLANNING MANAGER**  
 CITY OF RAMSEY  
 TODD LARSON  
 7550 SUNWOOD DR. NW,  
 RAMSEY, MN 55303  
 (763) 433-9821  
 tlarson@d.ramsey.mn.us

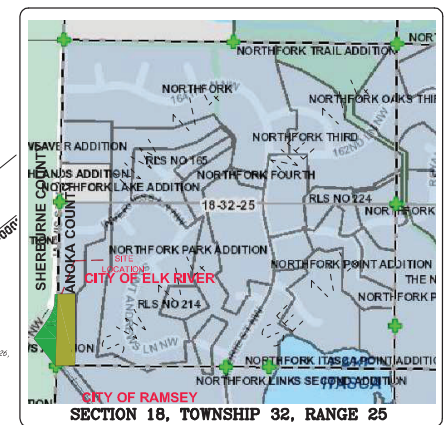
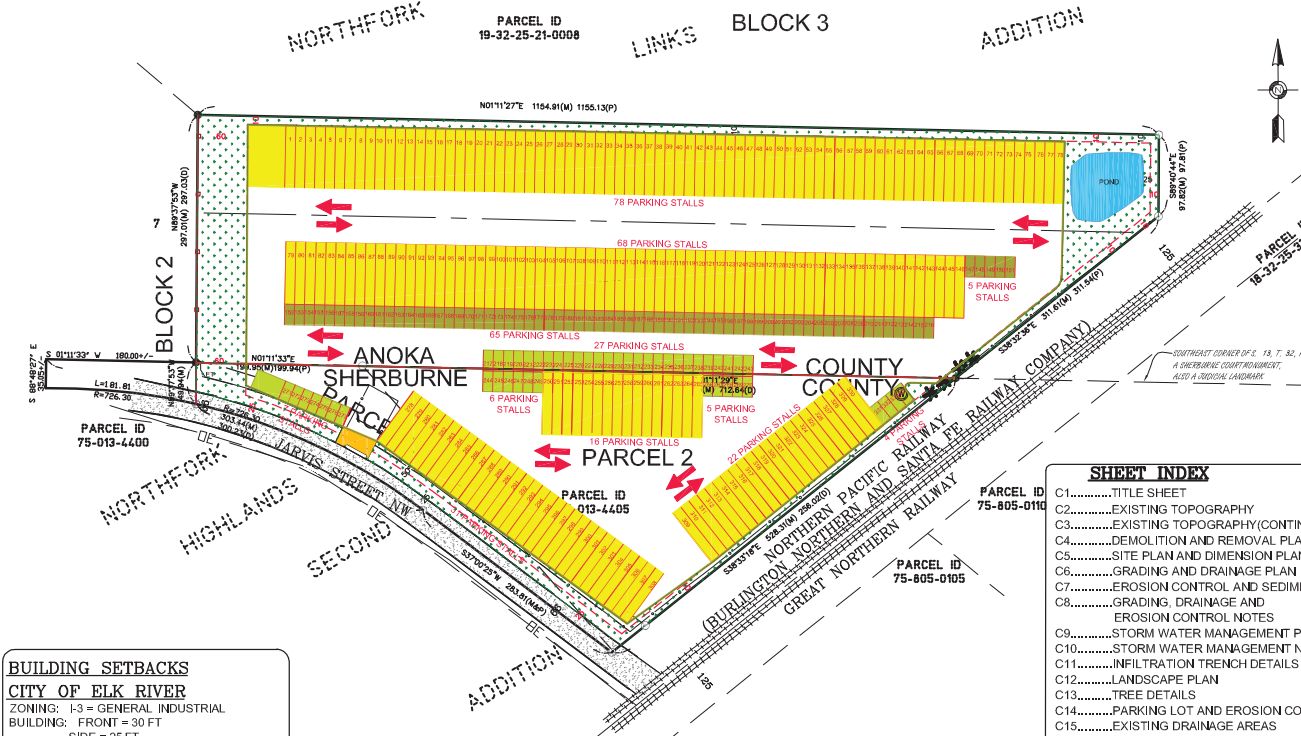
**SENIOR PLANNER**  
 CITY OF RAMSEY  
 CHRIS ANDERSON  
 7550 SUNWOOD DRIVE NW  
 RAMSEY, MN 55303  
 (763) 433-9817  
 canderson@cityoframsey.com

**PLANNING DEPARTMENT**  
 CITY OF ELK RIVER  
 ZACK CARLTON  
 COMMUNITY DEVELOPMENT  
 DIRECTOR  
 13065 ORONO PARKWAY  
 ELK RIVER, MN 55330  
 (763) 635-1035

**PUBLIC WORKS DIRECTOR**  
 /CHIEF ENGINEER  
 CITY OF ELK RIVER  
 JUSTIN FEMRITE  
 13065 ORONO PARKWAY  
 ELK RIVER, MN 55330  
 (763) 635-1000

**SENIOR PLANNER**  
 CITY OF ELK RIVER  
 CHRIS LEESBERG  
 13065 ORONO PARKWAY  
 ELK RIVER, MN 55330  
 (763) 635-1033  
 cleeseberg@elkrivermn.gov

**ENGINEER / SURVEYOR**  
 VLADIMIR SIRVIVER  
 ENGINEERING DESIGN &  
 SURVEYING, INC.  
 6480 WAYZATA BLVD.  
 MINNEAPOLIS, MN 55426  
 PHONE: (763) 545-2800  
 FAX (763) 545-2801  
 info@edsmn.com



**BUILDING SETBACKS**  
 CITY OF ELK RIVER  
 ZONING: I-3 = GENERAL INDUSTRIAL  
 BUILDING: FRONT = 30 FT  
 SIDE = 25 FT  
 REAR = 20 FT  
 PARKING: FRONT = 25 FT (STREET)  
 SIDE = 10 FT  
 REAR = 10 FT  
 PROPOSED ZONING: PUD= PLANNED UNIT DEVELOPMENT

**BUILDING SETBACKS CITY OF RAMSEY**  
 ZONING: I-1 = LIGHT INDUSTRIAL DISTRICT  
 BUILDING: FRONT = 35 FT  
 SIDE = 25 FT  
 REAR = 25 FT  
 PARKING: NORTH SIDE = 35 FT,  
 LANDSCAPE BUFFER = 60 FT  
 FRONT = N/A  
 REAR = N/A  
 PROPOSED ZONING: PUD= PLANNED UNIT DEVELOPMENT

**PROJECT BENCHMARK**  
 ELEVATION = 891.26 (NAVD 88)  
 MNDOT DISK "RUSTIC".

**PROPOSED PARKING**

TRUCK PARKING STALLS	75X12	217
REGULAR PARKING STALLS	25X12	119
TOTAL PARKING STALLS		334

**NOTES**

- PUD ZONING REQUIRES ALL VEHICLES / TRAILERS BE OPERABLE AND CURRENTLY LICENSED.
- FIRE WILL REQUIRE A GATE KEY SWITCH FROM THE KNOX COMPANY ([HTTPS://WWW.KNOXBOX.COM/PRODUCTS]) THAT WILL ALLOW THE FIRE DEPARTMENT TO ACCESS PROPERTY IN THE CASE OF AN EMERGENCY. MSFC 506.1 WHERE REQUIRED, WHERE ACCESS TO OR WITHIN A STRUCTURE OR AN AREA IS UNDULY DIFFICULT BECAUSE OF SECURED OPENINGS OR WHERE IMMEDIATE ACCESS IS NECESSARY FOR LIFE-SAVING OR FIRE-FIGHTING PURPOSES, THE FIRE CODE OFFICIAL IS AUTHORIZED TO REQUIRE A KEY BOX TO BE INSTALLED IN AN APPROVED LOCATION. THE KEY BOX SHALL BE OF AN APPROVED TYPE AND SHALL CONTAIN KEYS TO GAIN NECESSARY ACCESS AS REQUIRED BY THE FIRE CODE OFFICIAL.

**SHEET INDEX**

- C1.....TITLE SHEET
- C2.....EXISTING TOPOGRAPHY
- C3.....EXISTING TOPOGRAPHY(CONTINUE)
- C4.....DEMOLITION AND REMOVAL PLAN
- C5.....SITE PLAN AND DIMENSION PLAN
- C6.....GRADING AND DRAINAGE PLAN
- C7.....EROSION CONTROL AND SEDIMENT PLAN
- C8.....GRADING, DRAINAGE AND EROSION CONTROL NOTES
- C9.....STORM WATER MANAGEMENT PLAN
- C10.....STORM WATER MANAGEMENT NOTES
- C11.....INFILTRATION TRENCH DETAILS
- C12.....LANDSCAPE PLAN
- C13.....TREE DETAILS
- C14.....PARKING LOT AND EROSION CONTROL DETAILS
- C15.....EXISTING DRAINAGE AREAS
- C16.....PROPOSED DRAINAGE AREAS

**PROPOSED LEGEND**

- DENOTES PROPOSED BITUMINOUS SURFACE
- DENOTES PROPOSED CONCRETE AND CURB
- DENOTES PROPOSED PARKING STALLS 75X12
- DENOTES PROPOSED PARKING STALLS 25X12
- DENOTES PROPOSED CONCRETE 1 FOOT CURB
- DENOTES PROPOSED GRASS BUFFER
- DENOTES PROPOSED 1 FOOT GUTTER
- DENOTES PROPOSED TRAFFIC FLOW
- DENOTES PROPOSED POND
- DENOTES PROPOSED 6 FT HEIGHT OPAQUE FENCE

**LEGEND**

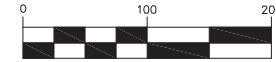
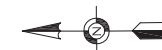
- DENOTES FOUND PROPERTY IRON
- DENOTES SET 1/2" X 18" REBAR WITH PLASTIC CAP "PLS 25105"
- DENOTES BOUNDARY LINE
- DENOTES LOT LINE
- DENOTES SETBACK LINE
- DENOTES RAILROAD LINE
- DENOTES EXISTING SPOT ELEVATION
- DENOTES CONCRETE SURFACE
- DENOTES EXISTING CONTOUR LINE
- DENOTES FINISH FLOOR ELEVATION
- DENOTES BITUMINOUS SURFACE
- DENOTES TREE LINE
- DENOTES STEEL FENCE
- DENOTES OVERHEAD ELECTRIC
- DENOTES ELECTRIC POWER POLE
- DENOTES MEASURED DISTANCE
- DENOTES PLATTED DISTANCE
- DENOTES SANITARY MANHOLE
- DENOTES ELECTRIC METER
- DENOTES DRAINAGE FLOW
- DENOTES ELECTRIC TRANSFORMER
- DENOTES WATER WELL
- DENOTES SHRUB

**EDS ENGINEERING DESIGN & SURVEYING**  
 6480 Wayzata Blvd., Minneapolis, MN 55426  
 OFFICE: (763) 545-2800 FAX: (763) 545-2801  
 EMAIL: info@edsmn.com WEBSITE: http://edsmn.com

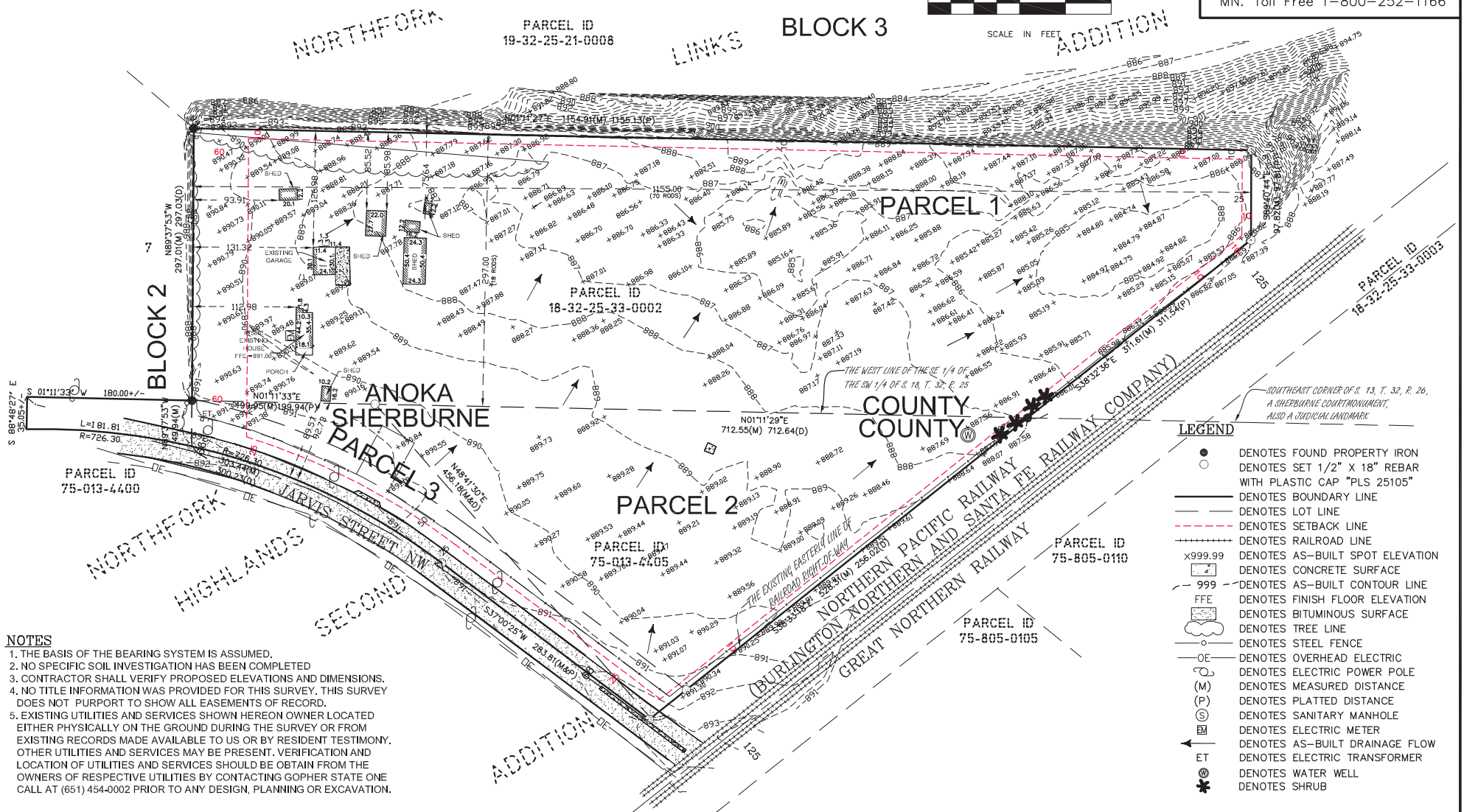
I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
*Vlad Sirviver*  
 VLADIMIR SIRVIVER P.E., NO. 25105 DATED: 12/31/2024

<b>TITLE SHEET</b>	FIELD WORK DATE: 09/26/2023	DRAWN BY: IS	PROJECT NO.: 23-146
LOCATION: 15861 JARVIS STREET NORTHWEST RAMSEY, MN 55330	FIELD BOOK NO.: EDS-15	CHECKED BY: VS	SHEET NO. C1

# EXISTING TOPOGRAPHY for NICK BABKIN



Call 48 Hours before digging  
**GOPHER STATE ONE CALL**  
 Twin Cities Area 651-454-0002  
 MN. Toll Free 1-800-252-1166



- NOTES**
1. THE BASIS OF THE BEARING SYSTEM IS ASSUMED.
  2. NO SPECIFIC SOIL INVESTIGATION HAS BEEN COMPLETED
  3. CONTRACTOR SHALL VERIFY PROPOSED ELEVATIONS AND DIMENSIONS.
  4. NO TITLE INFORMATION WAS PROVIDED FOR THIS SURVEY, THIS SURVEY DOES NOT PURPORT TO SHOW ALL EASEMENTS OF RECORD.
  5. EXISTING UTILITIES AND SERVICES SHOWN HEREON LOCATED EITHER PHYSICALLY ON THE GROUND DURING THE SURVEY OR FROM EXISTING RECORDS MADE AVAILABLE TO US OR BY RESIDENT TESTIMONY. OTHER UTILITIES AND SERVICES MAY BE PRESENT. VERIFICATION AND LOCATION OF UTILITIES AND SERVICES SHOULD BE OBTAIN FROM THE OWNERS OF RESPECTIVE UTILITIES BY CONTACTING GOPHER STATE ONE CALL AT (651) 454-0002 PRIOR TO ANY DESIGN, PLANNING OR EXCAVATION.

- LEGEND**
- DENOTES FOUND PROPERTY IRON
  - DENOTES SET 1/2" X 18" REBAR WITH PLASTIC CAP "PLS 25105"
  - DENOTES BOUNDARY LINE
  - - - DENOTES LOT LINE
  - - - DENOTES SETBACK LINE
  - +—+— DENOTES RAILROAD LINE
  - x999.99 DENOTES AS-BUILT SPOT ELEVATION
  - DENOTES CONCRETE SURFACE
  - 999 DENOTES AS-BUILT CONTOUR LINE
  - FFE DENOTES FINISH FLOOR ELEVATION
  - ☁ DENOTES BITUMINOUS SURFACE
  - ☁ DENOTES TREE LINE
  - DENOTES STEEL FENCE
  - OE— DENOTES OVERHEAD ELECTRIC
  - ⊙ DENOTES ELECTRIC POWER POLE
  - (M) DENOTES MEASURED DISTANCE
  - (P) DENOTES PLATTED DISTANCE
  - ⊙ DENOTES SANITARY MANHOLE
  - ⊙ DENOTES ELECTRIC METER
  - ← DENOTES AS-BUILT DRAINAGE FLOW
  - ET DENOTES ELECTRIC TRANSFORMER
  - ⊙ DENOTES WATER WELL
  - ✱ DENOTES SHRUB

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 6480 Wayzata Blvd, Minneapolis, MN 55426  
 OFFICE: (763) 545-2800 FAX: (763) 545-2801  
 EMAIL: info@edsmn.com WEBSITE: http://edsmn.com

I HEREBY CERTIFY THAT THIS SURVEY WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION, AND THAT I AM A DULY LICENSED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.  
*Vlad Sivriyev*  
 VLADIMIR SIVRIYEV L.S. NO. 25105 DATED: 12/13/2024

**JOB NAME:** NICK BABKIN  
**LOCATION:** 15861 JARVIS STREET NORTHWEST  
 RAMSEY, MN 55330

**FIELD WORK DATE:** 09/26/2023  
**FIELD BOOK NO.:** EDS-15

**DRAWN BY:** IS  
**CHECKED BY:** VS  
**PROJECT NO.:** 23-146  
**SHEET NO.:** C2

# EXISTING TOPOGRAPHY (CONTINUE)

Call 48 Hours before digging  
GOPHER STATE ONE CALL  
 Twin Cities Area 651-454-0002  
 MN. Toll Free 1-800-252-1166

## EXHIBIT A

### LEGAL DESCRIPTION OF LAND

#### Parcel 1:

That part or the South 70 rods or the West 18 rods or the Southwest Quarter of the Southwest Quarter of Section 18, Township 32, Range 25, Anoka County, Minnesota, lying North of the railroad right-of-way).  
 (Real Estate ID. No. 18-32-25-33-0002)

AND

#### Parcel 2:

That part of the Southeast Quarter or the Southeast Quarter of Section 13, Township 32, Range 26, Sherburne County, Minnesota, described as follows:

Beginning on the East line or said Section, 2.56 chains North or the Southeast corner thereof and at a point where said line is crossed by the Easterly line of the right-of-way of the St. Paul, Minneapolis & Manitoba Railway Company; thence North on section line 11.39 chains to center of road from State Road to Quarter post in said section line; thence South 47-1/2 degrees West to said Easterly line of said right-of-way; thence South 30 degrees East along said right-of-way to beginning.  
 (Real Estate ID No. 75-011-2300)

## EXHIBIT B

### E-3 EMPLOYMENT (BUSINESS PARK) PROPERTY

That part of South 1,155.00 feet of West 297.00 feet of the Southwest Quarter of the Southwest Quarter of Section 18, Township 32, Range 25, lying North of Railroad Right-of-Way, Excluding Road, Subject to Easement of Record, Anoka County, Minnesota, according to the recorded plat thereof.

### EXISTING HARDCOVER

EXISTING HOUSE	880 SQ. FT.
EXISTING GARAGE	729 SQ. FT.
EXISTING SHEDS	2,542 SQ. FT.
EXISTING CONCRETE SURFACE	1,223 SQ. FT.
EXISTING BITUMINOUS SURFACE	7,839 SQ. FT.
TOTAL IMPERVIOUS AREA	13,213 SQ. FT.
TOTAL LOT AREA	480,470 SQ. FT.
EXISTING HARDCOVER	2.8 %

### REFERENCE BENCHMARK

ELEVATION = 891.26 (NAVD 88) MNDOT DISK  
 "RUSTIC".

### BUILDING SETBACKS CITY OF RAMSEY

ZONING: I-1 = LIGHT INDUSTRIAL DISTRICT  
 BUILDING: FRONT = 35 FT  
           SIDE = 25 FT  
           REAR = 25 FT  
 PARKING: NORTH SIDE = 40 FT.  
           FRONT = N/A  
           REAR = N/A

### BUILDING SETBACKS CITY OF ELK RIVER

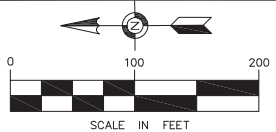
ZONING: I-3 = GENERAL INDUSTRIAL  
 BUILDING: FRONT = 30 FT  
           SIDE = 25 FT  
           REAR = 20 FT  
 PARKING: FRONT = 25 FT (STREET)  
           SIDE = 10 FT  
           REAR = 10 FT



I HEREBY CERTIFY THAT THIS SURVEY WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION, AND THAT I AM A DULY LICENSED LAND SURVEYOR UNDER THE LAWS OF THE STATE OF MINNESOTA.  
*Vlad Sivriver*  
 VLADIMIR SIVRIVER L.S. NO. 25105 DATED: 12/13/2024

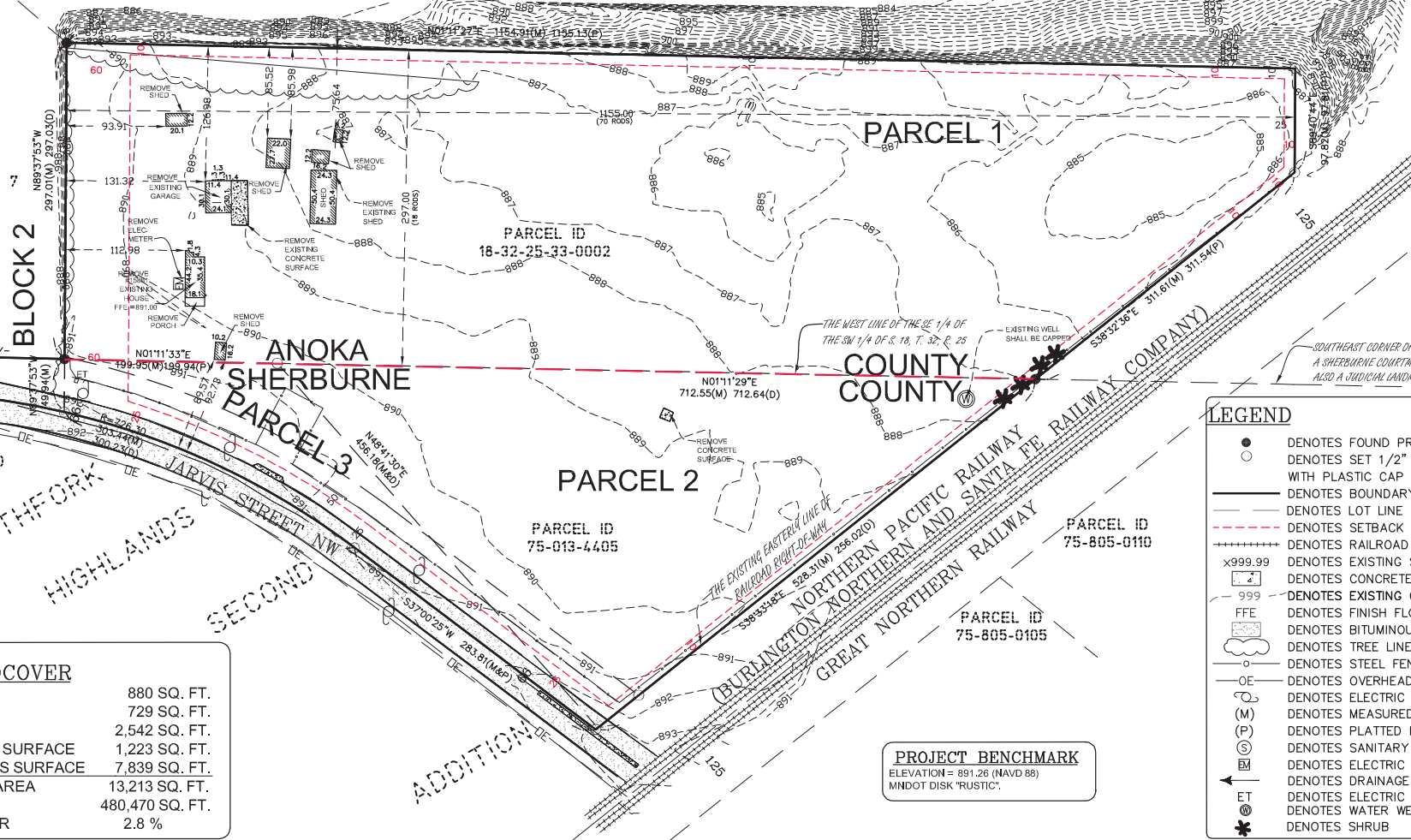
JOB NAME: NICK BABKIN	FIELD WORK DATE: 09/26/2023	DRAWN BY: IS	PROJECT NO.: 23-146
LOCATION: 15861 JARVIS STREET NORTHWEST RAMSEY, MN 55330	FIELD BOOK NO.: EDS-15	CHECKED BY: VS	SHEET NO. C3

# REMOVAL AND DEMOLITION PLAN



NORTHFORK LINKS BLOCK 3 ADDITION

PARCEL ID  
19-32-25-21-0008



PARCEL ID  
75-013-4400

PARCEL ID  
18-32-25-33-0002

PARCEL ID  
75-013-4405

PARCEL ID  
75-805-0110

PARCEL ID  
75-805-0105

SOUTHEAST CORNER OF S. 13, T. 32, R. 26,  
A SHERBURNE COURTHOUSE PLAT, ALSO A JUDICIAL LANDMARK

EXISTING HARDCOVER	
EXISTING HOUSE	880 SQ. FT.
EXISTING GARAGE	729 SQ. FT.
EXISTING SHEDS	2,542 SQ. FT.
EXISTING CONCRETE SURFACE	1,223 SQ. FT.
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TOTAL IMPERVIOUS AREA	13,213 SQ. FT.
TOTAL LOT AREA	480,470 SQ. FT.
EXISTING HARDCOVER	2.8 %

**PROJECT BENCHMARK**  
ELEVATION = 891.26 (NAVD 88)  
MINDOT DISK "RUSTIC".

LEGEND	
	DENOTES FOUND PROPERTY IRON
	DENOTES SET 1/2" X 18" REBAR WITH PLASTIC CAP "PLS 25105"
	DENOTES BOUNDARY LINE
	DENOTES LOT LINE
	DENOTES SETBACK LINE
	DENOTES RAILROAD LINE
	DENOTES EXISTING SPOT ELEVATION
	DENOTES CONCRETE SURFACE
	DENOTES EXISTING CONTOUR LINE
	DENOTES FINISH FLOOR ELEVATION
	DENOTES BITUMINOUS SURFACE
	DENOTES TREE LINE
	DENOTES STEEL FENCE
	DENOTES OVERHEAD ELECTRIC
	DENOTES ELECTRIC POWER POLE
	DENOTES MEASURED DISTANCE
	DENOTES PLATTED DISTANCE
	DENOTES SANITARY MANHOLE
	DENOTES ELECTRIC METER
	DENOTES DRAINAGE FLOW
	DENOTES ELECTRIC TRANSFORMER
	DENOTES WATER WELL
	DENOTES SHRUB

**EDS** ENGINEERING DESIGN & SURVEYING  
6480 Wayzata Blvd. Minneapolis, MN 55426  
OFFICE: (763) 545-2800 FAX: (763) 545-2801  
EMAIL: info@edsmn.com WEBSITE: http://edsmn.com

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
*Vlad Sivriyer*  
VLADIMIR SIVRIYER P.E., NO. 25105 DATED: 12/31/2024

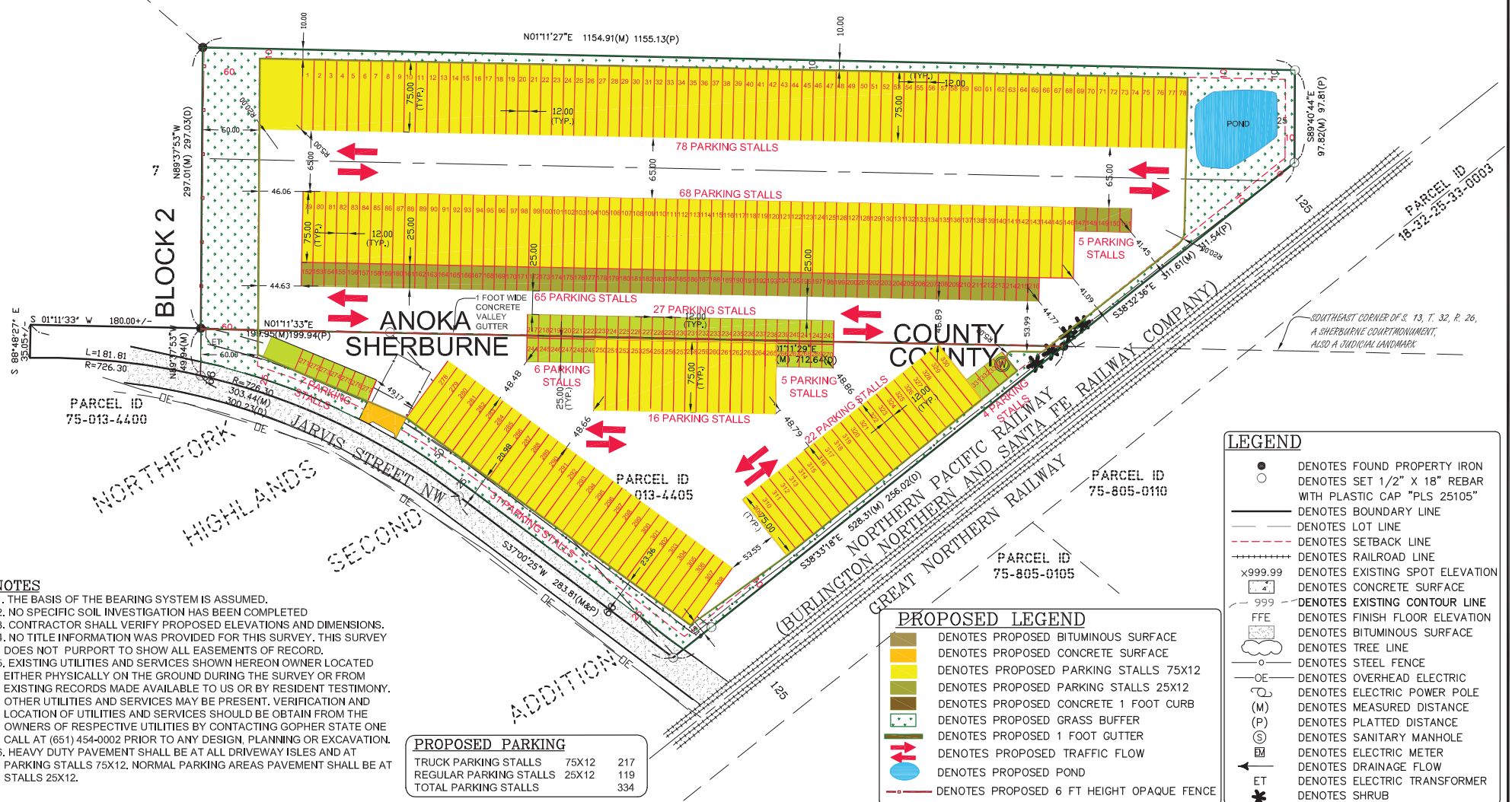
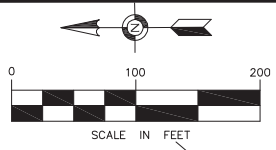
**REMOVAL AND DEMOLITION PLAN**  
LOCATION: 15861 JARVIS STREET NORTHWEST  
RAMSEY, MN 55330

FIELD WORK DATE: 09/26/2023	DRAWN BY: IS	PROJECT NO.: 23-146
FIELD BOOK NO.: EDS-15	CHECKED BY: VS	SHEET NO. C4

# SITE AND DIMENSION PLAN

## TRUCK PARKING LAYOUT

Call 48 Hours before digging  
**GOPHER STATE ONE CALL**  
 Twin Cities Area 651-454-0002  
 MN. Toll Free 1-800-252-1166



- NOTES**
1. THE BASIS OF THE BEARING SYSTEM IS ASSUMED.
  2. NO SPECIFIC SOIL INVESTIGATION HAS BEEN COMPLETED
  3. CONTRACTOR SHALL VERIFY PROPOSED ELEVATIONS AND DIMENSIONS.
  4. NO TITLE INFORMATION WAS PROVIDED FOR THIS SURVEY. THIS SURVEY DOES NOT PURPORT TO SHOW ALL EASEMENTS OF RECORD.
  5. EXISTING UTILITIES AND SERVICES SHOWN HEREON OWNER LOCATED EITHER PHYSICALLY ON THE GROUND DURING THE SURVEY OR FROM EXISTING RECORDS MADE AVAILABLE TO US OR BY RESIDENT TESTIMONY. OTHER UTILITIES AND SERVICES MAY BE PRESENT. VERIFICATION AND LOCATION OF UTILITIES AND SERVICES SHOULD BE OBTAIN FROM THE OWNERS OF RESPECTIVE UTILITIES BY CONTACTING GOPHER STATE ONE CALL AT (651) 454-0002 PRIOR TO ANY DESIGN, PLANNING OR EXCAVATION.
  6. HEAVY DUTY PAVEMENT SHALL BE AT ALL DRIVEWAY ISLES AND AT PARKING STALLS 75X12. NORMAL PARKING AREAS PAVEMENT SHALL BE AT STALLS 25X12.

PROPOSED PARKING		
TRUCK PARKING STALLS	75X12	217
REGULAR PARKING STALLS	25X12	119
<b>TOTAL PARKING STALLS</b>		<b>334</b>

PROPOSED LEGEND	
	DENOTES PROPOSED BITUMINOUS SURFACE
	DENOTES PROPOSED CONCRETE SURFACE
	DENOTES PROPOSED PARKING STALLS 75X12
	DENOTES PROPOSED PARKING STALLS 25X12
	DENOTES PROPOSED GRASS BUFFER
	DENOTES PROPOSED 1 FOOT CURB
	DENOTES PROPOSED CONCRETE 1 FOOT VALLEY GUTTER
	DENOTES PROPOSED TRAFFIC FLOW
	DENOTES PROPOSED POND
	DENOTES PROPOSED 6 FT HEIGHT OPAQUE FENCE

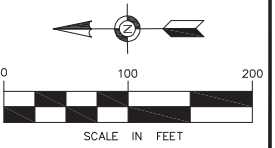
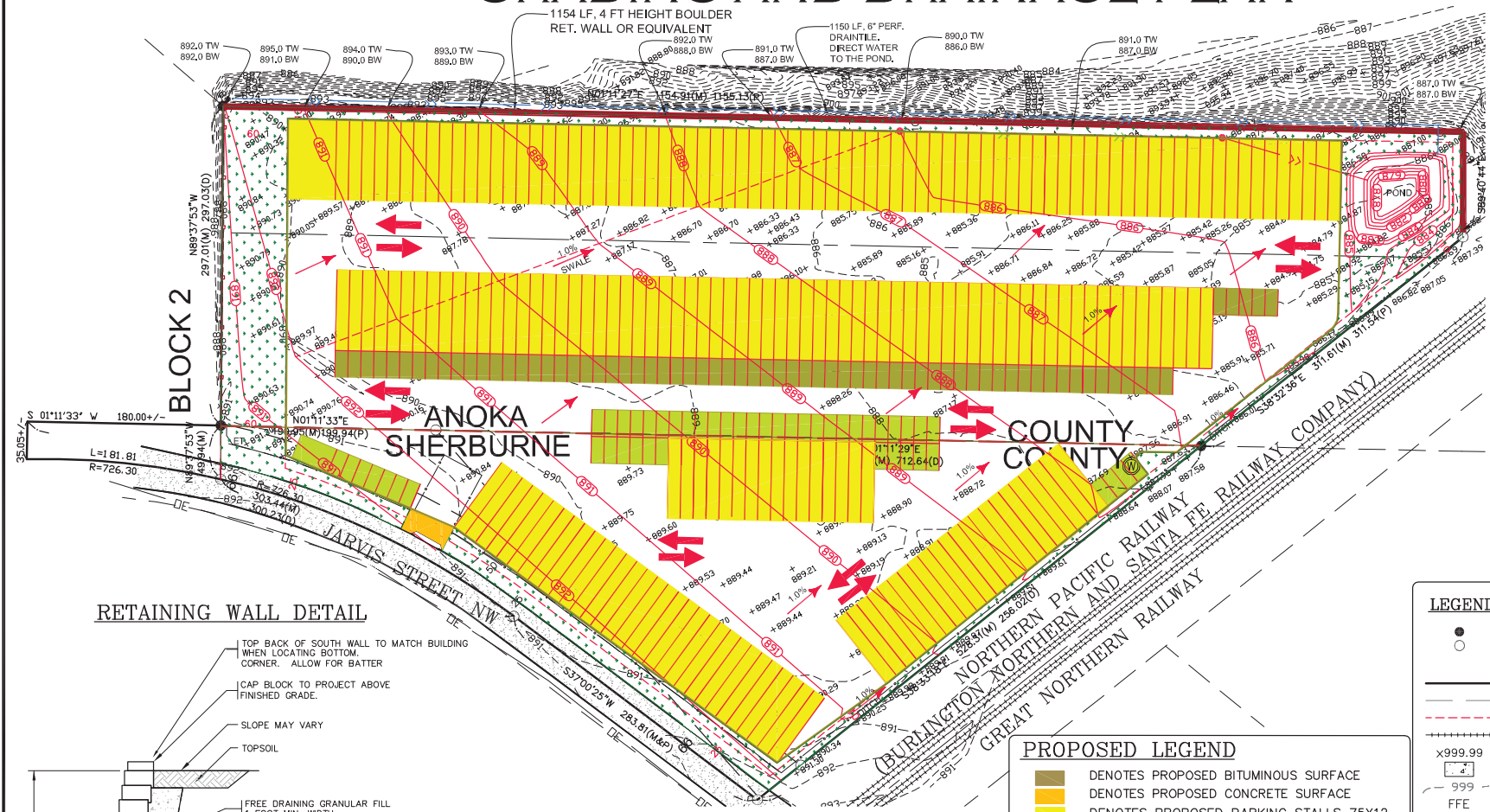
LEGEND	
	DENOTES FOUND PROPERTY IRON
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	DENOTES BOUNDARY LINE
	DENOTES LOT LINE
	DENOTES SETBACK LINE
	DENOTES RAILROAD LINE
	DENOTES EXISTING SPOT ELEVATION
	DENOTES CONCRETE SURFACE
	DENOTES EXISTING CONTOUR LINE
	DENOTES FINISH FLOOR ELEVATION
	DENOTES BITUMINOUS SURFACE
	DENOTES TREE LINE
	DENOTES STEEL FENCE
	DENOTES OVERHEAD ELECTRIC
	DENOTES ELECTRIC POWER POLE
	DENOTES MEASURED DISTANCE
	DENOTES PLATTED DISTANCE
	DENOTES SANITARY MANHOLE
	DENOTES ELECTRIC METER
	DENOTES DRAINAGE FLOW
	DENOTES ELECTRIC TRANSFORMER
	DENOTES SHRUB

**EDS ENGINEERING DESIGN & SURVEYING**  
 6480 Woyzata Blvd, Minneapolis, MN 55426  
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*Vlad Sivriker*  
 VLADIMIR SIVRIKER P.E., NO. 25105 DATED: 12/31/2024

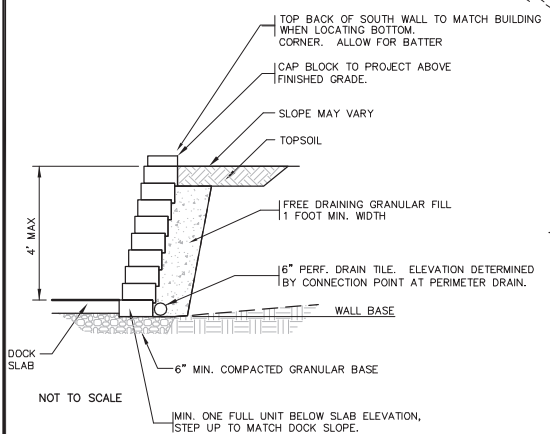
<b>SITE AND DIMENSION PLAN</b>	FIELD WORK DATE: 09/26/2023	DRAWN BY: IS	PROJECT NO.: 23-146
LOCATION: 15861 JARVIS STREET NORTHWEST RAMSEY, MN 55330	FIELD BOOK NO.: EDS-15	CHECKED BY: VS	SHEET NO. C5

# GRADING AND DRAINAGE PLAN



**PROJECT BENCHMARK**  
 ELEVATION = 891.26 (NAVD 88)  
 MNDOT DISK "RUSTIC".

## RETAINING WALL DETAIL



## NOTES

1. THE BASIS OF THE BEARING SYSTEM IS ASSUMED.
2. NO SPECIFIC SOIL INVESTIGATION HAS BEEN COMPLETED.
3. CONTRACTOR SHALL VERIFY PROPOSED ELEVATIONS AND DIMENSIONS.
4. NO TITLE INFORMATION WAS PROVIDED FOR THIS SURVEY. THIS SURVEY DOES NOT PURPORT TO SHOW ALL EASEMENTS OF RECORD.
5. EXISTING UTILITIES AND SERVICES SHOWN HEREON OWNER LOCATED EITHER PHYSICALLY ON THE GROUND DURING THE SURVEY OR FROM EXISTING RECORDS MADE AVAILABLE TO US OR BY RESIDENT TESTIMONY. OTHER UTILITIES AND SERVICES MAY BE PRESENT. VERIFICATION AND LOCATION OF UTILITIES AND SERVICES SHOULD BE OBTAIN FROM THE OWNERS OF RESPECTIVE UTILITIES BY CONTACTING GOPHER STATE ONE CALL AT (651) 454-0002 PRIOR TO ANY DESIGN, PLANNING OR EXCAVATION.

## PROPOSED LEGEND

- DENOTES PROPOSED BITUMINOUS SURFACE
- DENOTES PROPOSED CONCRETE SURFACE
- DENOTES PROPOSED PARKING STALLS 75X12
- DENOTES PROPOSED PARKING STALLS 25X12
- DENOTES PROPOSED CONCRETE 1 FOOT CURB
- DENOTES PROPOSED GRASS BUFFER
- DENOTES PROPOSED CONTOUR
- DENOTES PROPOSED GRADING SPOT ELEVATION
- DENOTES PROPOSED RETAINING WALL
- DENOTES PROPOSED 1 FOOT GUTTER
- DENOTES PROPOSED TRAFFIC FLOW
- DENOTES PROPOSED DRAINAGE FLOW
- DENOTES PROPOSED 6 FT HEIGHT OPAQUE FENCE

## LEGEND

- DENOTES FOUND PROPERTY IRON
- DENOTES SET 1/2" X 18" REBAR WITH PLASTIC CAP "PLS 25105"
- DENOTES BOUNDARY LINE
- DENOTES LOT LINE
- DENOTES SETBACK LINE
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- DENOTES EXISTING CONTOUR LINE
- DENOTES FINISH FLOOR ELEVATION
- DENOTES BITUMINOUS SURFACE
- DENOTES TREE LINE
- DENOTES STEEL FENCE
- DENOTES OVERHEAD ELECTRIC
- DENOTES ELECTRIC POWER POLE
- DENOTES MEASURED DISTANCE
- DENOTES PLATTED DISTANCE
- DENOTES SANITARY MANHOLE
- DENOTES ELECTRIC METER
- DENOTES DRAINAGE FLOW
- DENOTES ELECTRIC TRANSFORMER

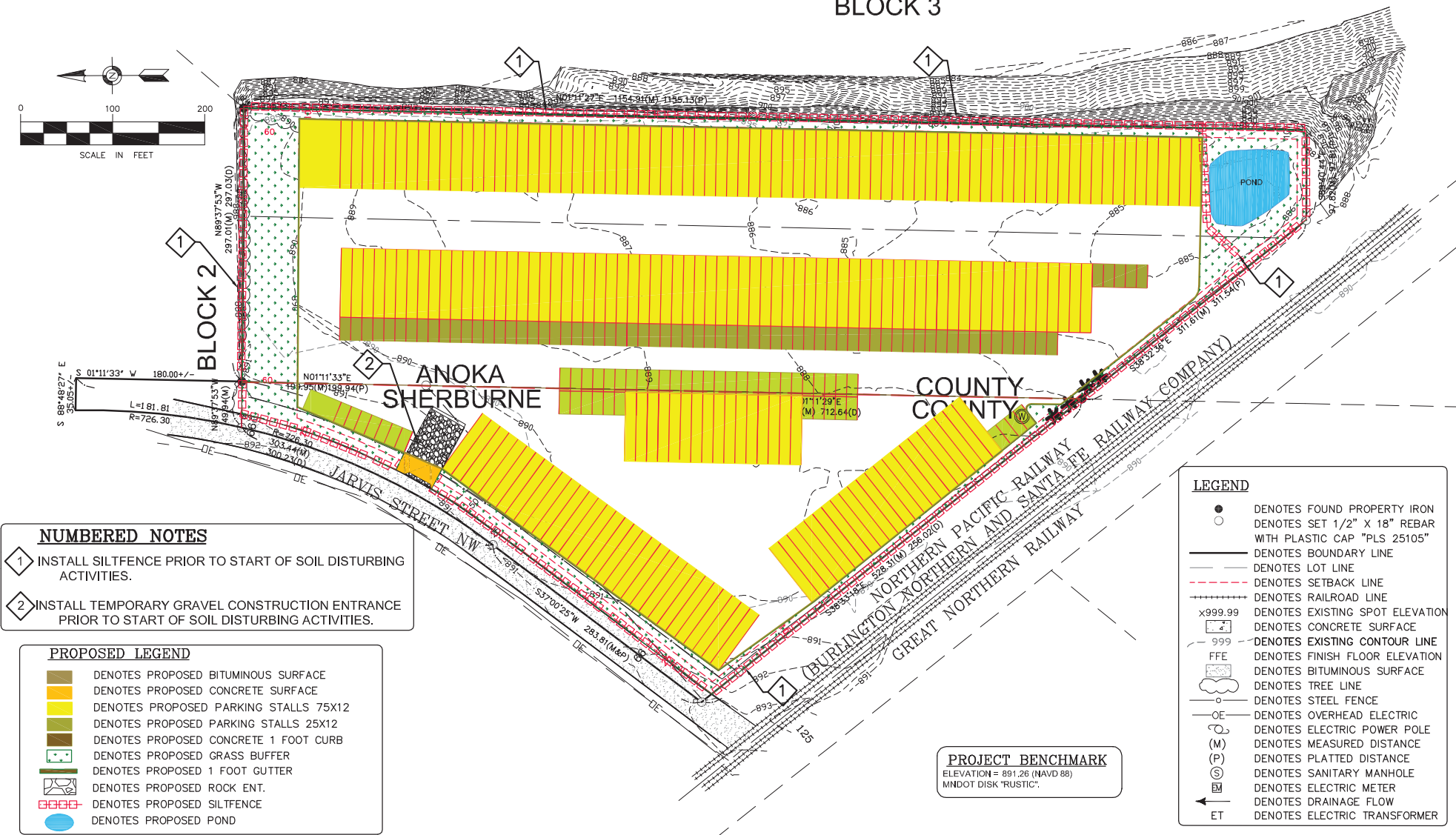
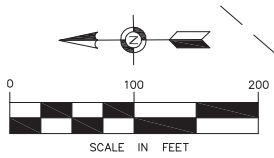
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*Vlad Sivirer*  
 VLADIMIR SIVIRER P.E., NO. 25105 DATED: 12/31/2024

<b>GRADING AND DRAINAGE PLAN</b>		FIELD WORK DATE: 09/26/2023	DRAWN BY: IS	PROJECT NO.: 23-146
LOCATION: 15861 JARVIS STREET NORTHWEST RAMSEY, MN 55330		FIELD BOOK NO.: EDS-15	CHECKED BY: VS	SHEET NO. C6

# EROSION CONTROL AND SEDIMENT PLAN

## BLOCK 3



- NUMBERED NOTES**
- 1 INSTALL SILTFENCE PRIOR TO START OF SOIL DISTURBING ACTIVITIES.
  - 2 INSTALL TEMPORARY GRAVEL CONSTRUCTION ENTRANCE PRIOR TO START OF SOIL DISTURBING ACTIVITIES.

- PROPOSED LEGEND**
- DENOTES PROPOSED BITUMINOUS SURFACE
  - DENOTES PROPOSED CONCRETE SURFACE
  - DENOTES PROPOSED PARKING STALLS 75X12
  - DENOTES PROPOSED PARKING STALLS 25X12
  - DENOTES PROPOSED CONCRETE 1 FOOT CURB
  - DENOTES PROPOSED GRASS BUFFER
  - DENOTES PROPOSED 1 FOOT GUTTER
  - DENOTES PROPOSED ROCK ENT.
  - DENOTES PROPOSED SILTFENCE
  - DENOTES PROPOSED POND

- LEGEND**
- DENOTES FOUND PROPERTY IRON
  - DENOTES SET 1/2" X 18" REBAR WITH PLASTIC CAP "PLS 25105"
  - DENOTES BOUNDARY LINE
  - DENOTES LOT LINE
  - DENOTES SETBACK LINE
  - DENOTES RAILROAD LINE
  - x999.99 DENOTES EXISTING SPOT ELEVATION
  - DENOTES CONCRETE SURFACE
  - DENOTES EXISTING CONTOUR LINE
  - DENOTES FINISH FLOOR ELEVATION
  - DENOTES BITUMINOUS SURFACE
  - DENOTES TREE LINE
  - DENOTES STEEL FENCE
  - DENOTES OVERHEAD ELECTRIC
  - DENOTES ELECTRIC POWER POLE
  - DENOTES MEASURED DISTANCE
  - DENOTES PLATTED DISTANCE
  - DENOTES SANITARY MANHOLE
  - DENOTES ELECTRIC METER
  - DENOTES DRAINAGE FLOW
  - DENOTES ELECTRIC TRANSFORMER

**PROJECT BENCHMARK**  
 ELEVATION = 891.26 (NAVD 88)  
 MINDOT DISK "RUSTIC".

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*Vlad Sivriyev*  
 VLADIMIR SIVRIYEV P.E., NO. 25105 DATED: 12/31/2024

<b>EROSION CONTROL AND SEDIMENT PLAN</b>	FIELD WORK DATE: 09/26/2023	DRAWN BY: IS	PROJECT NO.: 23-146
LOCATION: 15861 JARVIS STREET NORTHWEST RAMSEY, MN 55330	FIELD BOOK NO.: EDS-15	CHECKED BY: VS	SHEET NO. C7

# GRADING, DRAINAGE AND EROSION CONTROL NOTES

## GRADING NOTES

1. CONTRACTOR SHALL FIELD VERIFY THE LOCATIONS AND ELEVATIONS OF EXISTING UTILITIES AND TOPOGRAPHIC FEATURES PRIOR TO START OF SITE GRADING. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT ENGINEER OF ANY DISCREPANCIES OR VARIATIONS.
2. SUITABLE GRADING MATERIAL SHALL CONSIST OF ALL SOIL ENCOUNTERED ON THE SITE WITH EXCEPTION OF TOPSOIL DEBRIS, ORGANIC MATERIAL AND OTHER UNSTABLE MATERIAL, STOCKPILE TOPSOIL AND GRANULAR FILL AT LOCATIONS DIRECTED BY CONTRACTOR.
3. SUBGRADE EXCAVATION SHALL BE BACKFILLED IMMEDIATELY AFTER EXCAVATION TO HELP OFFSET ANY STABILITY PROBLEMS DUE TO WATER SEEPAGE OR STEEP SLOPES. WHEN PLACING NEW SURFACE MATERIAL ADJACENT TO EXISTING PAVEMENT, THE EXCAVATION SHALL BE BACKFILLED PROMPTLY TO AVOID UNDERMINING OF THE EXISTING PAVEMENT.
4. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL HORIZONTAL AND VERTICAL CONTROL.
5. GRADES SHOWN ARE FINISHED GRADES. CONTRACTOR SHALL ROUGH GRADE TO SUBGRADE ELEVATION.
6. ALL EXCESS MATERIAL, BITUMINOUS SURFACING, CONCRETE ITEMS, ANY ABANDONED UTILITY ITEMS, AND OTHER UNSTABLE MATERIALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OFF THE CONSTRUCTION SITE.
8. COMPLETION OF SITE GRADING OPERATIONS SHALL RESULT IN ALL AREAS BEING GRADED TO 'PLAN SUBGRADE ELEVATION'. THE PARKING LOT AND DRIVEWAY AREAS SHALL BE DETERMINED BY REFERRING TO THE SITE PLAN AND PAVEMENT SECTION DETAILS FOR LOCATION AND LIMITS OF BITUMINOUS PAVEMENT SECTIONS.
9. THE MINIMUM GRADED SLOPE FROM EDGE OF BUILDING SHALL BE 6 INCHES IN 10 FEET.
10. FINISHED GROUND AND SOD ELEVATION ADJACENT TO BUILDING SHALL BE 6" BELOW FLOOR ELEVATION. SLOPE GROUND AWAY FROM BUILDING A MINIMUM OF 6" IN 10 FEET BEYOND 10 FEET REFER TO PLAN GRADES.
11. CONTRACTOR IS RESPONSIBLE FOR GRADING AND SLOPING THE FINISHED GROUND SURFACE TO PROVIDE SMOOTH & UNIFORM SLOPES, WHICH PROVIDE POSITIVE DRAINAGE AWAY FROM BUILDINGS AND PREVENT POONDING IN LOWER AREAS. CONTACT ENGINEER IF FIELD ADJUSTMENTS TO GRADING PLANS ARE REQUIRED.
12. CONTRACTOR SHALL REMOVE ONLY THOSE TREES MARKED IN THE FIELD VERIFY WITH ENGINEER PRIOR TO REMOVAL.
13. ALL GRADING SHALL BE ACCORDING TO MnDOT 2105.
14. EXISTING CURB CUTS SHALL BE REMOVED AND REPLACED WITH CURB AND GUTTER PER CITY STANDARD DETAILS (AND SPECIFICATIONS) .

## GRADING, TURF ESTABLISHMENT & EROSION CONTROL SPECIFICATIONS

### TURF ESTABLISHMENT

ALL TURF ESTABLISHMENT SHALL BE ACCORDING TO MnDOT 2575 AND SHALL TAKE PLACE WITHIN 14 DAYS OF THE COMPLETED GRADING OPERATION.

### TOPSOIL

CONTRACTOR SHALL STRIP, STOCKPILE AND RE-SPREAD SUFFICIENT TOPSOIL TO PROVIDE A MINIMUM 4 INCH DEPTH (COMPACTED) TO ALL DISTURBED AREAS, TO BE SODDED OR SEEDED. (FOR TOPSOIL REQUIREMENTS SEE DETAIL ERO-6 ON SHEET C14.)

### SOD

SOD SHALL BE ACCORDING TO MnDOT 3878.

### SEEDING

AREAS IN BUFFERS AND ADJACENT TO OR IN WET AREAS MnDOT SEED MIX 33-261 (STORMWATER POND) AT 35 LBS PER ACRE.

DRY AREAS MnDOT SEED MIX 35-221 (DRY PRAIRIE GENERAL) AT 40 LBS PER ACRE.

MAINTENANCE SHALL BE IN ACCORDANCE TO THE MnDOT SEEDING MANUAL.

### MULCHING

APPLY UNIFORM COVERING OF EROSION CONTROL BLANKET SUCH IS RAMY TURF FUTERRA.

### FERTILIZER

TYPE 20-0-10 AT 400# PER ACRE.

### GRADING

ALL GRADING SHALL BE ACCORDING TO MnDOT 2105.

## SEDIMENT & EROSION CONTROL NOTES

1. CONTRACTOR IS RESPONSIBLE FOR THE CLEANLINESS OF THE SITE AND THE MAINTENANCE OF THE EROSION AND SEDIMENT CONTROLS.
2. THE STREET WILL BE SWEEPED CLEAN BEFORE THE END OF EACH DAY OF ACTIVE CONSTRUCTION, WHEN SEDIMENT IS TRACKED INTO THE STREET.
3. AREAS WITH SLOPES GREATER THAN 3 TO 1 AND AREAS NEXT TO WETLANDS/WATERBODIES GRADED OR EXPOSED DURING CONSTRUCTION SHALL BE PROTECTED WITH TEMPORARY VEGETATION, MULCHING, OR OTHER MEANS AS SOON AS PRACTICAL.
4. ALL EXPOSED SOIL AREAS WILL BE STABILIZED AS SOON AS PRACTICAL. UNWORKED SOILS THAT REMAIN EXPOSED AND NOT IN USE FOR LONGER THAN 7 DAYS WILL BE COVERED WITH TEMPORARY SEED (GRASS, OATS, OR WHEAT).
5. NO CONCRETE WASHOUT SHALL OCCUR ON SITE UNLESS IT IS DONE WITH AN APPROVED MINNESOTA POLLUTION CONTROL AGENCY (MPCA) DEVICE OR STANDARD.
6. STOCKPILES SHALL BE SURROUNDED WITH ADEQUATE PERIMETER CONTROL TO PREVENT SEDIMENTATION AND EROSION.
7. INLET PROTECTION FOR ALL STORM SEWER INLETS DOWNSTREAM OF THE SITE WITHIN ONE BLOCK OR AS DIRECTED BY THE CITY.
8. SITE SHALL BE KEPT CLEAN AT ALL TIMES AND REFUSE PROPERLY CONTROLLED.
9. TEMPORARY PUMPING SHALL NOT BE PERMITTED WITHOUT THE USE OF AN APPROVED MINNESOTA POLLUTION CONTROL AGENCY (MPCA) DEVICE OR STANDARD.
10. SOIL COMPACTION SHALL BE MINIMIZED; AREAS OF COMPACTED SOIL WILL BE REMOVED OR LOOSENED VIA TILLING TO A DEPTH OF NO LESS THAN 6-INCHES.
11. THE CONTRACTOR SHALL INSPECT ON A WEEKLY BASIS AND AFTER ANY RAINFALL GREATER THAN 1" ALL EROSION CONTROL DEVICES AND MAKE ANY REPAIRS IMMEDIATELY. AN INSPECTION LOG SHALL BE KEPT ON SITE DETAILING THESE INSPECTIONS AND REPAIRS PERFORMED.

## GENERAL NOTES

1. SEE SWMP NOTES ON SHEET C10 FOR FURTHER SWMP REQUIREMENTS. IN CASE OF CONFLICT BETWEEN THIS PLAN AND SWMP NOTES, THE SWMP NOTES SHALL OVERRULE.
2. ALL EROSION CONTROL MEASURES MUST BE INSTALLED AT THE INITIAL STAGES OF CONSTRUCTION AND MAINTAINED UNTIL ALL AREAS ALTERED HAVE BEEN RESTORED.
3. ALL REAR YARD AREAS OF LOTS AT SETBACK LINE ARE TO BE MULCHED AND SEEDED WITHIN 7 DAYS OF GRADING BY GRADING CONTRACTOR. ALL AREAS BETWEEN THE CURB AND SETBACK LINE ARE TO BE MULCHED AND SEEDED WITHIN 7 DAYS OF PRIVATE UTILITY INSTALLATION BY THE UTILITY CONTRACTOR.
4. STREET SWEEPING MUST BE UNDERTAKEN ON AN AS-NEEDED BASIS.
5. PERFORM SEEDING FOR FINAL STABILIZATION OF DISTURBED AREA AS FOLLOWS:
  - a. REPLACE TOPSOIL TO PROVIDE A UNIFORM THICKNESS. LOOSEN TOPSOIL TO MINIMUM DEPTH OF 3".
  - b. APPLY COMMERCIAL GRADE SLOW RELEASE FERTILIZER PER 1,000 SQUARE FEET.
  - c. INCORPORATE FERTILIZER INTO SOIL BY USE OF HARROW OR OTHER MEANS TO PLACE FERTILIZER BELOW GROUND LEVEL.



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*Vlad Sivriker*  
 VLADIMIR SIVRIKER P.E., NO. 25105 DATED: 12/31/2024

### GRADING, DRAINAGE AND EROSION CONTROL NOTES

LOCATION: 15861 JARVIS STREET NORTHWEST  
 RAMSEY, MN 55330

FIELD WORK DATE: 09/26/2023

FIELD BOOK NO.: EDS-15

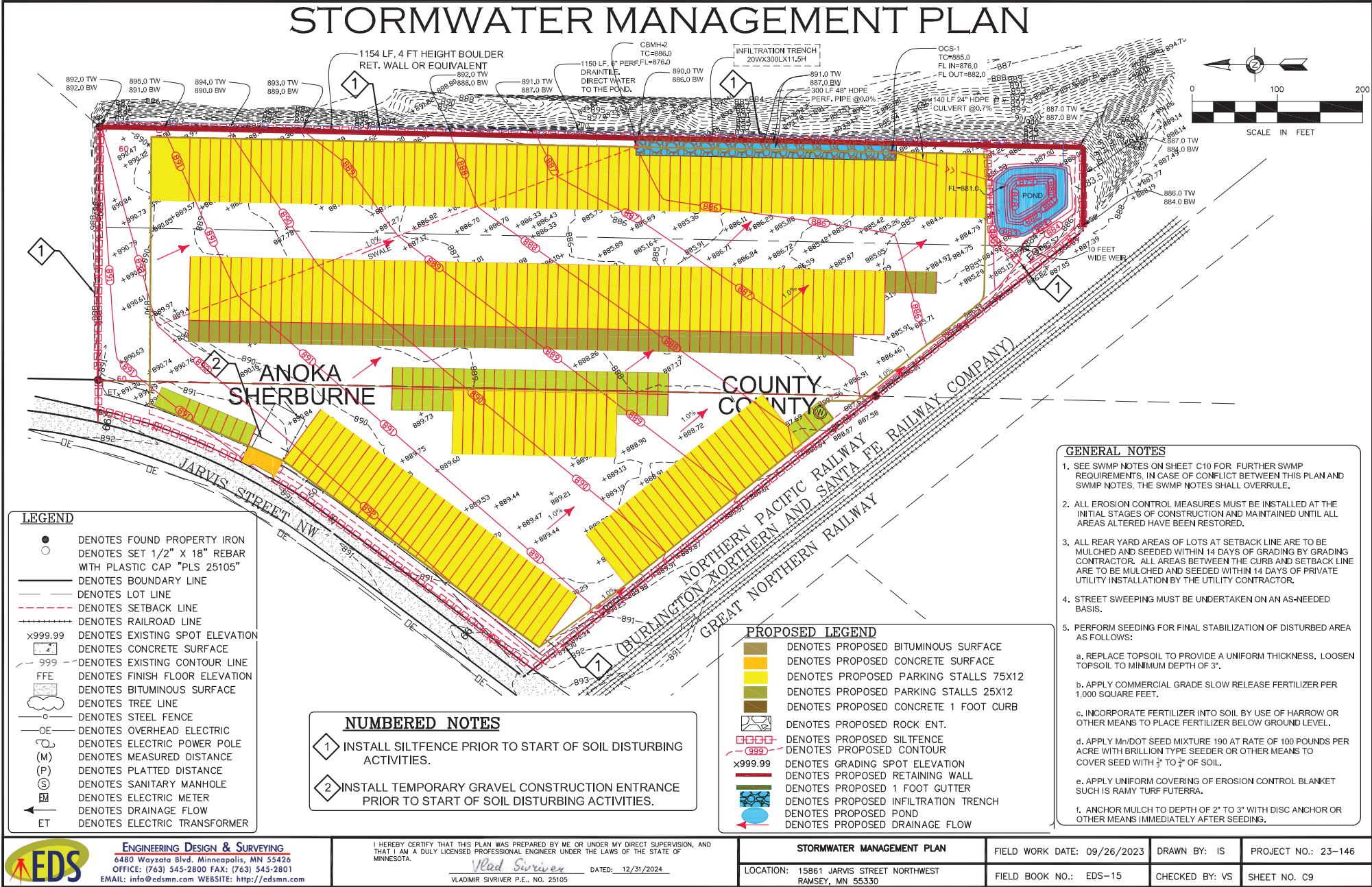
DRAWN BY: IS

CHECKED BY: VS

PROJECT NO.: 23-146

SHEET NO. C8

# STORMWATER MANAGEMENT PLAN



LEGEND	
●	DENOTES FOUND PROPERTY IRON
○	DENOTES SET 1/2" X 18" REBAR WITH PLASTIC CAP "PLS 25105"
—	DENOTES BOUNDARY LINE
- - -	DENOTES LOT LINE
- · - · -	DENOTES SETBACK LINE
—+—+—+—	DENOTES RAILROAD LINE
x999.99	DENOTES EXISTING SPOT ELEVATION
■	DENOTES CONCRETE SURFACE
- 999 -	DENOTES EXISTING CONTOUR LINE
FFE	DENOTES FINISH FLOOR ELEVATION
■	DENOTES BITUMINOUS SURFACE
○	DENOTES TREE LINE
○	DENOTES STEEL FENCE
—OE—	DENOTES OVERHEAD ELECTRIC
○	DENOTES ELECTRIC POWER POLE
(M)	DENOTES MEASURED DISTANCE
(P)	DENOTES PLATTED DISTANCE
(S)	DENOTES SANITARY MANHOLE
EM	DENOTES ELECTRIC METER
←	DENOTES DRAINAGE FLOW
ET	DENOTES ELECTRIC TRANSFORMER

NUMBERED NOTES	
1	INSTALL SILTFENCE PRIOR TO START OF SOIL DISTURBING ACTIVITIES.
2	INSTALL TEMPORARY GRAVEL CONSTRUCTION ENTRANCE PRIOR TO START OF SOIL DISTURBING ACTIVITIES.

PROPOSED LEGEND	
■	DENOTES PROPOSED BITUMINOUS SURFACE
■	DENOTES PROPOSED CONCRETE SURFACE
■	DENOTES PROPOSED PARKING STALLS 75X12
■	DENOTES PROPOSED PARKING STALLS 25X12
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- 999 -	DENOTES PROPOSED CONTOUR
x999.99	DENOTES GRADING SPOT ELEVATION
■	DENOTES PROPOSED RETAINING WALL
■	DENOTES PROPOSED 1 FOOT GUTTER
■	DENOTES PROPOSED INFILTRATION TRENCH
○	DENOTES PROPOSED POND
←	DENOTES PROPOSED DRAINAGE FLOW

GENERAL NOTES	
1.	SEE SWMP NOTES ON SHEET C10 FOR FURTHER SWMP REQUIREMENTS. IN CASE OF CONFLICT BETWEEN THIS PLAN AND SWMP NOTES, THE SWMP NOTES SHALL OVERRULE.
2.	ALL EROSION CONTROL MEASURES MUST BE INSTALLED AT THE INITIAL STAGES OF CONSTRUCTION AND MAINTAINED UNTIL ALL AREAS ALTERED HAVE BEEN RESTORED.
3.	ALL REAR YARD AREAS OF LOTS AT SETBACK LINE ARE TO BE MULCHED AND SEEDED WITHIN 14 DAYS OF GRADING BY GRADING CONTRACTOR. ALL AREAS BETWEEN THE CURB AND SETBACK LINE ARE TO BE MULCHED AND SEEDED WITHIN 14 DAYS OF PRIVATE UTILITY INSTALLATION BY THE UTILITY CONTRACTOR.
4.	STREET SWEEPING MUST BE UNDERTAKEN ON AN AS-NEEDED BASIS.
5.	PERFORM SEEDING FOR FINAL STABILIZATION OF DISTURBED AREA AS FOLLOWS: <ul style="list-style-type: none"> <li>a. REPLACE TOPSOIL TO PROVIDE A UNIFORM THICKNESS. LOOSEN TOPSOIL TO MINIMUM DEPTH OF 3".</li> <li>b. APPLY COMMERCIAL GRADE SLOW RELEASE FERTILIZER PER 1,000 SQUARE FEET.</li> <li>c. INCORPORATE FERTILIZER INTO SOIL BY USE OF HARROW OR OTHER MEANS TO PLACE FERTILIZER BELOW GROUND LEVEL.</li> <li>d. APPLY Mn/DOT SEED MIXTURE 190 AT RATE OF 100 POUNDS PER ACRE WITH BRILLIANT TYPE SEEDER OR OTHER MEANS TO COVER SEED WITH 3/8" TO 1/2" OF SOIL.</li> <li>e. APPLY UNIFORM COVERING OF EROSION CONTROL BLANKET SUCH IS RAMY TURF FUTERRA.</li> <li>f. ANCHOR MULCH TO DEPTH OF 2" TO 3" WITH DISC ANCHOR OR OTHER MEANS IMMEDIATELY AFTER SEEDING.</li> </ul>

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*Vlad Sivriev*  
 VLADIMIR SIVRIEV P.E., NO. 25105 DATED: 12/31/2024

STORMWATER MANAGEMENT PLAN		FIELD WORK DATE:	DRAWN BY:	PROJECT NO.:
LOCATION: 1561 JARVIS STREET NORTHWEST RAMSEY, MN 55330		09/26/2023	IS	23-146
FIELD BOOK NO.:		EDS-15	CHECKED BY: VS	SHEET NO. C9

# STORMWATER MANAGEMENT NOTES

## Storm Water Management Plan

The work described to implement the following Storm Water Management Plan (SWMP) shall be considered part of the Contract Documents and shall be performed by the Contractor. The work to install and maintain the Best Management Practices (BMPs) to prevent erosion and provide sediment control shall be in accordance with Permit No. MN R10000 and shall include, but are not necessarily be limited to, the requirements contained herein.

### 1. Construction Activity Information

Project Name: PARKING LOT CONSTRUCTION  
 Project Address/Location: Section-Township-Range = 18-032-25 AND 75-013-4405  
 City/Township: City of ELK RIVER State: MN Zip Code: 55330  
 County Parcel ID Number(s): 11-029-22-24-0020  
 All cities where construction will occur: City of ELK RIVER  
 All townships where construction will occur: Not Applicable  
 All counties where construction will occur: SHERBURNE COUNTY  
 Project Size (number of acres to be disturbed): 11.03  
 Project Type:  Residential  Commercial/Industrial  
 Road Construction  Other (describe)

Cumulative Impervious Surface:  
 Existing area of impervious surface to nearest quarter acre: 0.25  
 Post construction area of impervious surface to nearest quarter acre: 10.75  
 Receiving Waters:

Name of Water Body	Type	Appendix A Special Water?
Mississippi and Crow River	Yes	
4, 600 ft (0.87 mi.) from intersection of Mississippi and Crow River to the subject property.		

Dates of Construction  
 Estimated Construction Start Date: APRIL, 2025  
 Estimated Completion Date: NOVEMBER, 2025

### Contact Information

Owner of Project Site: NICK BABKIN  
 Business/Firm Name: NORTHSTAR TRUCK & RV PARKING LLC  
 Federal Tax ID Number: available as needed  
 State Tax ID Number: available as needed  
 Contact Person: NICK BABKIN  
 Title: OWNER  
 Name: NICK BABKIN  
 Phone: 763-913-4874  
 Mailing Address: 15861 JARVIS STREET NW,  
 City: ELK RIVER  
 State: MN Zip Code: 55330

### Contractor (Party who will oversee implementation of the SWMP. May be same party as Owner above):

NICK BABKIN  
 Business/Firm Name: NORTHSTAR TRUCK & RV PARKING LLC  
 Federal Tax ID Number: available as needed  
 State Tax ID Number: available as needed  
 Contact Person: NICK BABKIN  
 Title: Owner  
 Name: NICK BABKIN  
 Phone: 763-913-4874  
 Mailing Address: 9961 TROY LANE NORTH  
 City: MAPLE GROVE  
 State: MN Zip Code: 55311

### General Construction Project Information

Description of the construction activity (what will be built, general timelines, etc.).

PARKING LOT CONSTRUCTION. The construction activities will include grading operations for the construction of surface drainage and utilities. Roof drain sewer construction, exposed soil stabilization, and bituminous paving will follow grading operations.

### 2. General Site Information

Description of the location and type of all temporary and permanent erosion prevention and sediment control BMP's to be used, including the timing for installation and procedures used to establish additional temporary BMP's as necessary.

Contractor shall install and maintain the temporary and permanent erosion prevention and sediment control BMP's as shown on the accompanying drawing and as described herein. The timing shall be in accordance with the Construction Activity Sequence below and in accordance with sound and proactive construction scheduling and practices.

Accompanying this SWMP is a site plan that includes the following features:

- \* Existing and proposed grades, including dividing lines and direction of flow for all pre- and post-construction stormwater runoff drainage areas located within the project limits.
- \* Locations of impervious surfaces and soil types.
- \* Locations of areas not to be disturbed.
- \* Method(s) to be used for final stabilization of all exposed soil areas.

Description of stormwater mitigation measures required as the result of an environmental, archaeological, or other required local, state, or federal review of the project:

Not applicable to this project.

Description of the type and locations of BMP's appropriate for this site and sufficient to comply with all applicable requirements of the TMDL implementation plan and identification of the receiving water and of the areas of project site discharging to an impaired water that has an approved TMDL implementation plan that contains requirements for construction Stormwater discharges:

Not applicable to this project.

Selection of Permanent Stormwater Management System

Will the project create a new cumulative impervious surface greater than or equal to one acre?  Yes  No

If yes, a water quality volume of 1/2 inch of runoff from this area must be treated before leaving the site or entering surface waters (1 inch of runoff from this area if discharging to special waters).

Method(s) to be used to treat runoff from the new impervious surfaces created by the project:

Wet sedimentation basin  Infiltration/Filtration basin/POND

EXISTING

Regional ponds  Combination of practices

Description of treatment method(s) to be used, including design information for each method:

Existing pond will be used to collect water from the roof.

Description of why it is not feasible to meet the treatment requirement for water quality volume. This can include proximity to bedrock or road projects where the lack of right-of-way precludes the installation of any permanent stormwater management practices. Description of what other treatment, such as grassed swales, smaller ponds, or grit chambers, will be implemented to treat runoff prior to discharge to surface waters:

Not Applicable

Description of how a proposed alternative method to treat runoff from new impervious surfaces will achieve approximately 80% removal of total suspended solids on an annual average basis:

Not Applicable

### 3. Erosion Prevention Practices

Description of construction phasing, vegetative buffer strips, horizontal slope grading, and other construction practices to be used to minimize erosion:

Stage the soil disturbing activities to minimize the amount of disturbed soil prior to stabilization. Disturbed areas will be considered stabilized when covered with materials such as anchored mulch, staked sod, riprap, wood fiber blanket, or other materials that prevent erosion from occurring. Grass seeding alone will not be considered stabilization.

Disturb only those areas where proposed contours and elevations are shown to differ from the existing and where removals and other operations are noted. Special areas of the project site that are not to be disturbed by construction activity are noted on the plan.

Description of temporary erosion protection or permanent cover to be used for exposed positive slopes within 200 linear feet of a surface water (stream, lake, pond, marsh, wetland, reservoir spring, river, storm water basin, storm water drainage system, waterways, water courses, and irrigation systems) whether surface water is natural or artificial, public or private with maximum time an exposed soil area can remain exposed when the area is not actively being worked:

Provide year round stabilization to the above mentioned areas in accordance with the following table:

Steepest of slope (H:V)	Maximum Exposure Duration When Area Is Not Actively Worked
Greater than 3:1	7 Days
3:1 through 10:1	14 Days
Less than 10:1	21 Days

Description of practices to be used to stabilize the normal wetted perimeter of drainage or diversion ditches within 200 linear feet of the property edge or point of discharge to a surface water within 24 hours of connecting the ditch to the surface water.

Install seed, fertilizer, and disc-anchored mulch or temporary wood fiber blanket in ditches and swales within 24 hours of connecting the ditch or swale to the surface water where shown on the plan in accordance with the details.

Description of other erosion prevention practices to be used:

Install the specified energy dissipation method, such as riprap and geotextile fabric, at pipe outlets within 24 hours of installation. Permanently seed disturbed areas prior to end of seeding dates specified by MnDOT.

Inspectors send weekly and 2<sup>nd</sup> rainfall event Inspections records to City Inspector.

### 4. Sediment Control Practices

Description of sediment control practices to be used to minimize sediments from entering surface waters, including curb and gutter systems and storm drain inlets:

Permanent sediment control practices to be used on this project consist of sedimentation basins. Temporary sediment control practices to be used are silt fence, culvert inlet protection, storm sewer inlet protection, stone pad socks, ditch checks, and, if necessary, street sweeping.

### 5. Dewatering and Basin Draining

If the project includes dewatering or basin draining, describe the BMP's to be used to prevent the discharge from adversely affecting the receiving waters and downstream landowners.

N/A

### 6. Additional BMP's for Special Waters and Discharges to Wetlands

This project does not discharge stormwater directly to a Special Water. This project does not discharge stormwater to wetlands, except roof drain PVC.

### 7. Construction Activity Sequence

In addition to performing and sequencing the tasks associated with implementing this SWMP as described herein and shown on the plan, the Contractor shall perform construction activities in accordance with the following sequence:

- (1) Install silt fence along property line of project site where shown on plan and when property line is down gradient and within 100 feet of areas to have disturbed soil and where property line is within 20 feet of soil disturbing and other construction activities.
- (2) Install silt fence along edge of wetlands and at other locations shown on the plan.
- (3) Install stone exit pads where shown on plan and at other locations where vehicles and equipment will leave the site onto paved and gravel surfaces.
- (4) Construct storm water ponds and related piping and control structures as shown on the project plans. Install and maintain temporary erosion prevention measures as shown on the plan.
- (5) Install silt fence along bottom of storm water pond inlets and where shown on the plan.
- (6) Install trunk sanitary sewer.
- (7) Complete earthwork activities. Install and maintain sediment control measures such as ditch checks and stormwater inlet protection.
- (8) Complete utility construction. Install and maintain sediment control measures such as inlet protection as per work proceeds.
- (9) Remove silt deposits from site, remove silt deposits from stormwater basins.
- (10) Provide soil stabilization to disturbed areas by preparing topsoil, seeding, fertilizing, mulching, anchoring mulch in accordance with plans and specifications.
- (11) Remove perimeter silt fence, other silt fence, check dams, and other sediment control measures upon achieving final stabilization and Owner submits the Notice of Termination.

### 8. Inspections and Maintenance

Description of procedures to be taken to routinely inspect the construction site:

Contractor shall inspect erosion prevention and sediment control BMP's to ensure integrity and effectiveness. Repair, replace, or supplement non-functional BMP's to provide continually functional BMP's. Contractor shall inspect the entire construction site a minimum of once every seven (7) days during active construction and within 24 hours after a rainfall event greater than 0.5 inches in 24 hours. Inspectors shall include stabilized areas, erosion prevention and sediment control BMP's, and infiltration areas. Specific tasks associated with the inspection and maintenance of the BMP's include the following:

- \* Maintain and retain at the construction site written records of the inspections and maintenance performed. Records of each inspection and maintenance activity shall include: Date and time of inspection/maintenance activity; Name of person(s) Performing the activity; Finding of Inspection; Recommended corrective actions; Corrective actions taken; and Date and amount of rainfall events greater than 0.5 inches in 24 hours.
- \* Repair, replace, or supplement silt fences that become non-functional or accumulate sediment to the level of 1/3 the silt fence height or more within 24 hours of discovery or as soon as conditions allow access.
- \* Drain temporary and permanent sediment basins and remove sediment when the volume of sediment collected reaches 1/2 the permanent storage volume within 72 hours of discovery or as soon as conditions allow access.
- \* Inspect surface waters, drainage ditches, and stormwater conveyance systems for evidence of sediment deposited by erosion. Remove debris and deposited sediment and restabilize areas where sediment removal results in exposed soil within seven (7) days of discovery unless precluded by legal, regulatory, or physical constraints. Removal and stabilization shall be completed within seven (7) days of obtaining access. The NPDES/SDS permit holder is responsible for contacting the local, regional, state, and federal authorities and receiving the applicable permits prior to performing this work.
- \* Inspect construction site vehicle exit locations for evidence of sediment being tracked off-site onto paved surfaces. Remove tracked sediment from off-site paved surfaces within 24 hours of discovery.
- \* Inspect perimeter of construction site. Remove off-site accumulations of sediment in a manner and at a frequency to minimize off-site impacts.

### 9. Pollution Prevention Management Measures

Contractor shall implement the following pollution prevention management measures on the site:

- \* Solid Waste: Collect and properly dispose of sediment, asphalt and concrete millings, floating debris, paper, plastic, fabrics, construction and demolition debris, and other wastes in accordance with MPCA disposal requirements.
- \* Hazardous Materials: Properly store, provide required secondary containment, and dispose of oil products, fuels, paint products, and other hazardous substances to prevent spills, leaks, and other discharges in accordance with MPCA regulations. Provide restricted access storage areas to prevent unauthorized access and vandalism.
- \* Equipment Washing: Restrict external washing of trucks and other construction equipment to a defined area of site. Contain runoff and properly dispose of waste. Engine degreasing is prohibited on the property.
- \* Spill prevention: Park construction equipment and store potentially hazardous materials in a designated area located as far as practicable from potential environmentally sensitive areas. Construct impoundment dikes and take other measures required to contain spilled material. Remove and dispose of contaminated soil, vegetation, and other materials and perform other mitigation measures as required in accordance with MPCA regulations.
- \* Sanitary and Septic Waste: Provide and maintain temporary facilities in accordance with MPCA and Minnesota Department of Health regulations.

### 10. Final Stabilization

Contractor shall achieve final stabilization of the construction site by achieving the following:

- \* Soil disturbing activities have been completed and soils are stabilized by a uniform perennial vegetative cover with a density of 70 percent over the entire previous surface area or other equivalent means to prevent soil failure under erosive conditions.
- \* Temporary synthetic and structural erosion prevention and sediment control BMP's are removed.
- \* Sediment is removed from permanent sedimentation basins to return basins to the design capacity, removed from stormwater conveyance systems, and is stabilized or removed from the site.

### 11. Notice of Termination

Contractor shall notify Owner immediately upon achieving Final Stabilization. Owner must submit the Notice of Termination within 30 days after Final Stabilization or within 30 days of another owner assuming control according to Part II.B.5, over all areas of the site that have not undergone Final Stabilization.



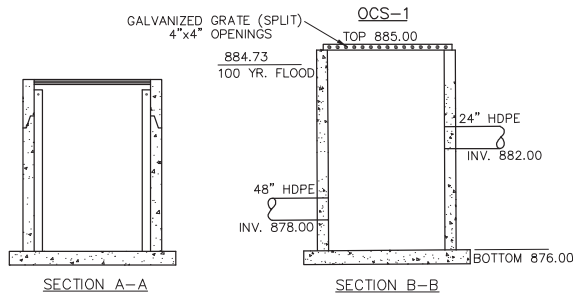
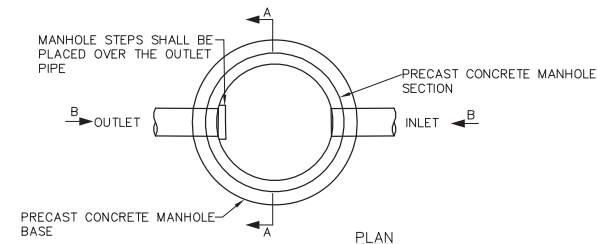
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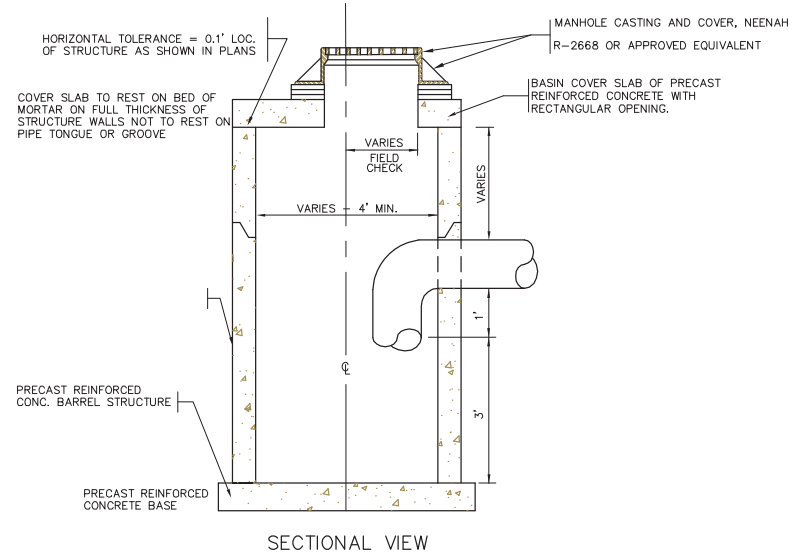
*Vladimir Surver*  
 VLADIMIR SURVER P.E., NO. 25105 DATED: 12/31/2024

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		EDS-15	VS	C10

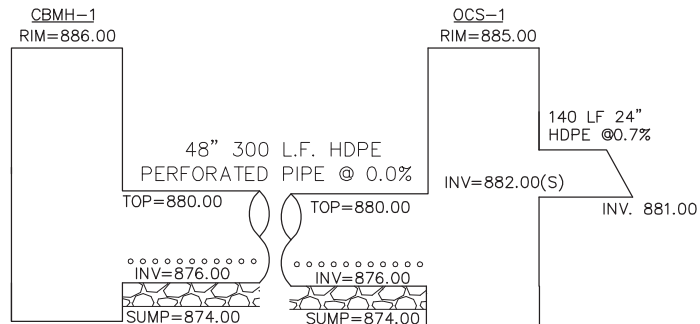
# INFILTRATION TRENCH DETAILS



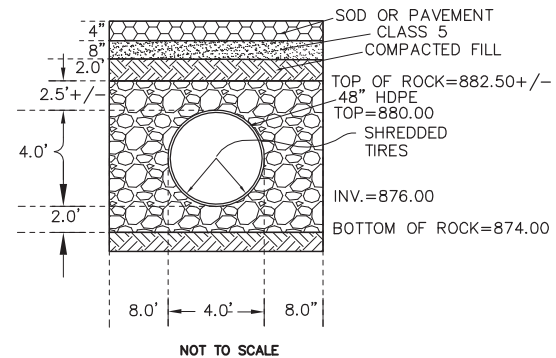
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C8 NOT TO SCALE



**2** SUMP CATCH BASIN  
C8 NOT TO SCALE



**3** INFILTRATION TRENCH PROFILE  
C8 NOT TO SCALE



**4** INFILTRATION TRENCH DETAIL  
C8



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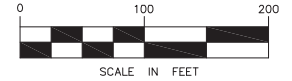
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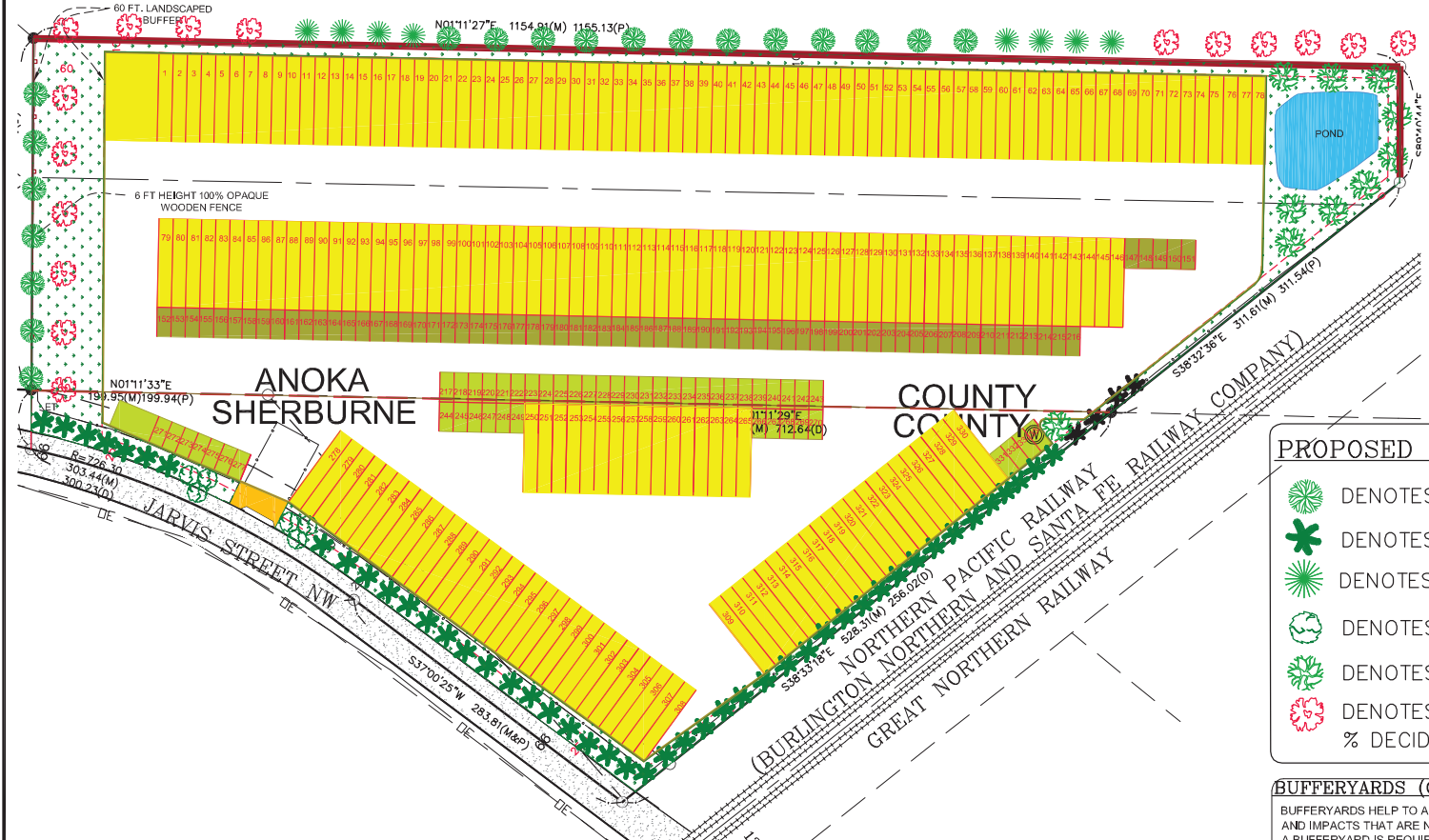
PROJECT NO.: 23-146

SHEET NO. C11

# LANDSCAPE PLAN



PROPOSED LEGEND	
	DENOTES PROPOSED BITUMINOUS SURFACE
	DENOTES PROPOSED CONCRETE SURFACE
	DENOTES PROPOSED PARKING STALLS 75X12
	DENOTES PROPOSED PARKING STALLS 25X12
	DENOTES PROPOSED CONCRETE 1 FOOT CURB
	DENOTES PROPOSED GRASS (SHALL BE SEEDS)
	DENOTES PROPOSED 1 FOOT GUTTER
	DENOTES PROPOSED POND



PROPOSED LEGEND TREES, BUSHES, SHRUBS	
	DENOTES GREEN GIANT ARBORVITAE TREE - 19
	DENOTES MEDORA JUNIPER THUJA - 46
	DENOTES BRANDON ARBORVITAE SHRUBS - 8
	DENOTES SMOOTH HYDRANGEA BUSHES - 6
	DENOTES RIVER BIRCH - 10
	DENOTES RED MAPLE - 16
	% DECIDUOUS 30%

PLANTING REQUIREMENTS (CITY OF RAMSEY)			
PLANTING TYPE	REQUIREMENTS	LF/NO. STALLS	QUANTITY
TREES (NORTH)	ONE TREE PER 50 LINEAL FEET OF SITE PERIMETER	297	6
TREES (EAST)	ONE TREE PER 50 LINEAL FEET OF SITE PERIMETER	1155	23
SHRUB (NORTH)	ONE SHRUB PER 30 LINEAL FEET OF SITE PERIMETER	297	10
SHRUB (EAST)	ONE SHRUB PER 30 LINEAL FEET OF SITE PERIMETER	1155	38
TREES	ONE TREE PER EVERY 10 PARKING SPACES	243	25
TREES TOTAL			54
SHRUB TOTAL			48
SHRUB / TREE TOTAL			102

**PARKING LOT LANDSCAPING REQUIREMENTS (CITY OF RAMSEY)**

ALL PARKING LOTS ARE REQUIRED TO PROVIDE INTERNAL AND/OR ADJACENT OVERSTORY TREE PLANTINGS IN AN EFFORT TO SHADE PARKING SURFACES AND PROVIDE VISUAL RELIEF. THE PLANTING SCHEDULE IS ESTABLISHED TO PROVIDE AN ACCEPTABLE NUMBER OF PLANTINGS THAT MAY BE PLANTED IN REGULAR SYMMETRICAL PATTERNS OR IRREGULAR CLUSTERS OR GROUPINGS. PLANTINGS ARE REQUIRED AT THE FOLLOWING MINIMUM SCHEDULE AND ARE CREDITED TOWARD THE MINIMUM PLANTING REQUIREMENTS FOR THE DISTRICT:

- ONE TREE PER EVERY TEN PARKING SPACES.
- EVERY OVERSTORY TREE PLANTING SHALL BE PROVIDED WITH A PLANTING AREA OF AT LEAST 162 SQUARE FEET (EQUIVALENT OF A NINE FOOT BY 18 FOOT PARKING STALL).
- ACCEPTABLE GROUND COVER MATERIALS INCLUDE SOD, MULCH, AND OTHER NATURAL GROUND COVER. LANDSCAPING ROCK AND PLASTIC UNDERLAYMENT IS PROHIBITED IN PARKING ISLANDS.

**BUFFERYARDS (CITY OF RAMSEY)**

BUFFERYARDS HELP TO ACHIEVE SCREENING BETWEEN DIFFERING USES WITH VARIED INTENSITIES AND IMPACTS THAT ARE NOT ALWAYS COMPLEMENTARY WHEN ADJACENT TO ONE ANOTHER. WHEN A BUFFERYARD IS REQUIRED, THE YARD SPACE AND PLANTING REQUIREMENTS ARE NOT TO BE REDUCED FOR OTHER PURPOSES SUCH AS FUTURE PARKING AND DRIVEWAYS, BUILDING EXPANSIONS, OR OTHER ACTIVITIES THAT ARE NOT IN KEEPING WITH THE PURPOSES OF BUFFERING AND SCREENING.

- BUFFERYARDS ARE INTENDED TO PROVIDE ADDITIONAL SCREENING OF BUSINESSES THAT ARE ADJACENT TO RESIDENTIAL AREAS. THE FOLLOWING TABLE DETAILS THE WIDTH OF THE BUFFERYARD ALONG THE COMMON ADJACENT PROPERTY LINE. AN ADDITIONAL INCREASE OF LANDSCAPE PLANTINGS IS REQUIRED IN THE BUFFERYARD. THE TABLE BELOW OUTLINES THE MINIMUM REQUIRED ADDITIONAL PLANTINGS, EXPRESSED AS A PERCENTAGE OF THE TOTAL REQUIRED SITE LANDSCAPING (THE PERCENTAGE IS APPLIED TO THE TOTAL NUMBER OF TREES AND SHRUBS FOR THE PROPOSED DEVELOPMENT).
- AS AN ALTERNATIVE METHOD FOR SCREENING WITHIN THE BUFFERYARD, FENCES THAT ARE 100 PERCENT OPAQUE MAY BE USED TO MITIGATE THE IMPACTS OF BUSINESSES THAT ARE ADJACENT TO RESIDENTIAL AREAS. WHEN A FENCE IS USED FOR SCREENING PURPOSES, THE BUFFERYARD PLANTING REQUIREMENTS MAY BE REDUCED BY 50 PERCENT FROM THE STATED MINIMUM REQUIREMENT.

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*Vlad Sivirven*  
 VLADIMIR SIVIRVEN P.E., NO. 25105

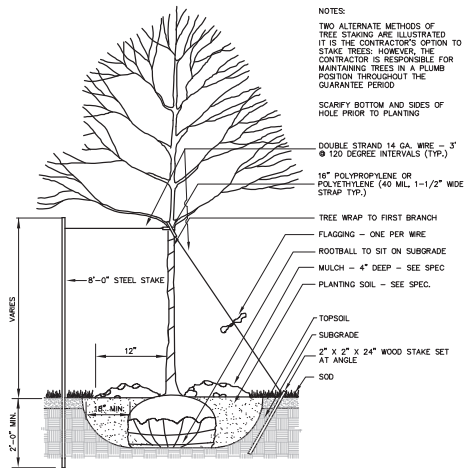
DATED: 12/31/2024

**LANDSCAPE PLAN**  
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 RAMSEY, MN 55330

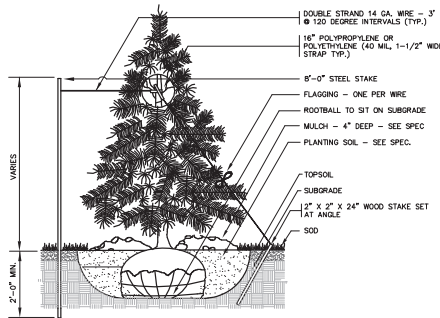
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 FIELD BOOK NO.: EDS-15

DRAWN BY: IS  
 CHECKED BY: VS  
 PROJECT NO.: 23-146  
 SHEET NO. C12

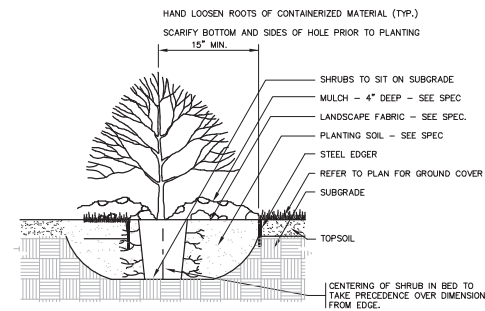
# TREE DETAILS



**DECIDUOUS TREE**  
NOT TO SCALE



**CONIFEROUS TREE**  
NOT TO SCALE

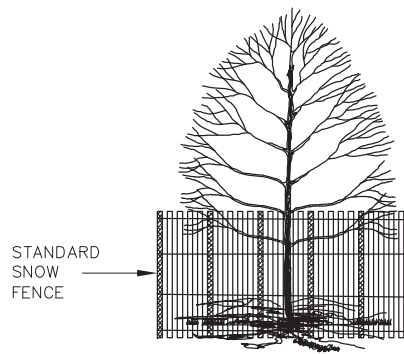


**SHRUB WITH STEEL EDGER**

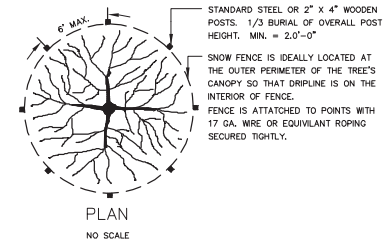
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NOTES:  
CONFER TO HAVE SHREDDED HARDWOOD MULCH UNLESS NOTED OTHERWISE. NO MULCH TO BE IN CONTACT WITH TRUNK.  
SCARIFY BOTTOM AND SIDES OF HOLE PRIOR TO PLANTING.

TWO ALTERNATE METHODS OF TREE STAKING ARE ILLUSTRATED. IT IS THE CONTRACTOR'S OPTION TO STAKE TREES. HOWEVER, THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING TREES IN A PLUMB POSITION THROUGHOUT THE GUARANTEE PERIOD.



STANDARD  
SNOW  
FENCE



PLAN  
NO SCALE

**TREE FENCING**  
NOT TO SCALE



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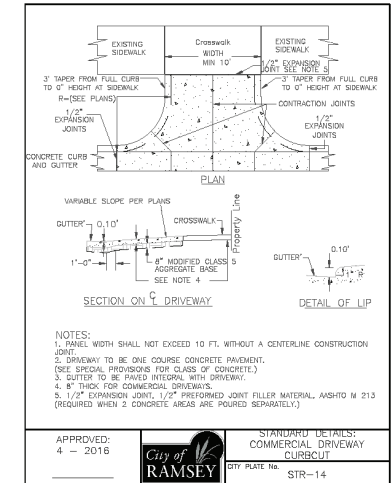
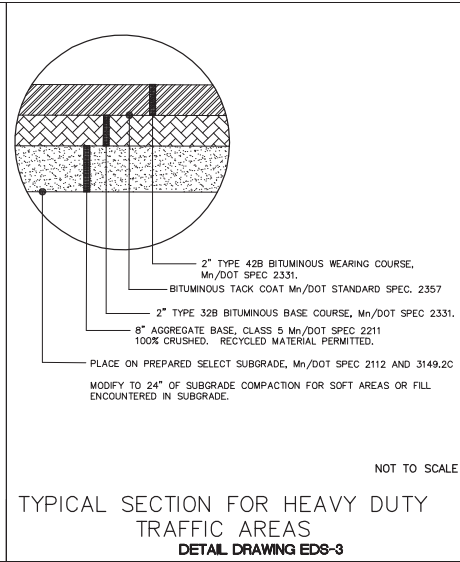
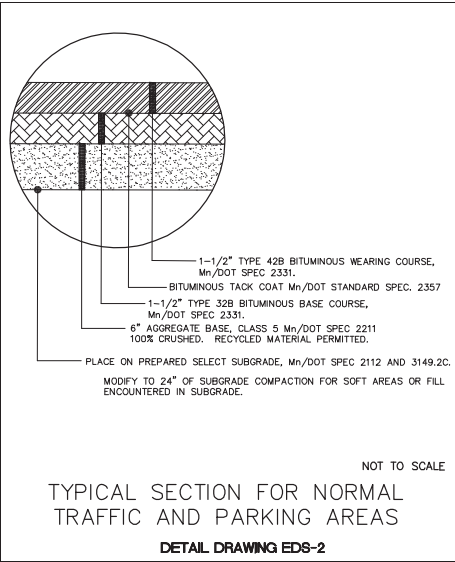
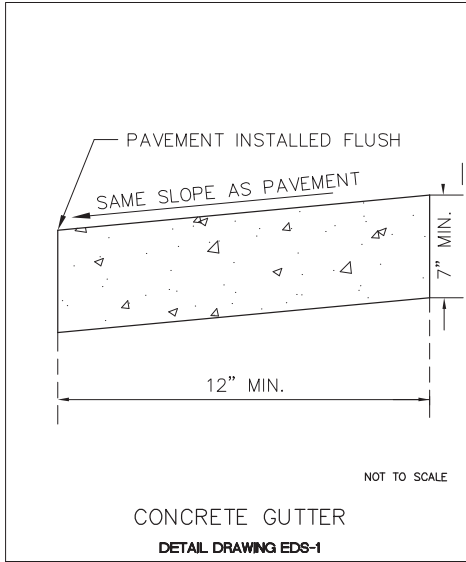
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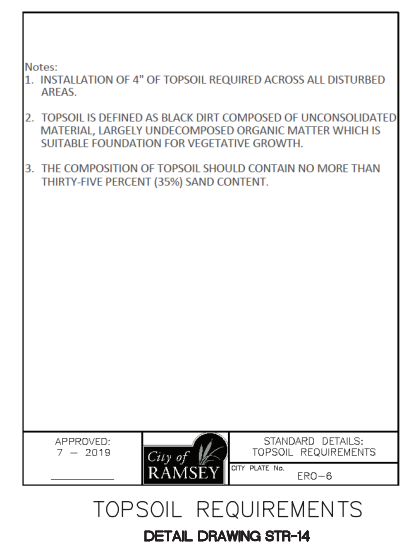
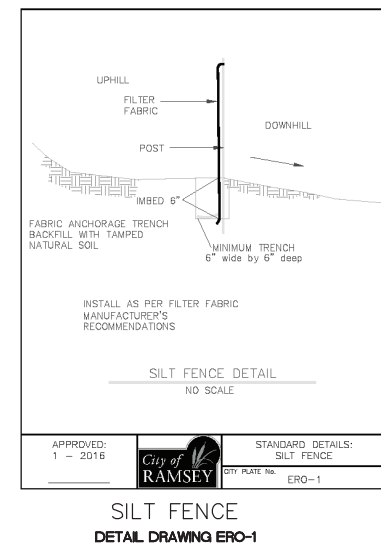
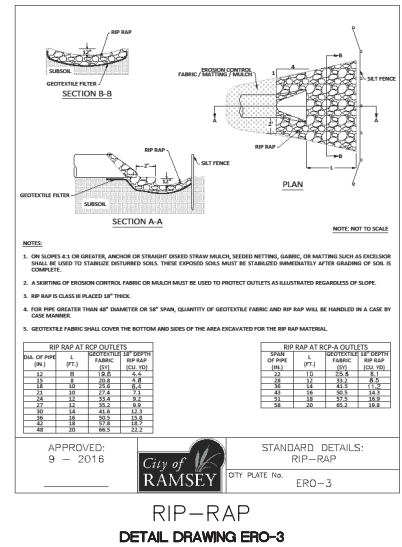
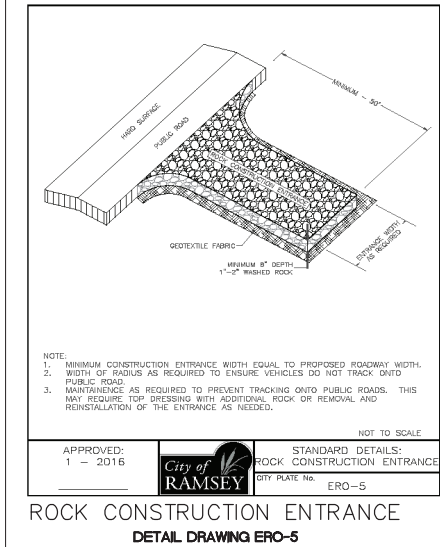
PROJECT NO.: 23-146

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# PARKING LOT AND EROSION CONTROL DETAILS



**TYPICAL COMMERCIAL DRIVEWAY CURBCUT**  
DETAIL DRAWING STR-14



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*Vlad Sivirer*  
VLADIMIR SIVIRER P.E., No. 25105

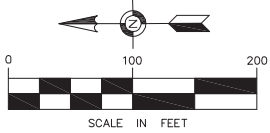
DATED: 12/31/2024

**PARKING LOT AND EROSION CONTROL DETAILS**  
LOCATION: 15861 JARVIS STREET NORTHWEST  
RAMSEY, MN 55330

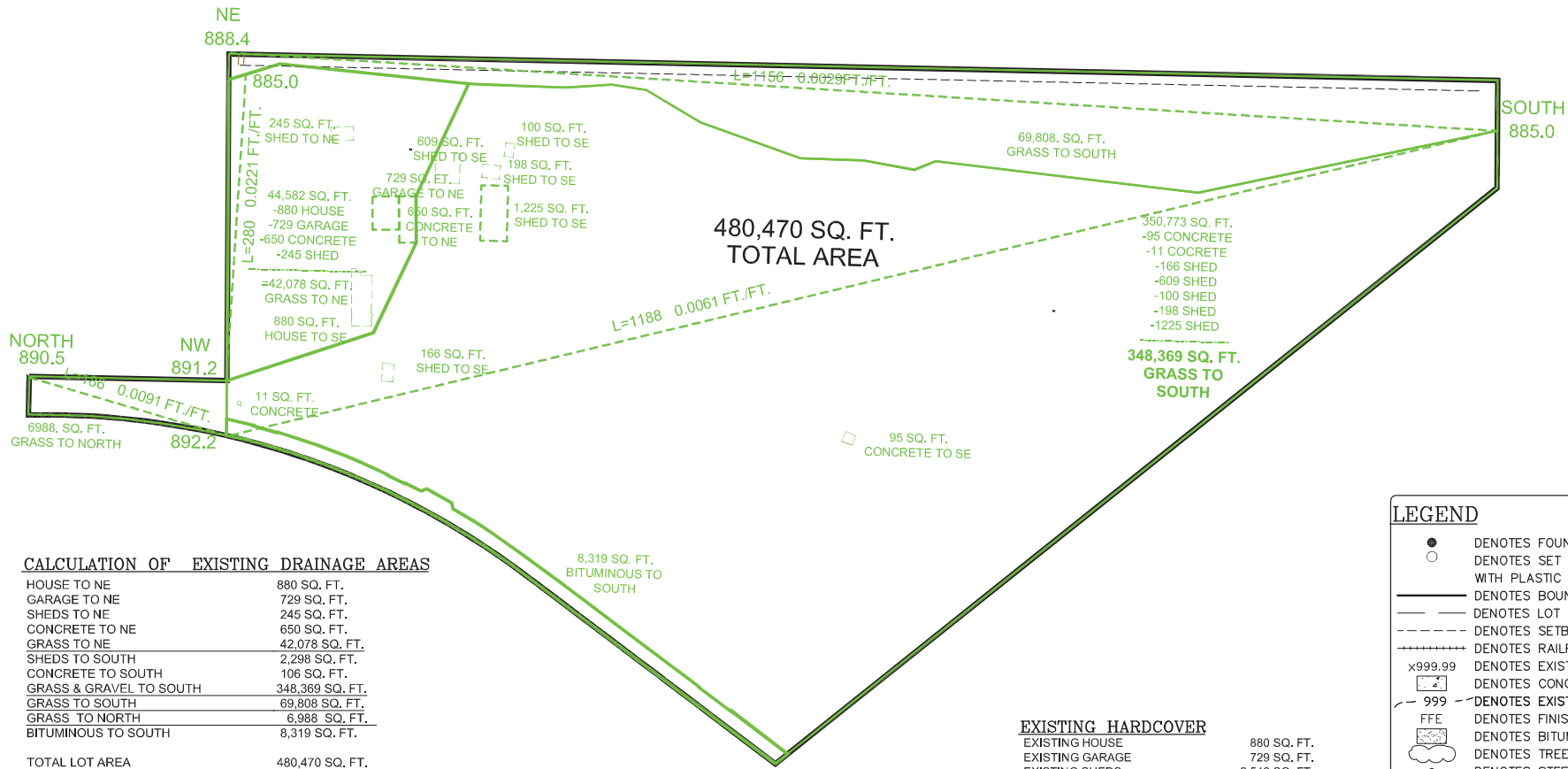
FIELD WORK DATE: 09/26/2023  
DRAWN BY: IS  
PROJECT NO.: 23-146

FIELD BOOK NO.: EDS-15  
CHECKED BY: VS  
SHEET NO. C14

# EXISTING DRAINAGE AREAS



Call 48 Hours before digging  
**GOPHER STATE ONE CALL**  
 Twin Cities Area 651-454-0002  
 MN. Toll Free 1-800-252-1166



**CALCULATION OF EXISTING DRAINAGE AREAS**

HOUSE TO NE	880 SQ. FT.
GARAGE TO NE	729 SQ. FT.
SHEDS TO NE	245 SQ. FT.
CONCRETE TO NE	650 SQ. FT.
GRASS TO NE	42,078 SQ. FT.
SHEDS TO SOUTH	2,298 SQ. FT.
CONCRETE TO SOUTH	106 SQ. FT.
GRASS & GRAVEL TO SOUTH	348,369 SQ. FT.
GRASS TO SOUTH	69,808 SQ. FT.
GRASS TO NORTH	6,988 SQ. FT.
BITUMINOUS TO SOUTH	8,319 SQ. FT.
<b>TOTAL LOT AREA</b>	<b>480,470 SQ. FT.</b>
<b>DOUBLE CHECK DRAINAGE AREA</b>	<b>480,470 SQ. FT.</b>

**EXISTING HARDCOVER**

EXISTING HOUSE	880 SQ. FT.
EXISTING GARAGE	729 SQ. FT.
EXISTING SHEDS	2,542 SQ. FT.
EXISTING CONCRETE SURFACE	1,223 SQ. FT.
EXISTING BITUMINOUS SURFACE	7,839 SQ. FT.
<b>TOTAL IMPERVIOUS AREA</b>	<b>13,213 SQ. FT.</b>
<b>TOTAL LOT AREA</b>	<b>480,470 SQ. FT.</b>
<b>EXISTING HARDCOVER</b>	<b>2.8 %</b>

**PROJECT BENCHMARK**  
 ELEVATION = 891.26 (NAVD 88)  
 MNDOT DISK "RUSTIC".

**LEGEND**

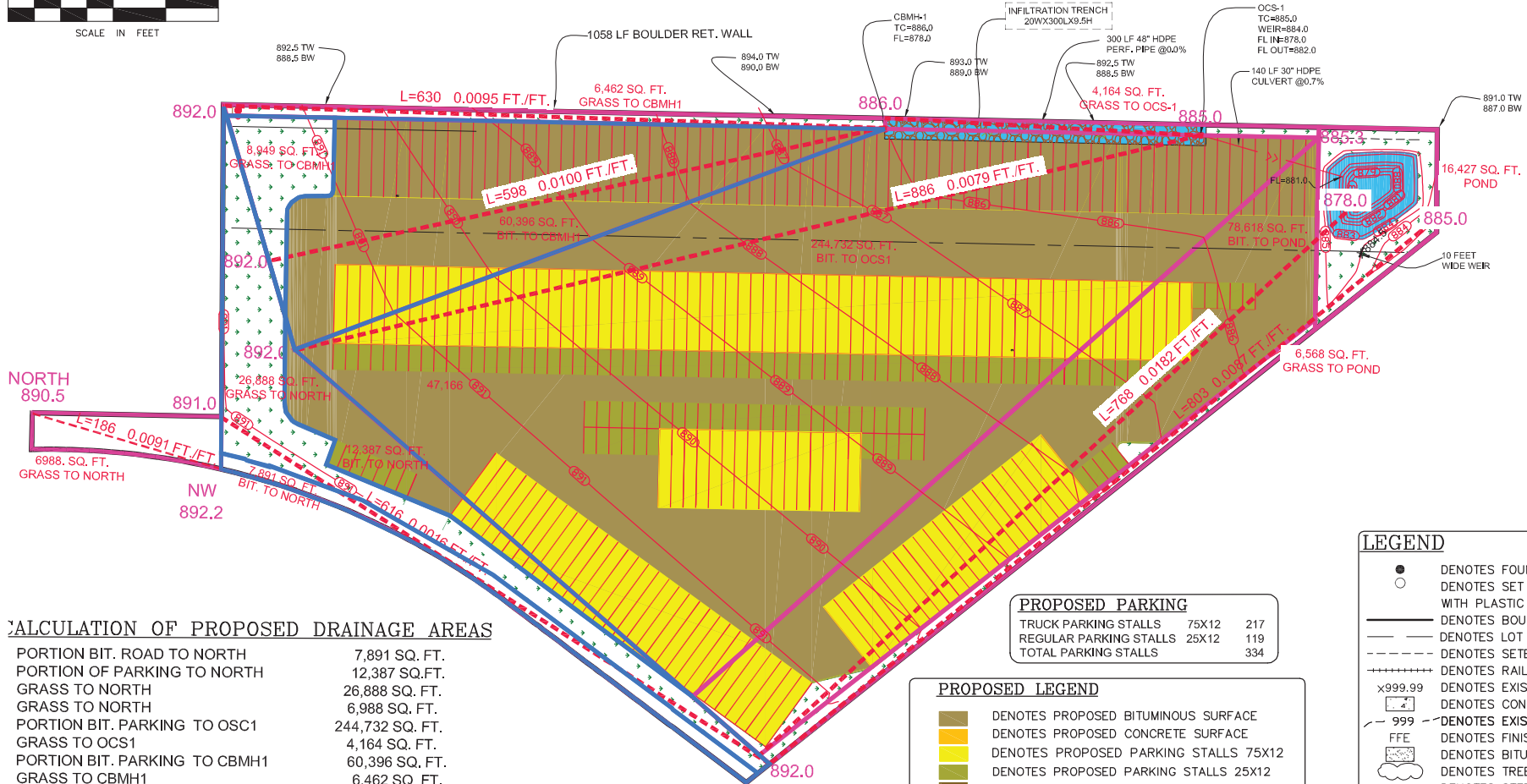
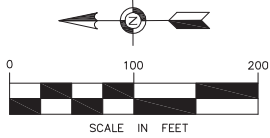
- DENOTES FOUND PROPERTY IRON
- DENOTES SET 1/2" X 18" REBAR WITH PLASTIC CAP "PLS 25105"
- DENOTES BOUNDARY LINE
- DENOTES LOT LINE
- - - DENOTES SETBACK LINE
- - - - DENOTES RAILROAD LINE
- x999.99 DENOTES EXISTING SPOT ELEVATION
- DENOTES CONCRETE SURFACE
- - - DENOTES EXISTING CONTOUR LINE
- FFE DENOTES FINISH FLOOR ELEVATION
- DENOTES BITUMINOUS SURFACE
- DENOTES TREE LINE
- DENOTES STEEL FENCE
- OE- DENOTES OVERHEAD ELECTRIC
- DENOTES ELECTRIC POWER POLE
- (M) DENOTES MEASURED DISTANCE
- (P) DENOTES PLATTED DISTANCE
- ⊙ DENOTES SANITARY MANHOLE
- ⊠ DENOTES ELECTRIC METER
- ← DENOTES DRAINAGE FLOW
- ET DENOTES ELECTRIC TRANSFORMER

**EDS ENGINEERING DESIGN & SURVEYING**  
 6480 Wayzata Blvd., Minneapolis, MN 55426  
 OFFICE: (763) 545-2800 FAX: (763) 545-2801  
 EMAIL: info@edsmn.com WEBSITE: http://edsmn.com

I HEREBY CERTIFY THAT THIS PLAN WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION, AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.  
*Vlad Sivriev*  
 VLADIMIR SIVRIEV P.E., NO. 25105 DATED: 12/13/2024

<b>EXISTING DRAINAGE AREAS</b>	FIELD WORK DATE: 09/26/2023	DRAWN BY: IS	PROJECT NO.: 23-146
LOCATION: 15861 JARVIS STREET NORTHWEST RAMSEY, MN 55330	FIELD BOOK NO.: EDS-15	CHECKED BY: VS	SHEET NO. C15

# PROPOSED DRAINAGE AREAS



## CALCULATION OF PROPOSED DRAINAGE AREAS

PORTION BIT. ROAD TO NORTH	7,891 SQ. FT.
PORTION OF PARKING TO NORTH	12,387 SQ. FT.
GRASS TO NORTH	26,888 SQ. FT.
GRASS TO NORTH	6,988 SQ. FT.
PORTION BIT. PARKING TO OSC1	244,732 SQ. FT.
GRASS TO OCS1	4,164 SQ. FT.
PORTION BIT. PARKING TO CBMH1	60,396 SQ. FT.
GRASS TO CBMH1	6,462 SQ. FT.
GRASS TO CBMH1	8,949 SQ. FT.
PORTION BIT. PARKING TO POND	78,618 SQ. FT.
GRASS TO POND	6,568 SQ. FT.
POND	16,427 SQ. FT.
DRAINAGE AREA	480,470 SQ. FT.
TOTAL LOT AREA	480,470 SQ. FT.

PROPOSED PARKING	
TRUCK PARKING STALLS 75X12	217
REGULAR PARKING STALLS 25X12	119
TOTAL PARKING STALLS	334

PROPOSED LEGEND	
	DENOTES PROPOSED BITUMINOUS SURFACE
	DENOTES PROPOSED CONCRETE SURFACE
	DENOTES PROPOSED PARKING STALLS 75X12
	DENOTES PROPOSED PARKING STALLS 25X12
	DENOTES PROPOSED CONCRETE 1 FOOT CURB
	DENOTES PROPOSED GRASS BUFFER
	DENOTES PROPOSED 1 FOOT GUTTER
	DENOTES PROPOSED TRAFFIC FLOW
	DENOTES PROPOSED RETAINING WALL
	DENOTES PROPOSED POND
	DENOTES PROPOSED CONTOUR
	DENOTES PROPOSED GRADING SPOT ELEVATION

LEGEND	
	DENOTES FOUND PROPERTY IRON
	DENOTES SET 1/2" X 18" REBAR WITH PLASTIC CAP "PLS 25105"
	DENOTES BOUNDARY LINE
	DENOTES LOT LINE
	DENOTES SETBACK LINE
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	DENOTES TREE LINE
	DENOTES STEEL FENCE
	DENOTES OVERHEAD ELECTRIC
	DENOTES ELECTRIC POWER POLE
	DENOTES MEASURED DISTANCE
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	DENOTES ELECTRIC METER
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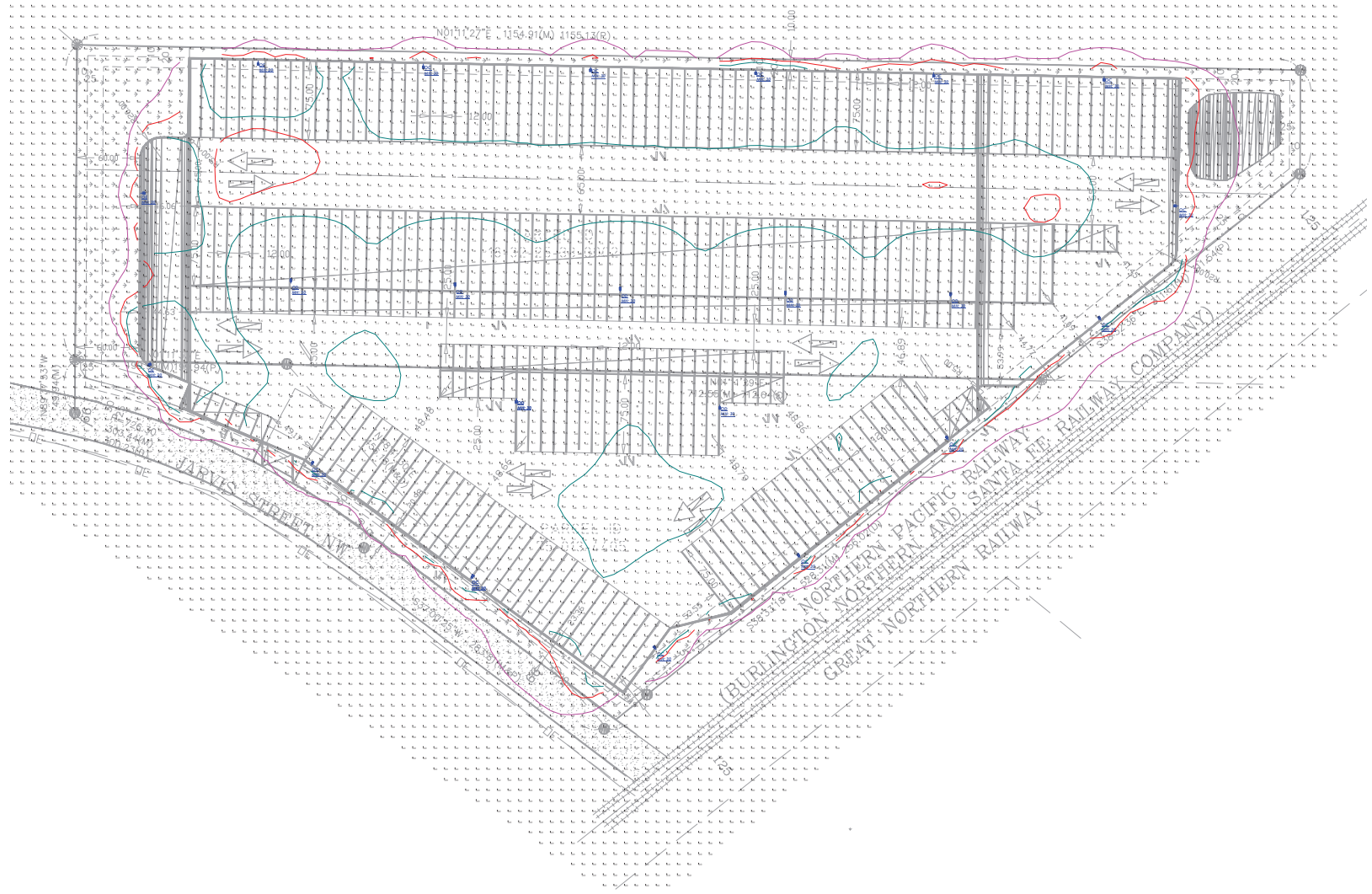
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*Vlad Sivriver*  
 VLADIMIR SIVRIVER P.E., NO. 25105 DATED: 12/13/2024

**PROPOSED DRAINAGE AREAS**  
 LOCATION: 15861 JARVIS STREET NORTHWEST  
 RAMSEY, MN 55330

FIELD WORK DATE: 09/26/2023  
 FIELD BOOK NO.: EDS-15

DRAWN BY: IS  
 CHECKED BY: VS  
 PROJECT NO.: 23-146  
 SHEET NO. C16



Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min
PARKING	Illuminance	Fc	1.42	8.3	0.2	8.10	41.50
SITE	Illuminance	Fc	0.07	3.8	0.0	N.A.	N.A.

Symbol	Qty	Label	Manufacturer	Description	Arrangement	Lum. Lumens	Lum. Watts	LLP
OC	13	OC	COOPER LIGHTING SOLUTIONS - LUMARK (FORMERLY EATON)	PRV-XL-PA48-740-U-T4W-HSS (28' POLE WITH 2' BASE)	Single	28286	303	0.900
OD	8	OD	COOPER LIGHTING SOLUTIONS - LUMARK (FORMERLY EATON)	PRV-XL-PA48-740-U-5WQ (30' POLE WITH 2' BASE)	Single	40868	303	0.900
OE	3	OE	COOPER LIGHTING SOLUTIONS - LUMARK (FORMERLY EATON)	PRV-XL-PA3A-740-U-T4W-HSS (18' POLE WITH 2' BASE)	Single	17617	172	0.900

- Standard Reflectance of 80/50/20 unless noted otherwise
- Not a Construction Document, for Design purposes only
- Standard indoor calc points @ 30' A.F.F. unless noted otherwise
- Standard outdoor calc points @ Grade unless noted otherwise
- Signs calc points @ 0' A.F.F.
- Mlazar Associates assumes no responsibility for installed light levels due to field conditions, etc.

#	Date	Comments

Revisions

RIMA Project #: 144682  
 Drawn By: EP  
 Date: 12/13/2024  
 Scale: 1" = 40'

# Northstar Truck & RV Parking

Staff photos taken January 14, 2025



Photo 1. The site is behind the fence. Residential property is wooded.



Photo 2. The entryway off Jarvis St.



Photo 3. Looking northward from the entryway highlighting the fence.



Photo 4. Inside looking east at the berm on the Links at Northfork property.



Photo 5. Looking southeast at the berm.



Phot 6. Trailers and miscellaneous items.



Photo 7. Looking southeast with the railroad on the right.



Photo 8. The middle of the site with trucks, cars, and junk vehicles.



Photo 9. Looking north at the adjacent residential property

City of Elk River Ordinance \_\_\_\_\_  
City of Ramsey Ordinance #25-04

The Cities of Ramsey and Elk River hereby ordain:

WHEREAS, Northstar Truck and RV Parking LLC owns land in the cities of Ramsey and Elk River described as:

Parcel 1 (Ramsey):

*That part of the South 1155 feet of the West 297 feet of the Southwest Quarter of the Southwest Quarter of Section 18, Township 32, Range 25, Anoka County, Minnesota, lying North of the railroad right-of-way, excluding road, subject to easement of record.*

Parcel 2 (Elk River):

*That part of the Southeast Quarter of the Southeast Quarter of Section 13, Township 32, Range 26, Sherburne County, Minnesota, described as follows:  
Beginning on the East line or said Section, 2.56 chains North or the Southeast corner thereof and at a point where said line is crossed by the Easterly line of the right-of-way of the St. Paul, Minneapolis & Manitoba Railway Company; thence North on section line 11.39 chains to center of road from State Road to Quarter post in said section line; thence South 47-1/2 degrees West to said Easterly line of said right-or-way; thence South 30 degrees East along said right-of-way to beginning.*

Collectively, “Subject Properties;” and

WHEREAS, the Subject Properties cross the municipal and county borders; and

WHEREAS, the Subject Properties are guided Industrial in the Elk River 2040 Comprehensive Plan and Business Park in the Ramsey 2040 Comprehensive Plan;

WHEREAS, the property owner’s business is consistent with each city’s Comprehensive Plans, yet inconsistent with the current zoning classifications; and

WHEREAS, both Cities agree that it is in the best interest to have common and consistent land use regulations for the Subject Properties; and

WHEREAS, both Cities jointly adopt this ordinance in conformance with each City’s Charter and other municipal regulations.

**Section 1. Authority.**

This ordinance is adopted pursuant to and under the authority of the City Charter of the City of Ramsey and the City Charter of the City of Elk River.

**Section 2. Zoning Map Amendment.**

Pursuant to Article VII – Planned Unit Developments of Ramsey City Code and Section 30-1564 of Elk River City Code, the Subject Properties are hereby rezoned from I-1 Light Industrial (Ramsey) and I3 General Industrial (Elk River) to PUD Planned Unit Development:

### **Section 3. Uses within the Planned Unit Development**

- 3.1 The use of the property shall only be in conformance with uses described within this PUD.
- 3.2 Use of the property is a business use for parking of the following:
  - a. Semi-trucks, semi-trailers, and other delivery trucks;
  - b. Recreational vehicles, campers, motorhomes, and fifth-wheel trailers;
  - c. Boats; and
  - d. Personal vehicles related to a vehicle in a., b., or c. above.
- 3.3 The use of this property may not be used for any vehicles owned by a business holding a state dealer's license or any inoperable or unlicensed vehicle,
- 3.4 Outdoor storage of any products, equipment, materials, supplies, or debris is not permitted, except for snow removal equipment and supplies for the site in conformance with the applicable municipality's regulations.

### **Section 4. Site Development Standards**

- 4.1 The site plan, grading and drainage plans, and landscape plan shall conform to the plan set dated [REDACTED] as prepared by Engineering Design & Surveying (EDS) and approved by each municipality's City Engineer for construction.
- 4.2 All subsequent modifications to the site improvements must be in conformance with each municipality's regulations.

### **Section 5. Site Performance Standards**

- 5.1 All vehicles must be properly licensed.
- 5.2 In the event a gate is installed, access to the property must include key boxes for both Elk River and Ramsey Fire Departments.
- 5.3 A concrete stripe must be maintained along the city/county border in order to clarify any relevant jurisdictional issues.

### **Section 6. Memorialization.**

Ramsey City Code Section 106-739 is amended to add "Northstar Truck and RV Parking" and this ordinance number to the list of approved Planned Units of Record.

### **Section 7. Summary**

The following official summary of Ordinance #25-04 has been approved by the City Council of the City of Ramsey as clearly informing the public of the intent and effect of the Ordinance:

“Ordinance #25-04 rezones property from I-1 within the City of Ramsey and I3 within the City of Elk River to PUD and sets forth standards on the uses and site improvements thereon. The property is addressed 15861 Jarvis St. NW with Anoka County Parcel ID of 18-32-25-33-0002 and Sherburne County Parcel ID of 75-00013-4405.”

PASSED by the City Council of the City of Ramsey, Minnesota, the \_\_\_\_ day of February, 2025.

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk

PASSED by the City Council of the City of Elk River, Minnesota, the \_\_\_\_ day of February, 2025.

\_\_\_\_\_  
Mayor

ATTEST:

\_\_\_\_\_  
City Clerk

Introduction date:  
Posting dates:  
Adoption date:  
Publication date:  
Effective date:

Councilmember \_\_\_\_\_ introduced the following resolution and moved for its adoption:

**RESOLUTION #25-028**

**A RESOLUTION APPROVING A SITE PLAN REVIEW FOR NORTHSTAR TRUCK AND RV PARKING AT 15861 JARVIS STREET NORTHWEST**

**WHEREAS**, The City of Ramsey received an application from Northstar Truck and RV Parking (the “**Applicant**”) requesting a Planned Unit Development with a Site Plan Review approval on the property legally described as follows:

*That part of the South 1155 feet of the West 297 feet of the Southwest Quarter of the Southwest Quarter of Section 18, Township 32, Range 25, Anoka County, Minnesota, lying North of the railroad right-of-way, excluding road, subject to easement of record.*

(the “**Subject Property**”).

**WHEREAS**, the **Subject Property** is zoned PUD, Planned Unit Development, which is a specialized zoning designation, and guided for business park uses in the 2040 Comprehensive Plan; and

**WHEREAS**, all principal uses within the PUD zoning district require a formal Site Plan Review from Planning Commission and City Council; and

**WHEREAS**, the **Applicant** appeared before the Planning Commission for a public hearing on January 23, 2025, that said public hearing was properly advertised; and

**WHEREAS**, the Planning Commission reviewed the Site Plan at their regular meeting on January 23, 2025, and recommended [REDACTED]; and

**WHEREAS**, the City Council reviewed the request at their meeting on February 11, 2025.

**NOW THEREFORE, BE IT RESOLVED BY THIS CITY COUNCIL OF THE CITY OF RAMSEY, ANOKA COUNTY, STATE OF MINNESOTA, as follows:**

That the Ramsey City Council hereby grants approval of the Site Plan for Northstar Truck and RV Parking on the **Subject Property** in accordance with relevant City Codes, subject to the following conditions:

1. Final legal form approved by the City Attorney.
2. That the **Applicant** enters into a Development Agreement with the City and provide the necessary sureties guaranteeing the work will be done according to plan and in a timely manner.
3. That the **Applicant** shall update plans based on Staff comments as noted in ProjectDox.

4. That the **Applicant** shall secure all other necessary permits from other applicable agencies, including the City of Elk River and the Lower Rum River WMO, and provide a copy of said permits to the City.
5. If a gate is to be added from the Jarvis Street NW entrance, fire department lock boxes for both the Ramsey and Elk River Fire Departments are required.
6. Landscaping on the adjacent golf course property must receive written permission.
7. All site improvements must be made according to plans dated \_\_\_\_ prepared by EDS and approved by the City Engineer.
8. All site improvements noted on said plans must be completed within one year of the date of approval of this resolution.
9. This Site Plan Review resolution is contingent upon rezoning of the Subject Property from I-1 to PUD.



# Request for Action

**To**  
Planning Commission

**Item Number**  
4.2

**Meeting Date**  
February 25, 2025

**Prepared By**  
Chris Leeseberg, Senior Planner

**Item Description**  
Conditional Use Permit Amendment: Outdoor Storage, 12777 Meadowvale Rd. NW, Elk River WinLectric

**Reviewed by**  
Zack Carlton

## Action Requested

Recommend, by motion, approval of an amendment to Conditional Use Permit (CU 22-11) extending the conditions deadline until July 1, 2025, and add a condition that all areas utilized for outdoor storage be completely screened from view from all public rights-of-way.

## Background/Discussion

The subject parcel was granted a Conditional Use Permit (CUP) in 2022 yet has to be recorded. One of the conditions of approval required the removal of three temporary outdoor storage structures from the property within two years of City Council approval of the CUP (by July 18, 2024). One of these structures was removed. The applicant has indicated a continued need for these structures while securing nearby indoor storage and is requesting an extension to the timeline for their removal until July 1, 2025.

Additionally, another condition of approval with the original CUP stipulated that all outdoor storage must occur within fenced areas. Currently, the area behind the building continues to be used for outdoor storage. If this area is to remain in use for outdoor storage, it must be screened to be in compliance with Section 30-807, which mandates that outdoor storage areas be completely screened from view from all public rights-of-way.

Staff recommends the CUP (CU 22-11) conditions deadline be extended to July 1, 2025, and add a condition that all areas utilized for outdoor storage be completely screened from view from all public rights-of-way.

## Financial Impact

None

## Mission/Policy/Goal

Appropriately govern in an ever-changing environment.

## Attachments

- CU 24-22 Location Map
- The Elk River Vision

*A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity*



2. Aerials
3. CU 22-11 CC Action Letter



110




# Project Location Map

WinLectric

Conditional Use Permit

Case No: CU 24-22



75-00433-0180

1

18936 YORK  
ST NW

12777 MEADOWVALE  
RD NW

12767 MEADOWVALE  
RD NW

1970 10TH  
ST NW

8 10TH  
ST NW

1978 10TH  
ST NW

12767 MEADOWVALE  
RD NW

12777 MEADOWVALE  
RD NW

18936 YORK  
ST NW



13065 Orono Parkway  
Elk River, MN 55330

August 8, 2022

Troy Tramm  
12777 Meadowvale Road  
Elk River, MN 55330

Via email: [Tetamm@winlectric.com](mailto:Tetamm@winlectric.com)

Wholesalers Property CO LLC – Ward Allen  
3110 Kettering Blvd  
Kettering, OH 45439

RE: City Council Action for Case No. CU 22-11

To Whom it May Concern:

On July 18, 2022, the City Council reviewed your application for a Conditional Use Permit to allow outdoor storage at 12777 Meadowvale Road, and approved the request with the following conditions:

1. The property shall comply with Section 30-807.
2. Fencing, as outlined in Section 30-807, shall be installed in the location as shown on the site plan dated July 18, 2022.
3. The fence shall be installed by June 1, 2023.
4. The parking lot shall be striped by September 1, 2022.
5. All outdoor storage must occur in fenced in areas or within the three temporary structures.
6. No product can be stored on top of the three temporary structures or higher than the top of the fence.
7. The temporary structure located in the northwest corner shall be relocated next to the other two temporary storage structures by September 1, 2022.
8. The three temporary outdoor storage structures shall be removed from the property two years after City Council approval of the CUP (July 18, 2024).
9. An amendment to this Conditional Use Permit will be required for any changes to the site plan dated July 18, 2022.

If you have any questions regarding the meeting or the conditions listed above, please let me know. Once the above conditions have been met, the permit will be recorded at Sherburne County and a copy sent to you.

Sincerely,

Chris Leeseberg, Senior Planner  
Ph: 763.635.1033  
[cleeseberg@ElkRiverMN.gov](mailto:cleeseberg@ElkRiverMN.gov)

Phone: 763.635.1000

[www.ElkRiverMN.gov](http://www.ElkRiverMN.gov)





# Request for Action

**To**  
Planning Commission

**Item Number**  
4.3

**Meeting Date**  
February 25, 2025

**Prepared By**  
Zack Carlton, Community Development Director

**Item Description**  
Preliminary Plat and Ordinance Amendment (PUD):  
The Villas at Fillmore Third Addition, PID 75-00931-  
0030, Riverside Development

**Reviewed by**  
Chris Leeseberg

## Action Requested

Recommend, by motion, approval of an ordinance amendment codifying and updating the PUD for The Villas at Fillmore.

Recommend, by motion, approval of the preliminary plat for The Villas at Fillmore Third Addition, with the following Conditions:

1. Council approval of the ordinance amendment approving the PUD changes.
2. A Water Availability Charge for each lot must be paid before connection to the municipal water system.
3. A tee will need to be installed for each connection to the existing HDPE water main; tapping HDPE pipe will not be permitted.
4. Compliance with the City of Elk River Engineering Design Standards is mandatory for all planning and construction activities.
5. The development agreement must include a prorated cost share for the water main extension that was installed along Fillmore Street in 2022 and was paid for by ERMU in the amount of \$32,725.
6. The sanitary sewer will be privately owned/maintained.
7. The storm pipe will be privately owned/maintained as it captures only storm water from the private street.
8. Extend sewer and water stubs to easement line.
9. All retaining walls will be privately owned and maintained by the property owners or HOA, this must be noted on the plans.
10. The developer will need to install one to two pipe crossings under the private road. ERMU will provide the pipe and location of crossings when ready.
11. Electrical service for the lift station is considered commercial. The developer will need to provide a secondary electric service.

## Background/Discussion

The applicant is proposing to complete the final phase of The Villas at Fillmore. This phase includes six single-family lots located on a private drive. The site has limited development potential due to existing wetlands, storm water features, sanitary sewer limitations, and a large overhead powerline. These factors contributed to the developers decision to construct the project with a private street.

### The Elk River Vision

*A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity*



## **Site Plan**

The project includes one private cul-de-sac providing access to all six parcels, which are generally 60-feet wide. Parking is provided on the driveway and with six parking stalls perpendicular to the private street. The homes will be a minimum of 10-feet apart and maintenance will be addressed with an HOA.

## **Public Utilities**

All six homes will be connected to city sewer and water. However, the sewer service requires a private lift station to convey the effluent to the city's system. The city will not maintain this lift station, and the development will connect to the public system near the intersection of 177th Ave and Fillmore Street.

The water main is located beneath the trail on the west side of Fillmore Street and the developer will need to make two connections to provide the required loop of service. The water main providing service to this phase was installed by the developer with the first phase of this subdivision, but ERMU reimbursed the developer as the extension was for a looped service at the time. ERMU has requested reimbursement for this cost as the extension is now providing service for this phase. These types of utility services are generally installed at the developer's expense.

## **Ordinance Amendment**

The proposed ordinance amendment will take the place of a PUD agreement. The standards of the previous PUD agreement will be codified into city ordinances, providing more clarity and transparency for the public. The only changes proposed are for the inclusion of the third phase and memorializing the private street and utility items.

## **Preliminary Plat**

City ordinance section 30-375 outlines the required findings for approval of a Preliminary Plat.

1. The proposed subdivision is consistent with the zoning regulations (article VI of this chapter) and conforms in all respects with all requirements of this Code, including the zoning regulations and this article. The property was rezoned to Planned Unit Development with the first phases of this subdivision. With the PUD designation, the development will follow their own development standards, as approved in the ordinance amendment for the PUD
2. The proposed subdivision is consistent with all applicable general and specialized city, county, and regional plans, including, but not limited to, the city's comprehensive development plan. As the property was rezoned to PUD and is guided for residential uses by the Comprehensive Plan, the subdivision is consistent with all applicable plans.
3. The physical characteristics of the site, including but not limited to topography, soils, vegetation, susceptibility to erosion and siltation, susceptibility to flooding, and drainage are suitable for the type and density of development and uses contemplated.  
The physical characteristics of the site are suitable for the type and density of development and uses contemplated.
4. The proposed subdivision makes adequate provision for water supply, storm drainage, sewage transportation, erosion control and all other services, facilities and improvements otherwise required in this article.  
The subdivision will include infrastructure to meet city requirements. The sanitary service and required lift station will be privately owned and maintained, but will become public after making the connection to existing services. The water service also requires reimbursement for previous work paid for by ERMU, but that now serves the current development phase. Finally, additional permits required to comply with erosion control standards are required and will be secured prior to building permit issuance. These concerns have been included as conditions of approval.

5. The proposed subdivision will not cause substantial environmental damage. Although there will be site grading, the subdivision will not cause substantial environmental damage.
6. The proposed subdivision will not conflict with easements of record or with easements established by judgment of a court. The proposed subdivision will not conflict with easements of record or with easements established by judgment of a court.
7. The proposed subdivision will not have an undue and adverse impact on the reasonable development of neighboring land. The only undeveloped property is one similarity size and shaped directly to the west of the subject parcel. The proposed subdivision will not impact reasonable development of neighboring land.
8. The proposed subdivision is not premature. A subdivision is premature if any of the following exists:
- a. Lack of adequate stormwater drainage.
  - b. Lack of adequate roads.
  - c. Lack of adequate sanitary sewer systems.
  - d. Lack of adequate off-site public improvements or support systems.
- The subdivision is not premature as the above conditions have been provided for.

**Financial Impact**

None

**Mission/Policy/Goal**

Support the growth and development of the community.  
Responsibly grow.  
Opportunity to live, work, and play.

**Attachments**

- 1. Location Map
- 2. Preliminary Plat
- 3. Civil Plans



# Project Location Map

Riverside Development

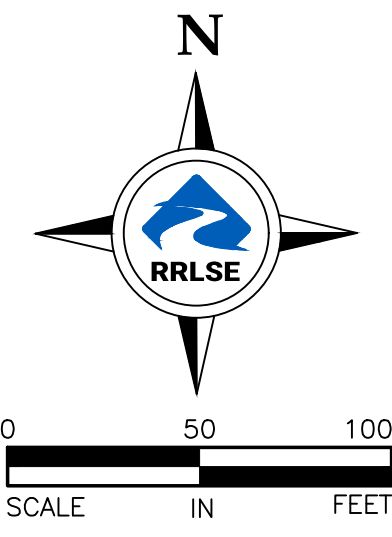
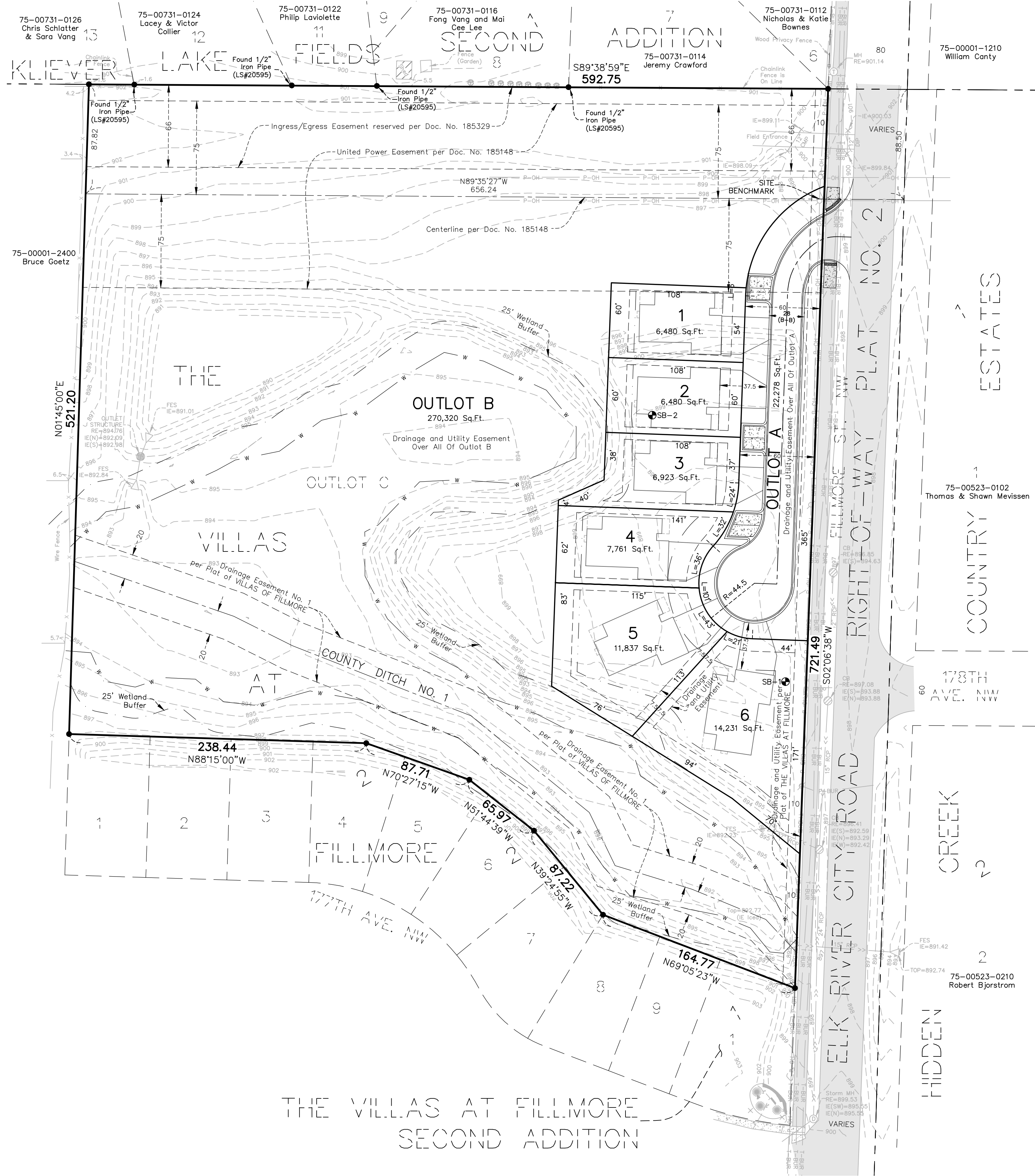
Preliminary Plat

Final Plat

Ordinance Amendment

Case No: P 25-01, P 25-02, & OA 25-01



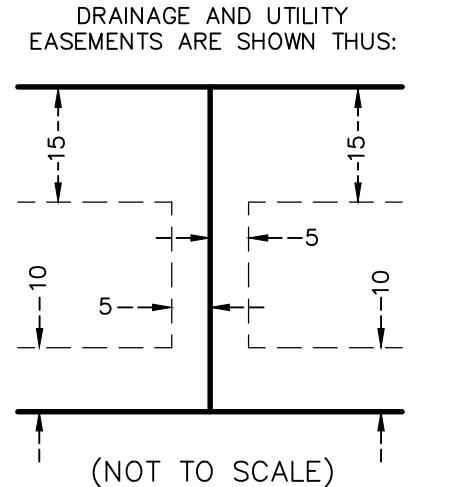


DESCRIPTION OF PROPERTY TO BE PLATTED:  
(PID NO. 75-00931-0030)

Outlot C, THE VILLAS AT FILLMORE, Sherburne County, Minnesota.

NOTES:

- The professional surveyor has made no investigation or independent search for easements of record, encumbrance, restrictive covenants, ownership title evidence, or any other facts that an accurate and current title search may disclose.
- The underground utilities shown are depicted per observed evidence. The surveyor makes no guarantee that the underground utilities shown comprise all such utilities in the area, either in service or abandoned. The surveyor further does not warrant that the underground utilities shown are in the exact location indicated although he does certify that they are located as accurately as possible from information available. The surveyor has not physically located the underground utilities. Prior to any excavation, contact Gopher State One Call for an on-site location (651-454-0002). The subsurface utility information on this survey is utility quality level D. This quality level was determined according to the guidelines of CI/ASCE 38-2, entitled "Standard Guidelines for the Collection and Depiction of Existing Subsurface Utility Data."
- Project Benchmark: MnDOT Monument No. POWER MNDZ AZ MK. Elevation = 902.23 feet (NGVD 1929)  
Site Benchmark: Railroad Spike in East Face of Power Pole on the West side of Fillmore Street NW, 400 feet North of the intersection with 178th Ave. NW. Elevation = 899.85 feet (NGVD 1929)

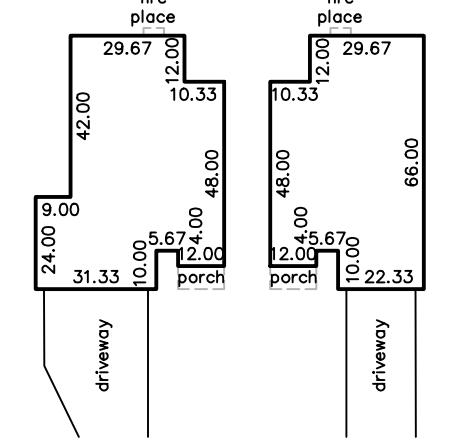


(NOT TO SCALE)  
Being 10.00 feet in width and adjoining left right of way lines unless otherwise shown on this plot and being 5.00 feet in width and adjoining all side lot lines unless otherwise shown on this plot and being 15.00 feet in width and adjoining all rear lot lines unless otherwise shown on this plot.

LEGEND

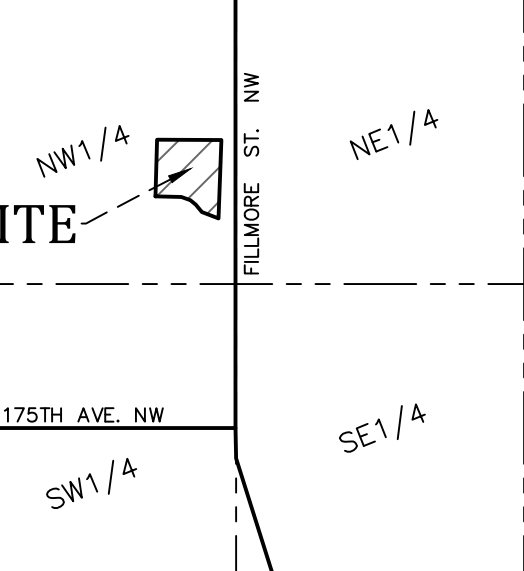
- = DENOTES FOUND 1/2" X 14" IRON PIPE WITH PLASTIC CAP MARKED "RLS 49138" UNLESS OTHERWISE NOTED
- 980 = DENOTES EXISTING CONTOUR (1 FOOT INTERVAL)
- - - = DENOTES BUILDING SETBACK LINE
- - - = DENOTES 25' WETLAND BUFFER
- - - = DENOTES EDGE OF DELINEATED WETLAND PER PLAT OF THE VILLAS AT FILLMORE
- SB-1 = DENOTES SOIL BORING BY OTHERS
- - - = DENOTES SANITARY SEWER LINE
- - - = DENOTES CATCHBASIN
- - - = DENOTES FES
- - - = DENOTES STORM SEWER LINE
- - - = DENOTES UTILITY POLE
- - - = DENOTES OVERHEAD UTILITY LINES
- - - = DENOTES BURIED TELECOMMUNICATIONS
- - - = DENOTES TELECOMMUNICATIONS MANHOLE
- ⊙ = DENOTES SIGN
- ⊙ = DENOTES DECIDUOUS TREE (DIAMETER)
- ⊙ = DENOTES CONIFEROUS TREE (DIAMETER)
- ⊙ = SHRUB
- = BITUMINOUS SURFACE
- = CONCRETE SURFACE

TYPICAL HOUSE



VICINITY MAP

SEC. 1, T. 32, R. 26



Municipality:	City of Elk River	Proposed Building Setbacks:	Front yard: 37.5 feet (From Street Curb)
Existing Zoning:	R1c - Single Family Residential	Side yard: 25 feet (Street)	Rear yard: 20 feet
Proposed Zoning:	Planned Unit Development	Side yard: 5 feet (Interior)	
Road Mileage:	300 LF or 0.06 Miles (Private)	Lot Summary: 6 - Single Family Residential Lots	2 - Outlots
Proposed Utilities:	Sewer: Municipal Water: Municipal	Owner/Subdivider:	Riverside Development Company of Princeton 19230 Evans St. NW Suite 115 Elk River, MN 55330 Marly Glines - (218) 360-4188
Street Lighting:	As Required	Surveyor:	Rum River Land Surveyors & Engineers PO Box 1044 - 3601 Thurston Ave. Princeton, MN 55371 (763) 427-5860 brian@raa-inc.com
Plot Area:	Total Area: 346,310 Sq.Ft. = 7.950 Acres Proposed ROW: 0 Sq.Ft. = 0.00 Acres Park Area: 0 Sq.Ft. = 0.00 Acres		

DATE	REVISION
Jan 22, 2025 - 3:19pm	
K:\cod_surr\Land Desktop 2008\2135.36 VILLAS 3rd\dwg\2135.36 PRE PLAT.dwg	

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Land Surveyor under the laws of the State of Minnesota.

*Brian Person*  
Brian Person, PLS  
Date 01/21/2025 Lic. No. 49138

DESIGNED BY: SMM  
DRAWN BY: JJ  
CHECKED BY: BP

DATE: 01/21/2025  
FILE NO: P-2135.36



THE VILLAS AT FILLMORE  
3RD ADDITION

PRELIMINARY PLAT  
FOR  
RIVERSIDE DEVELOPMENT COMPANY OF PRINCETON  
SHERBURNE COUNTY, MINNESOTA

SHEET 1 OF 1 SHEETS

# THE VILLAS AT FILLMORE THIRD ADDITION

CITY OF ELK RIVER, MINNESOTA



THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-2, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."

## GOVERNING SPECIFICATIONS

THE 2020 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" SHALL APPLY.

THE 2023 EDITION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) STANDARD SPECIFICATIONS SHALL APPLY.

ALL FEDERAL, STATE AND LOCAL LAWS, REGULATIONS, AND ORDINANCES SHALL BE COMPLIED WITH IN THE CONSTRUCTION OF THIS PROJECT.

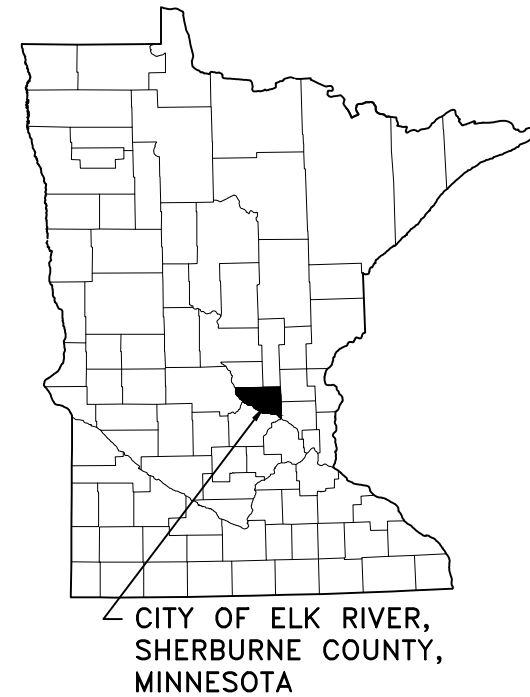
ALL TRAFFIC CONTROL DEVICES AND SIGNING SHALL CONFORM TO THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, INCLUDING THE LATEST FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS.

ALL REQUIREMENTS OF THE PROJECT MANUAL FOR THE THE VILLAS AT FILLMORE THIRD ADDITION PROJECT.

## SHEET INDEX

THIS PLAN CONTAINS 20 SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	CONSTRUCTION NOTES AND PROJECT LEGEND
3-6	DETAILS
7	TYPICAL LOT SECTIONS, SOIL NOTES, AND DETAILS
8	EXISTING CONDITIONS AND REMOVALS PLAN
9	GRADING AND EROSION CONTROL PLAN
10	WATERMAIN AND SANITARY SEWER PLAN
11	STREET AND STORM SEWER PLAN
12	RESTORATION PLAN
13	PEDESTRIAN RAMP CONSTRUCTION PLAN
14	FORCEMAIN CONSTRUCTION PLAN
15-20	MNDOT PEDESTRIAN RAMP DETAILS



I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

*Craig J. Jochum*  
 CRAIG J. JOCHUM, P.E.  
 HAKANSON ANDERSON  
 DESIGN ENGINEER

23461 LIC. NO. DATE 1/23/25

DATE	REVISION

SHEET 1 OF 20 SHEETS

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**GENERAL CONSTRUCTION NOTES:**

1. SAWCUT AND MILL WHEN PLACING NEW PAVEMENT ADJACENT TO INPLACE PAVEMENT AND AT TERMINI OF CONSTRUCTION TO ENSURE A UNIFORM JOINT.
2. BITUMINOUS AND CONCRETE ITEMS DISTURBED BY CONSTRUCTION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF IN ACCORDANCE WITH MN/DOT SPEC. 2104.
3. USE TACK COAT BETWEEN ALL BITUMINOUS MIXTURES. THE BITUMINOUS TACK COAT MATERIAL SHALL BE APPLIED AT A UNIFORM RATE OF 0.05 GAL/SY TO 0.07 GAL/SY BETWEEN BITUMINOUS LAYERS. THE APPLICATION RATES ARE FOR UNDILUTED EMULSIONS.
4. PERFORMANCE GRADED (PG) ASPHALT BINDER PG 58H-34, SPEC. 3151, SHALL BE USED FOR ALL BITUMINOUS MIXES ON THIS PROJECT. SPECIFIC PG GRADES ARE SHOWN AT THE END OF THE MIX DESIGNATION NUMBER SHOWN ON THE TYPICAL SECTION.
5. THE BITUMINOUS MIXTURES SHALL MEET THE REQUIREMENTS OF SPECIFICATIONS 2360 AND 3139.
6. CONTRACTOR SHALL SCHEDULE CONSTRUCTION IN COOPERATION WITH UTILITY RELOCATIONS IF ANY ARE NECESSARY. INSTALLATION OF NEW CONDUITS WILL BE REQUIRED. THE UTILITY COMPANY WILL FURNISH THE CONDUIT AND CONTRACTOR SHALL INSTALL THEM. THIS WORK SHALL BE INCIDENTAL.
7. ALL STORM SEWER SHALL BE REINFORCED CONCRETE PIPE UNLESS OTHERWISE NOTED.
8. HYDRANTS SHALL MEET THE REQUIREMENTS OF CSP 3401.
9. PIPE BEDDING SHALL BE PER CSP 3406.
10. SUBGRADE PREPARATION FOR ALL TRAILS AND WALKS SHALL BE INCIDENTAL.
11. CONCRETE CURB AND GUTTER THROUGH THE CATCH BASINS WILL BE PAID AS ITEM 2531 CONCRETE CURB AND GUTTER DESIGN SURMOUNTABLE.
12. CONCRETE CURB AND GUTTER SHALL BE THE 28" SURMOUNTABLE DESIGN SHOWN IN CSP 2000.
13. EXTRA DEPTH WATERMAIN IS REQUIRED AS INDICATED ON THE PLANS. THIS WORK SHALL BE INCIDENTAL.
14. THE BITUMINOUS WEARING COURSE SHALL NOT BE PLACED UNTIL THE YEAR 2026.
15. CONCRETE MAT SHALL BE CABLE CONCRETE CC35 MANUFACTURED BY IECS, INC. OR APPROVED EQUAL.

**GENERAL EROSION CONTROL NOTES:**

1. PRIOR TO ANY CONSTRUCTION ACTIVITIES THE CONTRACTOR SHALL ACQUIRE THE NECESSARY MPCA-NPDES STORMWATER PERMIT.
2. THE CONTRACTOR SHALL INSTALL EROSION AND SEDIMENT CONTROL FACILITIES (BMP'S) PRIOR TO GRADING AND REMOVAL ACTIVITIES. BMP'S SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION ACTIVITIES AND POTENTIAL FOR EROSION HAS PASSED.
3. THE CONTRACTOR SHALL SCHEDULE THEIR OPERATIONS TO MINIMIZE THE AMOUNT OF DISTURBED AREA AT ANY GIVEN TIME.
4. BMP'S SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND DOCUMENTED IN THE SWPPP INSPECTION LOG. SWPPP SHALL BE PROVIDED TO THE CITY.
5. ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS APPROVED BY THE ENGINEER AND THE CITY OF ELK RIVER. THIS WORK SHALL BE INCIDENTAL.
6. THE CONTRACTOR SHALL FILE NOTICE OF TERMINATION (NOT) WITH THE MPCA WHEN FINAL STABILIZATION IS APPROVED BY THE ENGINEER.
7. SILT FENCE & BLANKET SHALL BE INSTALLED BEHIND THE CURB DURING CONSTRUCTION BEFORE CLASS 5 AGGREGATE BASE IS CONSTRUCTED.
8. ALL TEMPORARY SOIL STABILIZATION AND SEEDING NECESSARY TO MEET THE REQUIREMENTS OF THE PROJECT MANUAL, MPCA NPDES CONSTRUCTION PERMIT AND MN/DOT STANDARD SPECIFICATION SHALL BE INCIDENTAL.
9. THE CONTRACTOR SHALL POST THE SWPPP ON SITE. THE METHOD OF POSTING SHALL BE APPROVED BY THE ENGINEER.
10. ALL DEWATERING REQUIRED TO COMPLETE THE CONSTRUCTION SHALL BE INCIDENTAL.

**CITY STANDARD PLATES**

PLATE NO.	DESCRIPTION	SHEET NUMBER
2000	SURMOUNTABLE AND B618 CONCRETE CURB AND GUTTER	3
2001	CONCRETE "V" GUTTER	3
2005	CATCH BASIN FRAME PLACEMENT SURMOUNTABLE CURB AND GUTTER	3
2009	STANDARD SILT FENCE	3
2015	ROCK CONSTRUCTION ENTRANCE BUILDING PERMIT	3
2500	SHALLOW CATCH BASIN	3
2503	CONCRETE PIPE JOINT TIES	4
2504	TRASH GUARD	4
2507	STANDARD INLET CASTING	4
2511	CATCH BASIN INLET PROTECTION	4
2513	CONCRETE EROSION MAT OUTLET INSTALLATION DETAIL	4
3400	TYPICAL WATER LAYOUT	4
3401	PACER HYDRANT DETAIL	4
3402	HYDRANT RESTRAINT DETAIL	4
3403	TYPICAL SERVICE DETAIL	5
3404	WATERMAIN CROSSING	5
3406	CLASS B PIPE BEDDING	5
3408	TRACER WIRE DETAIL	5
3409	TRACER WIRE LAYOUT DETAIL	5
4000	STANDARD MANHOLE	5
4007	TYPICAL HOUSE SERVICE	5

**LEGEND**

FO-BUR BURIED FIBER OPTIC CABLE  
T-BUR BURIED TELEPHONE CABLE  
G GAS MAIN  
P-BUR BURIED ELECTRIC CABLE  
P-OH OVERHEAD ELECTRIC CABLE

UTILITY PEDESTAL  
POWER POLE  
GUY WIRE  
LIGHT POLE  
SIGNAL POLE  
STORM SEWER  
STORM SEWER MANHOLE  
CATCH BASIN  
FES  
WATERMAIN  
WATERMAIN MANHOLE  
HYDRANT  
GATE/BUTTERFLY VALVE  
WATER SERVICE  
SANITARY SEWER  
SANITARY SEWER MANHOLE  
SANITARY CLEANOUT  
SEWER SERVICE  
TRUNCATED DOMES  
CONCRETE CURB & GUTTER

PROPOSED BITUMINOUS PAVEMENT  
EXISTING BITUMINOUS PAVEMENT  
CONCRETE PAVEMENT

SIGN  
MAILBOX  
DETAIL NUMBER  
SHEET NUMBER

SAWCUT BITUMINOUS OR CONCRETE  
DELINEATED WETLAND  
EXISTING TREELINE

DOUBLE SILT FENCE  
SILT FENCE  
INLET PROTECTION DEVICE  
DRAINAGE ARROW  
CONSTRUCTION ENTRANCE

EXISTING CONTOUR  
PROPOSED CONTOUR  
PROJECT PROPERTY LINE  
SURROUNDING PROPERTY LINE  
RIGHT OF WAY LINE  
EASEMENT LINE  
BUILDING SETBACK LINE  
SOIL BORING

CONIFEROUS TREE  
DECIDUOUS TREE  
CSP CITY STANDARD PLATE

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DATE	REVISION

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

*Craig J. Jochem*  
**CRAG J. JOCHUM, P.E.**  
 Lic. No. 23461

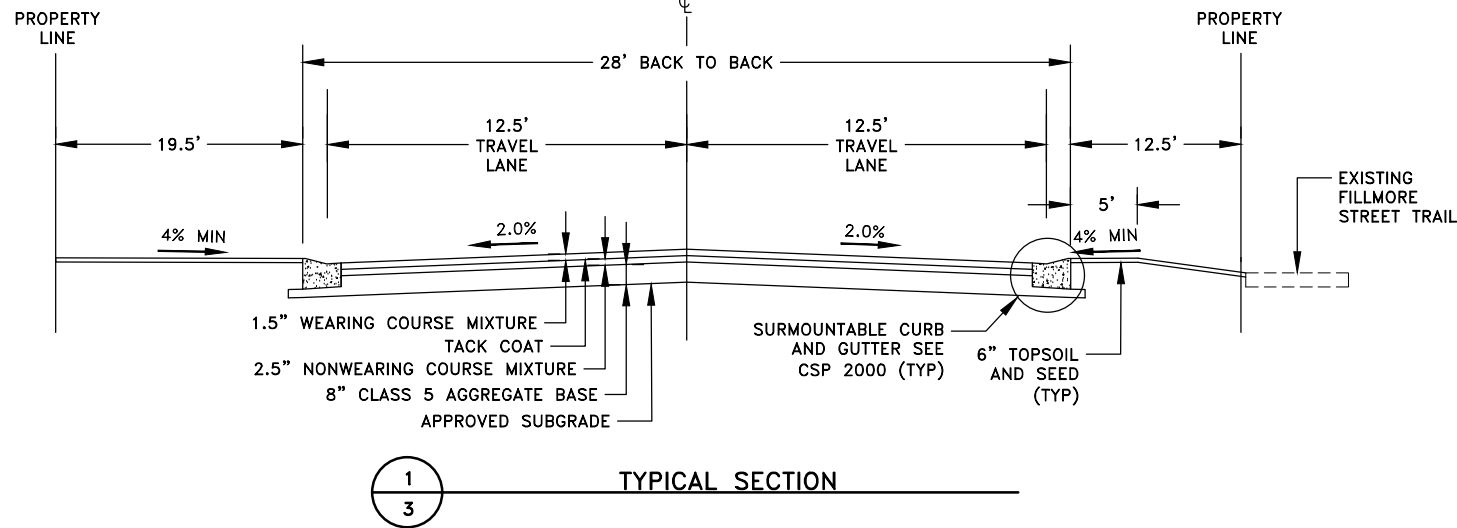
DESIGNED BY: CJJ  
 DRAWN BY: SGJ  
 CHECKED BY: TAE



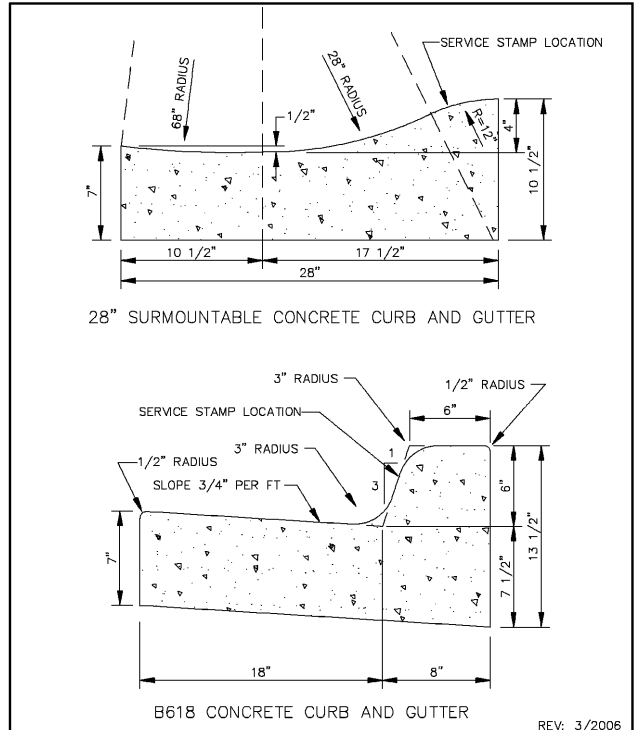
THE VILLAS AT FILLMORE THIRD ADDITION

CONSTRUCTION NOTES AND PROJECT LEGEND  
 CITY OF ELK RIVER, MINNESOTA

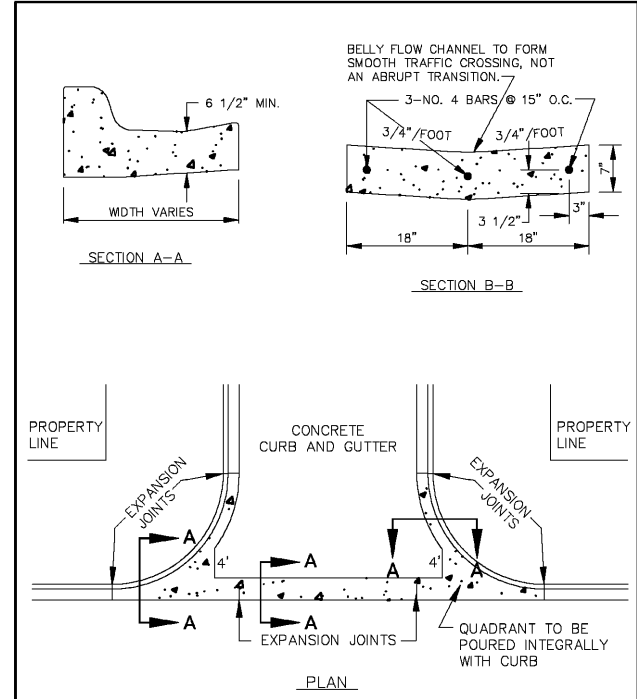
SHEET 2 OF 20 SHEETS



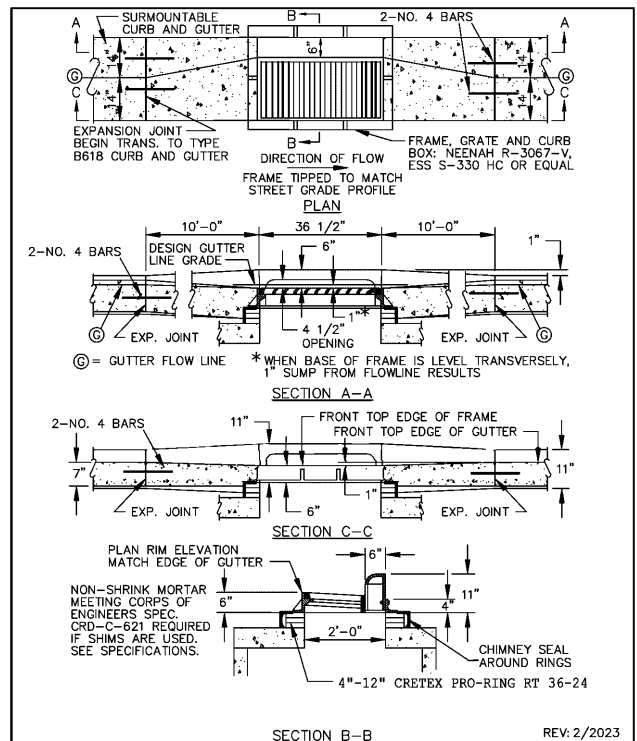
1  
3  
TYPICAL SECTION



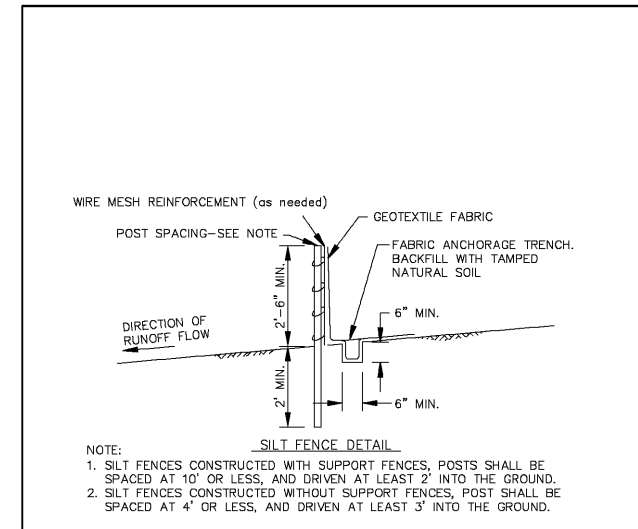
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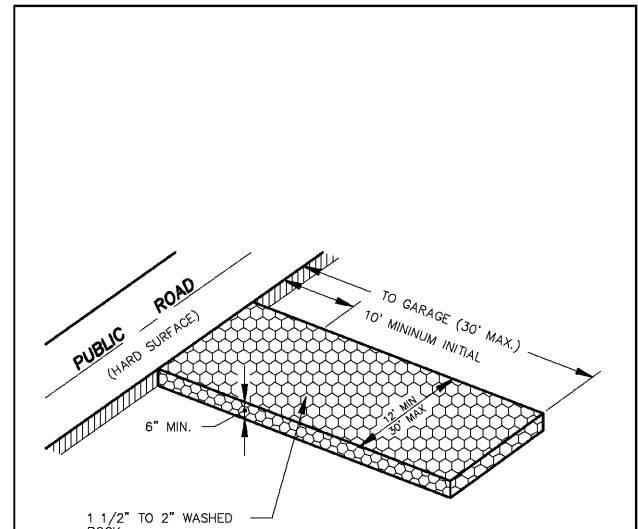
CONCRETE \"V\" GUTTER  
STANDARD PLATE NO. 2001



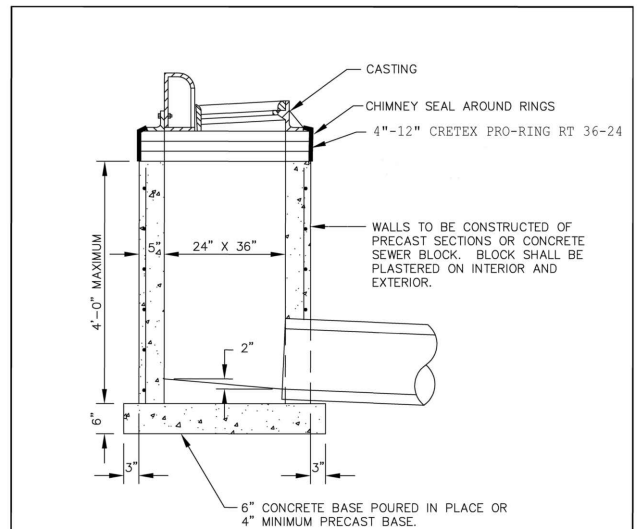
CATCH BASIN FRAME PLACEMENT SURMOUNTABLE CURB AND GUTTER  
STANDARD PLATE NO. 2005



STANDARD SILT FENCE  
STANDARD PLATE NO. 2009



ROCK CONSTRUCTION ENTRANCE BUILDING PERMIT  
STANDARD PLATE NO. 2015



SHALLOW CATCH BASIN  
STANDARD PLATE NO. 2500

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DATE	REVISION

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**CRAIG J. JOCHUM, P.E.**  
 Lic. No. 23461

DESIGNED BY: CJJ  
 DRAWN BY: SGJ  
 CHECKED BY: TAE



THE VILLAS AT FILLMORE THIRD ADDITION

DETAILS  
 CITY OF ELK RIVER, MINNESOTA

SHEET 3 OF 20 SHEETS

PIPE SIZE	THREAD DIAMETER
12	5/8" ROLLED THREADS (SEE NOTE 4)
15	
18	
21	
24	
27	
30	3/4" CUT OR ROLLED
33	
36	
42	
48	
54	1" CUT OR ROLLED
60	
66	
72	
78	
84	
90	
96	
102	
108	
120	
132	

**NOTE:**

- HOLES SHALL BE CAST-IN OR DRILLED 16" FROM OUTSIDE EDGE OF JOINT.
- NUTS AND WASHERS ARE NOT REQUIRED ON INSIDE OF PIPE WITH 27" DIAMETER OR LESS.
- TIES ARE NOT REQUIRED FOR BELL PIPE 24" AND SMALLER. ON OTHER SIZES, TIE MAY BE INSERTED FROM INSIDE.
- CUT THREADS MAY BE USED IF WASHER AND NUT ARE USED.
- PIPE SIZES LISTED ARE INSIDE DIAMETER OF ROUND PIPE OR EQUIVALENT DIAMETER OF PIPE ARCH.

REV: 3/2006

STANDARD PLATE NO. **2503**

3/4" STEEL BARS - TRANSVERSE AND LONGITUDINAL. WELD EACH INTERSECTION.

6" TYPICAL

EQUIDISTANT

FLARED END SECTION

REV: 3/2006

STANDARD PLATE NO. **2504**

INSTALL CAP SCREWS WITH BOLT HEADS EXPOSED

36 1/2"

35 1/4"

3 1/8"

33"

36"

43"

CURB BOX ADJUSTABLE 6" TO 9"

1 7/8"

5"

17 3/4"

6"

15 1/2"

24"

31"

CASTINGS SHALL BE NEENAH R-3067-V OR EQUAL

REV: 3/2006

STANDARD PLATE NO. **2507**

DEFLECTOR PLATE

OVERFLOW 1 - CENTER OF FILTER ASSEMBLY

OVERFLOW 2 - TOP OF CURB BOX

10" FILTER ASSEMBLY

HIGH FLOW FABRIC

WIMCO MODEL# CG 3067 OR EQUAL

REV: 3/2006

STANDARD PLATE NO. **2511**

CROSS SECTIONAL VIEW

PLAN

CONCRETE MAT

EXISTING GROUND

STORM WATER SHALL BE CONTAINED TO THE CHANNEL.

USE CONCRETE SAW TO CUT MAT TO EXACT SHAPE AND SIZE

PRIOR TO INSTALLING HEADWALL, ANCHOR TRENCH INITIAL LEADING EDGE OF MAT 18", FILL AND COMPACT ANCHOR TRENCH. INSTALL HEADWALL OVER MAT.

MAXIMUM GRADE PER MFG SPECIFICATIONS

CONCRETE MAT

CONSTRUCTION NOTES:

- GRADE OUTFALL AREA SO THAT WATER WILL FLOW DOWN THE CENTER OF THE CHANNEL AND BE CONTAINED TO THE CHANNEL. ALL SUBGRADE SURFACES PREPARED FOR PLACEMENT OF MATS SHALL BE SMOOTH AND FREE OF ALL ROCKS, STICKS, ROOTS, OTHER PROTRUSIONS, OR DEBRIS OF ANY KIND. THE PREPARED SURFACE SHALL PROVIDE A FIRM UNYIELDING FOUNDATION FOR THE MATS WITH NO SHARP OR ABRUPT BREAKS IN THE GRADE.
- APPLY SEED DIRECTLY TO THE PREPARED SOIL PRIOR TO CONCRETE MAT INSTALLATION. USE SEED PER PROJECT SPECIFICATIONS.
- PRIOR TO INSTALLING HEADWALL, ANCHOR TRENCH INITIAL LEADING EDGE OF MAT 18", FILL AND COMPACT ANCHOR TRENCH. INSTALL HEADWALL OVER MAT.
- INSTALL CONCRETE MAT ROLLS. AVAILABLE WIDTHS ARE 3'-5", 8', 10', 12', & 16" AVAILABLE IN CUSTOM LENGTHS. A CONCRETE SAW SHALL BE USED TO CUT THE MAT TO EXACT SIZE AND SHAPE.
- APPLICATIONS WHERE OUTFALLS FLOW INTO RETENTION PONDS, EXTEND CONCRETE MAT A MINIMUM OF 3' BELOW LOW WATER ELEVATION.

REV: 2/2017

STANDARD PLATE NO. **2513**

CURB BOX AND STOP TO UTILITY EASEMENT SEE SERVICE DETAIL

PROPERTY LINE

VALVE

VALVE CROSS

VALVE

VALVE

VALVE

CORPORATION COCK

PROPERTY LINE

SEE HYDRANT DETAIL

VALVES ON ALL MAINS LEAVING INTERSECTIONS

REV: 2/2017

STANDARD PLATE NO. **3400**

NOZZLE SECTION

OPERATING NUT - 1" PENTAGON

2 1/2" HOSE CONNECTION (THREAD SIZE 3 1/16" O.D., 7 1/2" T.P.I.)

4 1/2" PUMPER CONNECTION (THREAD SIZE 5 9/16" O.D., 4" T.P.I.)

NUT CAP TYPE WITH CHAINS

NOZZLE ARRANGEMENT TWO 2 1/2" NOZZLE PLUS PUMPER NOZZLE WITH 1" PENTAGON OPERATING NUTS

NOMINAL GROUND LINE

NOTE:

- 16" BREAKOFF SECTION
- LEFT HAND OPERATING NUT (1 1/2" POINT TO FLAT)
- PAINT BARREL SECTION RED TO GRADE.
- DO NOT PLUG DRAIN HOLES.

STANDPIPE

BOTTOM

7/8" ROUND HOLES

NOTE: "HYDRAFINDER" HYDRANT LOCATING DEVICES REQUIRED - SEE SPECIFICATIONS.

REV: 3/2006

STANDARD PLATE NO. **3401**

EDGE OF MAT OR BACK OF CURB

5'-0"

"HYDRAFINDER"

MINIMUM COVER AS REQUIRED IN SPECIFICATIONS OR AS SHOWN ON PLANS.

VALVE WITH MECHANICAL JOINT TEE AND VALVE BOX

6"-12"

2'-6"

GATE VALVE ADAPTOR

TRACER WIRE

6mm POLYETHYLENE OR EQUAL COVER

WATERMAIN WITH MECHANICAL JOINT TEE FOR SIZE FIRE HYDRANT LINE

0.75" - 1.5" DRAIN ROCK

CONCRETE BLOCK

TRACER WIRE ACCESS BOX AND GROUNDING ROD SAME AS FOR WATER SERVICE SHOWN IN DETAIL 3408

JOINT RESTRAINING COUPLING-TWO 3/4" ALL THREAD RODS

NOTE: ALL HYDRANT LEADS SHALL BE RESTRAINED FROM THE MAIN TO THE HYDRANT THROUGH THE USE OF THREADED RODS OR MEGALUGS AS RESTRAINTS IN ACCORDANCE WITH THE STANDARD DETAIL PLATES.

REV: 2/2017

STANDARD PLATE NO. **3402**

Jan 24, 2025 - 11:09am  
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DATE	REVISION

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**CRAM J. JOCHUM, P.E.**  
Date 1/23/25 Lic. No. 23461

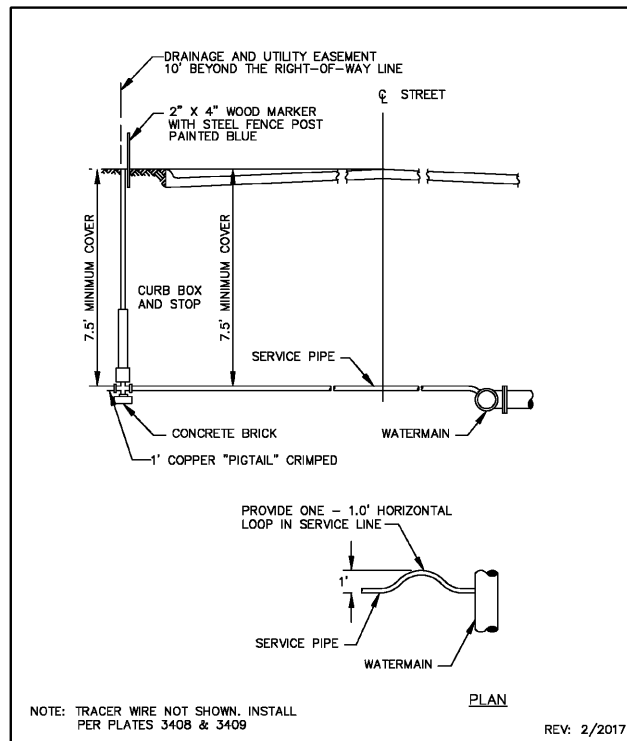
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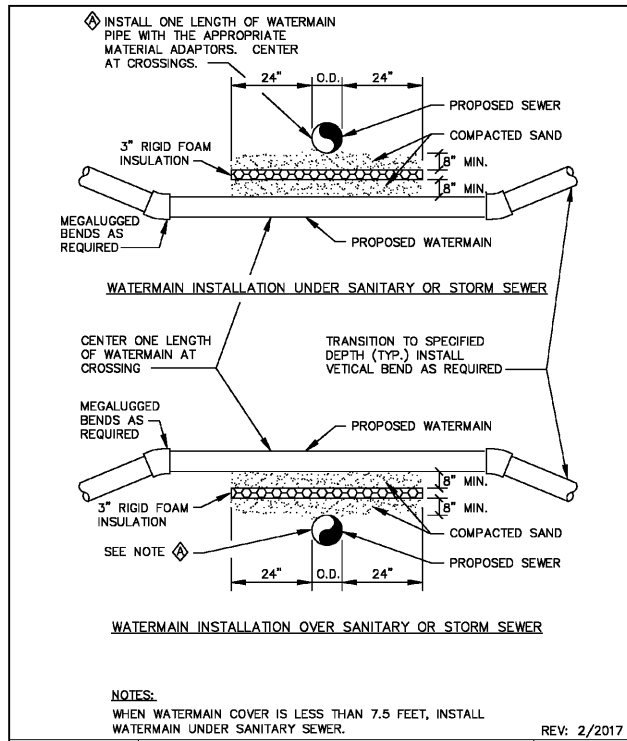
THE VILLAS AT FILLMORE THIRD ADDITION

DETAILS  
CITY OF ELK RIVER, MINNESOTA

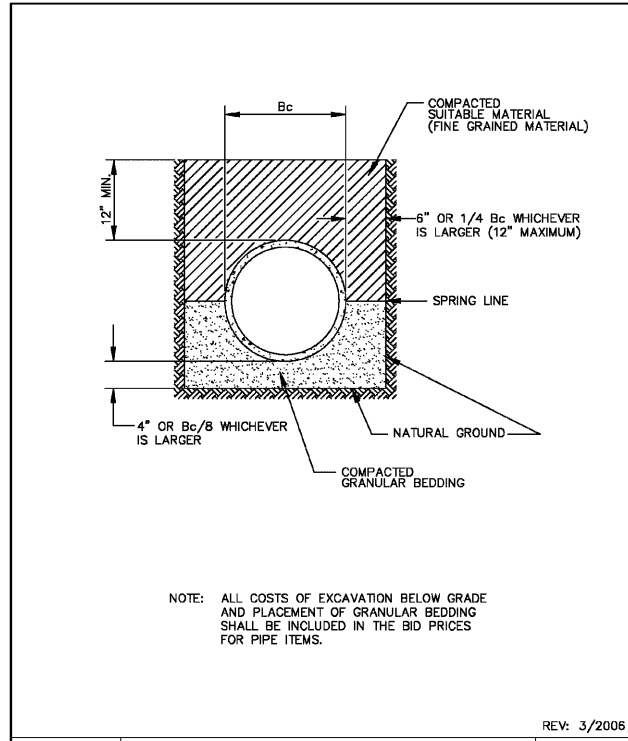
SHEET 4 OF 20 SHEETS



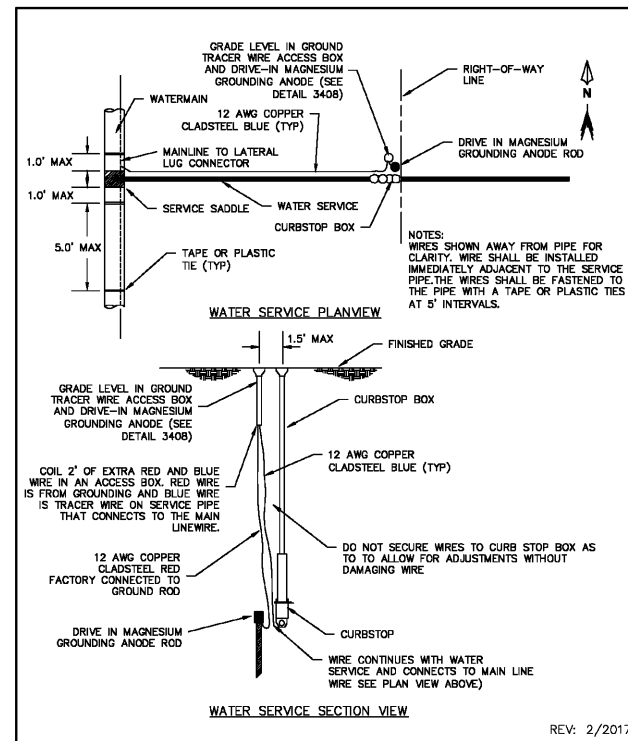
**TYPICAL SERVICE DETAIL**  
STANDARD PLATE NO. **3403**  
REV: 2/2017



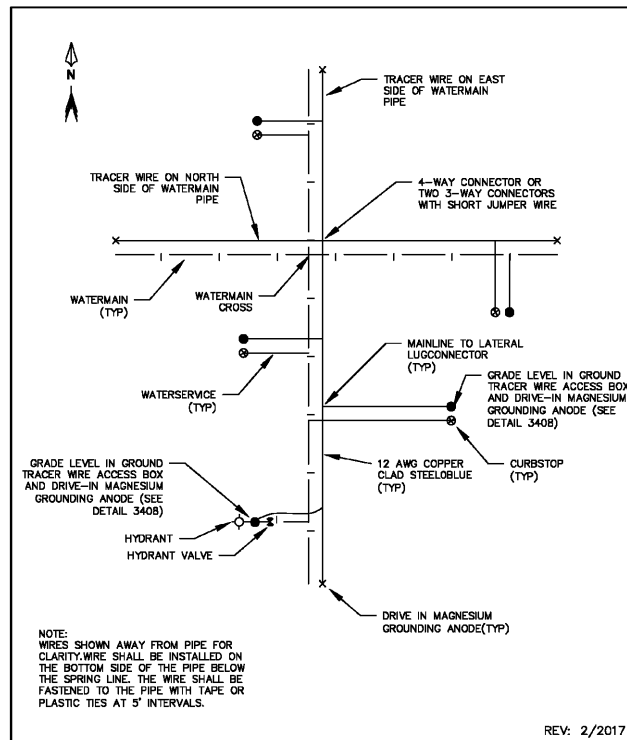
**WATERMAIN CROSSING**  
STANDARD PLATE NO. **3404**  
REV: 2/2017



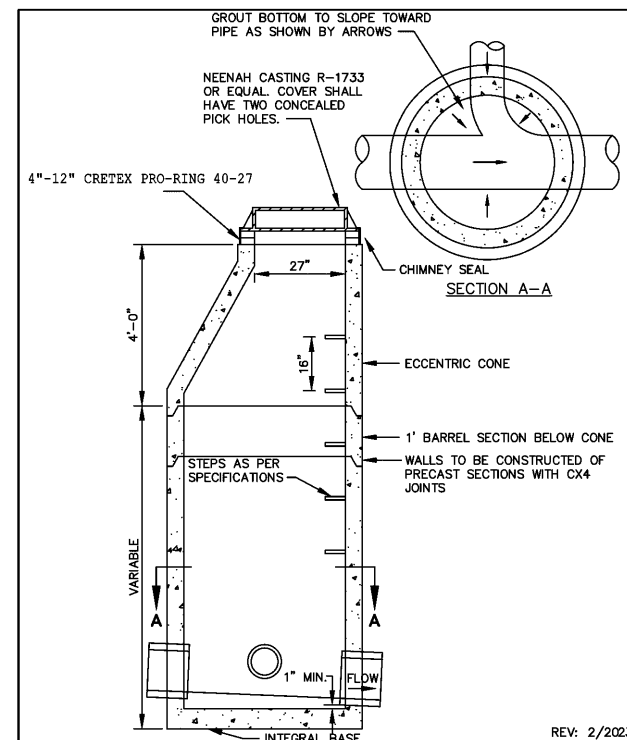
**CLASS B PIPE BEDDING**  
STANDARD PLATE NO. **3406**  
REV: 3/2006



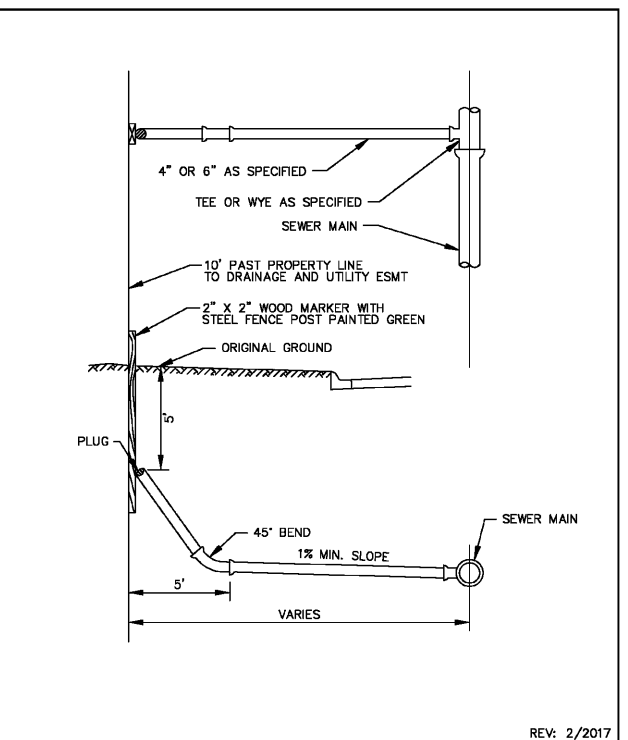
**TRACER WIRE DETAIL**  
STANDARD PLATE NO. **3408**  
REV: 2/2017



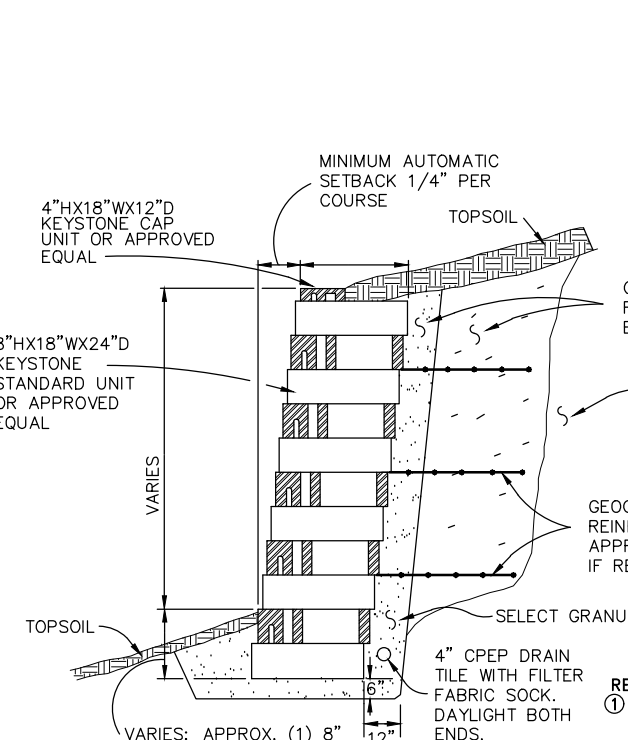
**TRACER WIRE LAYOUT DETAIL**  
STANDARD PLATE NO. **3409**  
REV: 2/2017



**STANDARD MANHOLE**  
STANDARD PLATE NO. **4000**  
REV: 2/2023



**TYPICAL HOUSE SERVICE**  
STANDARD PLATE NO. **4007**  
REV: 2/2017



**TYPICAL RETAINING WALL SECTION 1**  
NO SCALE

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DATE	REVISION

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**CRAIG J. JOCHUM, P.E.**  
 Lic. No. 23461  
 Date 1/23/25

DESIGNED BY: CJJ  
 DRAWN BY: SGJ  
 CHECKED BY: TAE

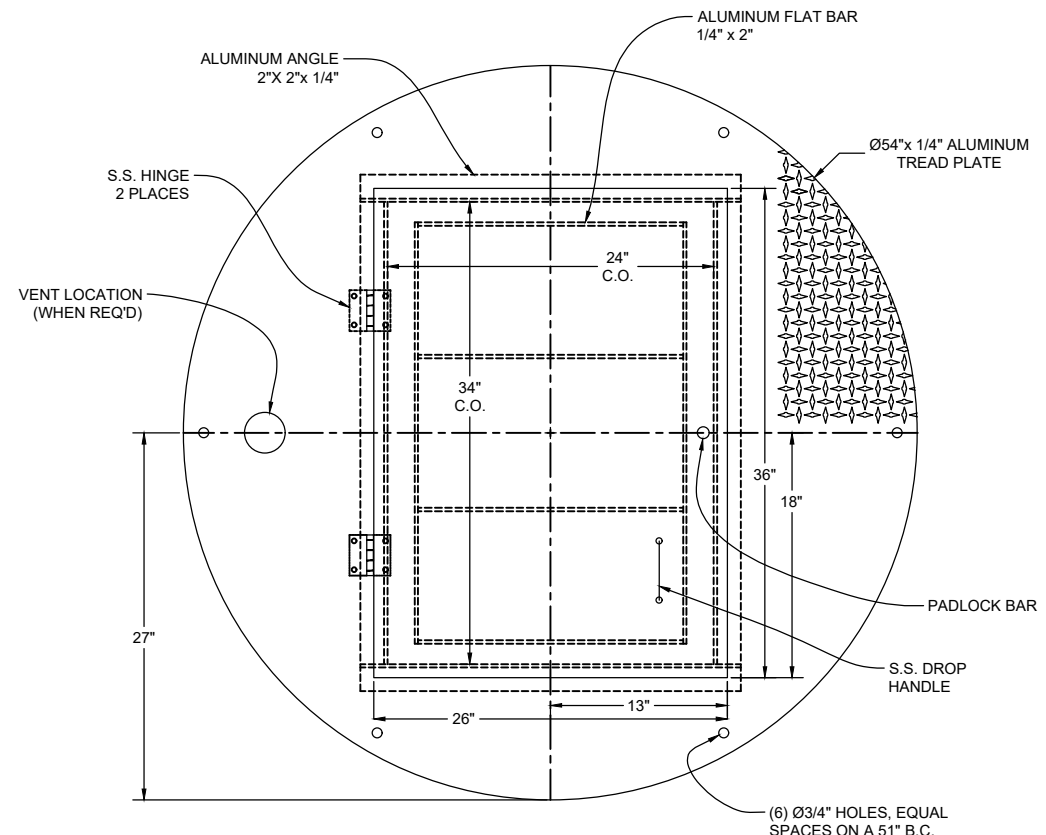
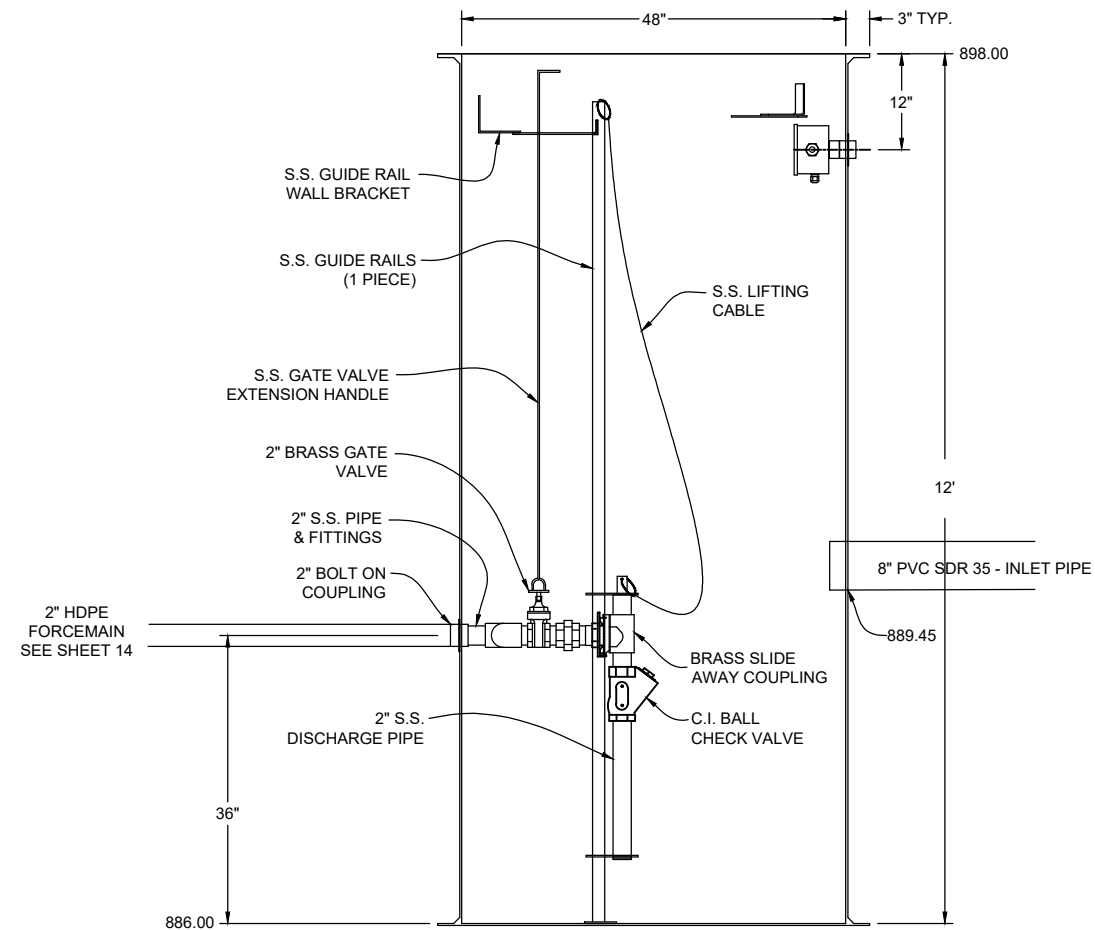
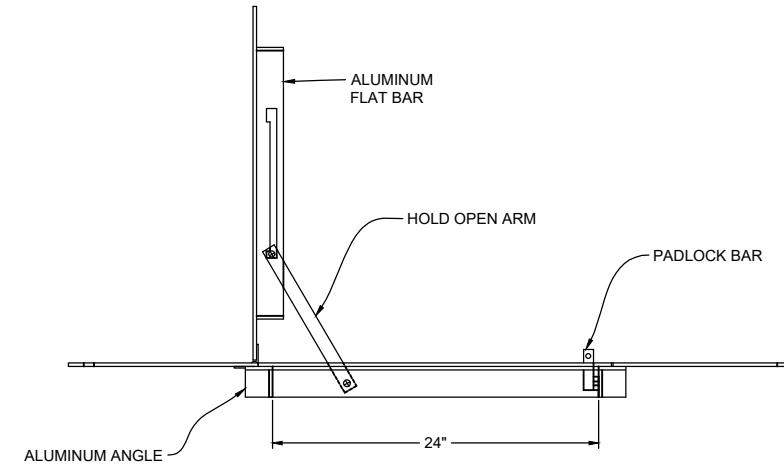
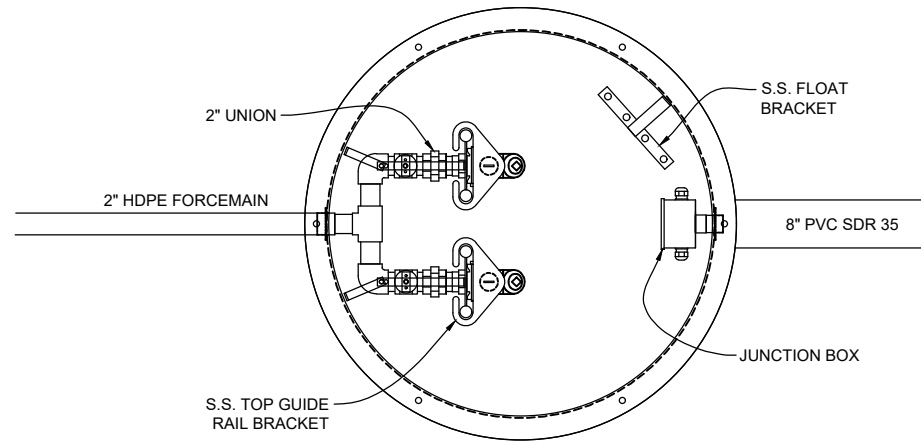
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 LAND SURVEYORS + ENGINEERS  
 505 First Street, Princeton, MN 55371  
 0-763.389.4476 RRLSE.com

**THE VILLAS AT FILLMORE THIRD ADDITION**

**DETAILS**

CITY OF ELK RIVER, MINNESOTA

SHEET **5** OF **20** SHEETS



-300 PSF LOAD RATING

**SP** STEELE PLASTICS, LLC  
 1280 Sturgis Road  
 Conway, AR 72034  
 (501) 327-5122 Fax (501) 327-0807

TITLE	PROJECT	JOB NO.
CUSTOMER	P.O. NO.	DWG. NO.

**SP** STEELE PLASTICS, LLC  
 1280 Sturgis Road  
 Conway, AR 72034  
 (501) 327-5122 Fax (501) 327-0807

TITLE	PROJECT	JOB NO.
CUSTOMER	P.O. NO.	DWG. NO.

GENERAL NOTES:  
 1. SEE SHEET 10 FOR SANITARY SEWER PLAN

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 Date 1/23/25 **CRAM J. JOCHUM, P.E.** Lic. No. 23461

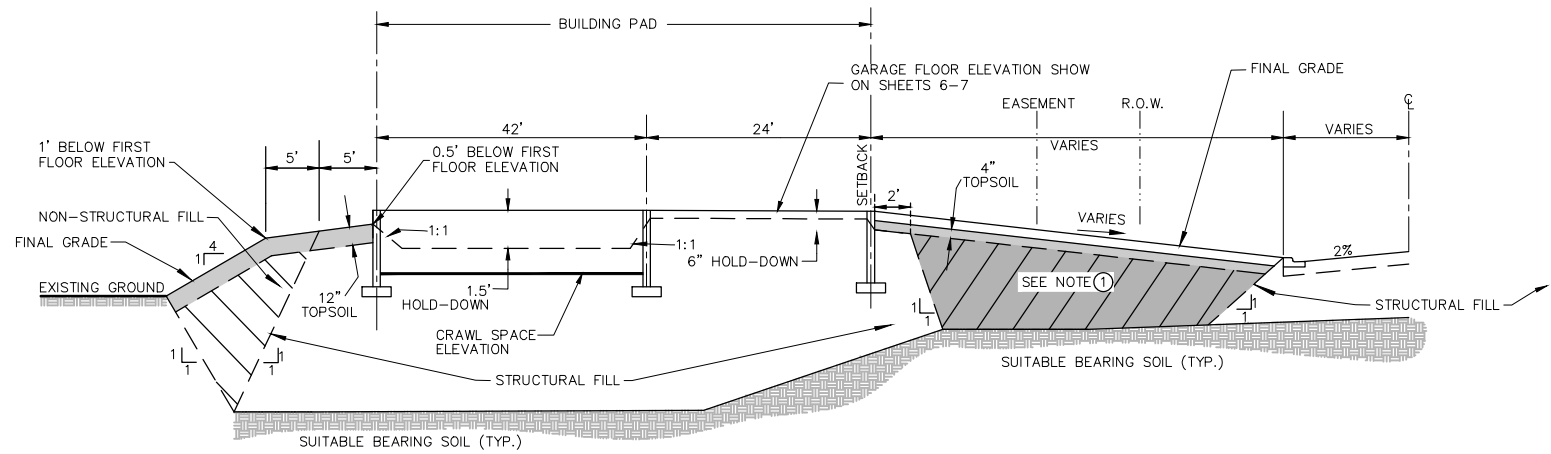
DESIGNED BY: CJJ  
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THE VILLAS AT FILLMORE THIRD ADDITION

DETAILS  
 CITY OF ELK RIVER, MINNESOTA

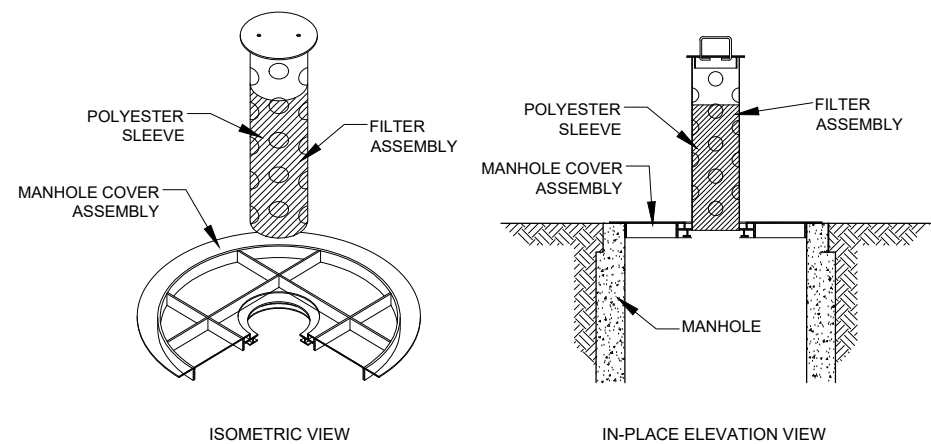
SHEET 6 OF 20 SHEETS



1  
7  
TYPICAL LOT SECTION

- GENERAL SOIL NOTES:
1. STRIP AND REUSE ALL INPLACE TOPSOIL IN AREAS TO BE DISTURBED BY CONSTRUCTION.
  2. STREET AND LOT FILL SHALL BE CONSTRUCTED WITH STRUCTURAL FILL. ALL STRUCTURAL FILL SHALL BE CONSTRUCTED UNDER THE DIRECTION OF A GEOTECHNICAL ENGINEER. STRUCTURAL FILL FOR THIS PROJECT IS DEFINED IN THE GEOTECHNICAL REPORT. CONTRACTOR SHALL ONLY USE SOIL CLASSIFIED AS SP OR SP-SM IN THE ROAD CORE.
  3. THE FINAL GRADES SHOWN ASSUME A 15 PERCENT SOIL COMPACTION FACTOR ON THE TOTAL FILL. IF THE ACTUAL COMPACTION FACTOR IS GREATER OR LESS THAN 15 PERCENT, THE CONTRACTOR SHALL WORK WITH THE ENGINEER DURING THE GRADING OPERATION TO ADJUST THE FINAL GRADES AS NECESSARY TO BALANCE THE EARTHWORK. ADJUSTMENT TO FUTURE LOT GRADES AND STREETS WILL BE MADE AS NECESSARY.
  4. THE CONTRACTOR SHALL STRIP AND TEMPORARILY STOCKPILE ALL EXISTING TOPSOIL FOR REUSE. SALVAGING TOPSOIL SHALL BE PAID PER ITEM 2106 SALVAGE TOPSOIL. PLACING SALVAGED TOPSOIL OVER THE DISTURBED AREAS AS SHOWN AND PLACING FILL IN OTHER AREAS SHALL BE INCIDENTAL.
  5. SHEET 9 SHOWS THE FINAL GRADING PLAN AFTER THE HOME CONSTRUCTION IS COMPLETE. THESE GRADES REPRESENT THE SURFACE LABELED AS THE FINAL GRADE ON THE TYPICAL LOT SECTION ON SHEET 7. IN GENERAL THIS PROJECT INCLUDES GRADING THE SITE TO THE HOLD-DOWN GRADES THROUGH THE BUILDING PADS AS SHOW ON THE TYPICAL LOT SECTIONS ON SHEET 7 AND GRADING TO THE FINAL GRADES OUTSIDE THE BUILDING PADS, UNLESS OTHERWISE NOTED, AS SHOWN ON THE TYPICAL LOT SECTIONS ON SHEET 7. NOT ALL THE LOTS FOLLOW THE TYPICAL SECTION.

REFERENCE NOTES:  
 ① ONSITE SILTY/SANDY TOPSOIL WITH LESS THAN 5% ORGANICS CAN BE USED AS FILL IN THESE AREAS.



2  
7  
STORM DRAIN INLET PROTECTION

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*Craig J. Jochum*  
 CRAIG J. JOCHUM, P.E.  
 Lic. No. 23461  
 Date 1/23/25

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 DRAWN BY: SGJ  
 CHECKED BY: TAE

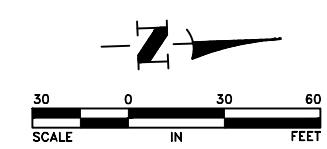
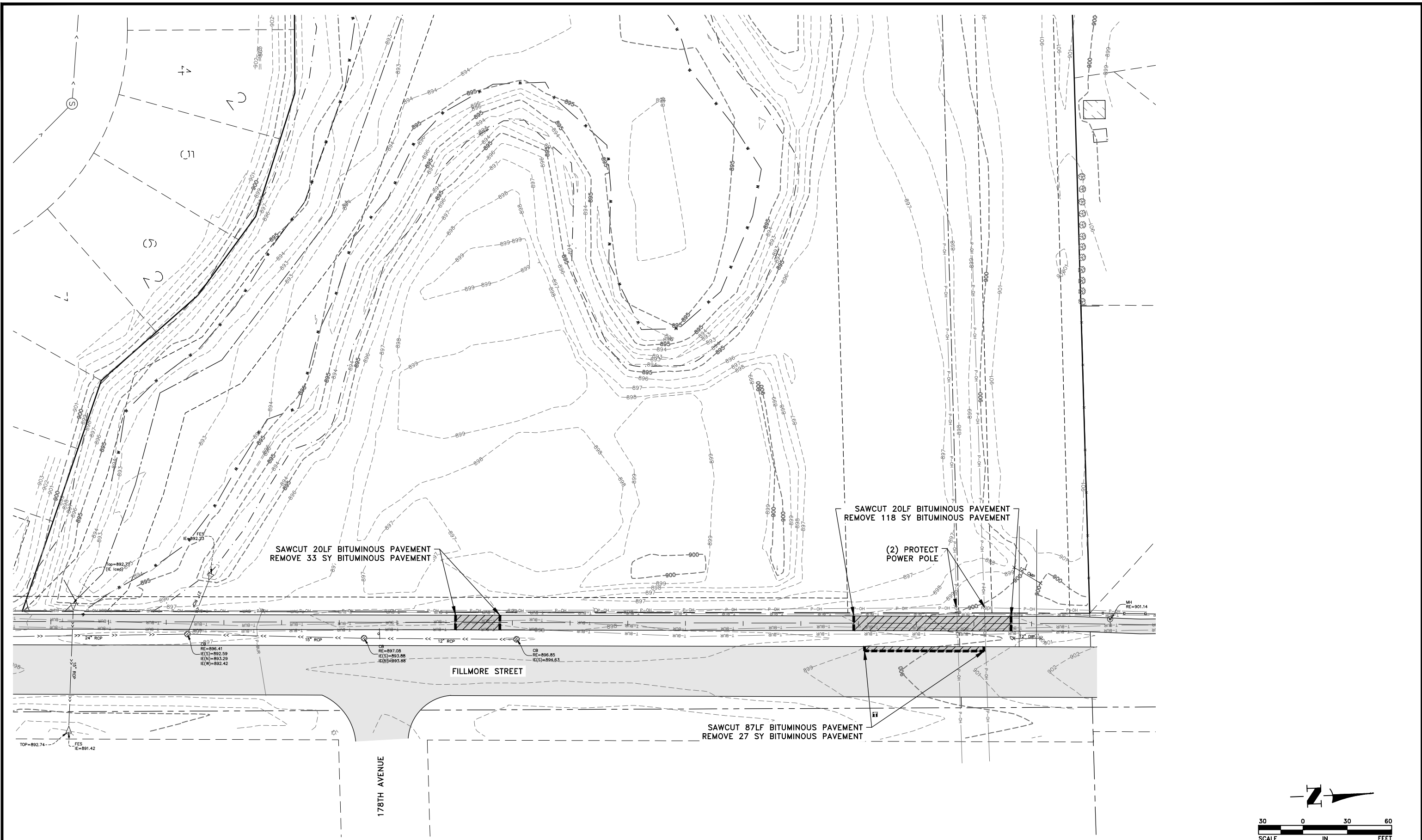


THE VILLAS AT FILLMORE THIRD ADDITION

TYPICAL LOT SECTIONS, SOIL NOTES, AND DETAILS  
 CITY OF ELK RIVER, MINNESOTA

SHEET 7 OF 20 SHEETS

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**CRAG J. JOCHUM, P.E.**  
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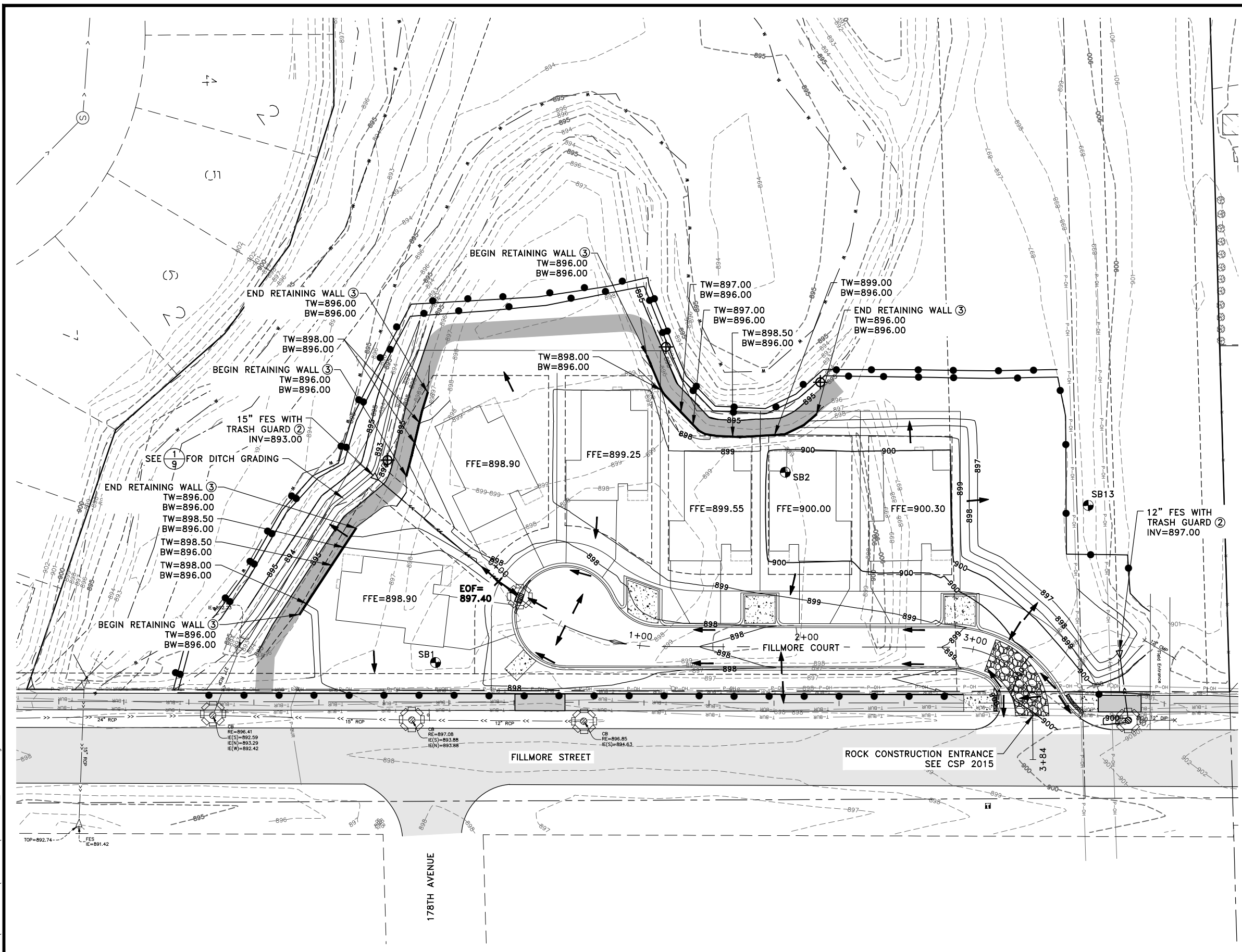
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THE VILLAS AT FILLMORE THIRD ADDITION

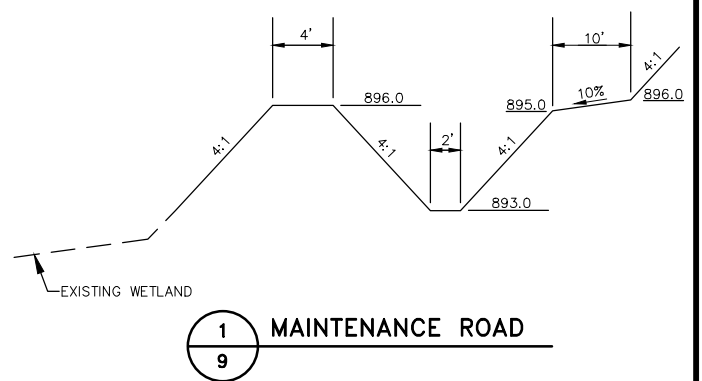
EXISTING CONDITIONS AND REMOVALS PLAN  
 CITY OF ELK RIVER, MINNESOTA

SHEET 8 OF 20 SHEETS



- LEGEND**
- SILT FENCE SEE CSP 2009
  - DOUBLE SILT FENCE SEE CSP 2009
  - INLET PROTECTION SEE CSP 2511 AND  $\frac{2}{7}$
  - EOF=902.0 EMERGENCY OVERFLOW
  - X 902.0 SPOT ELEVATION
  - DRAINAGE DIRECTION ARROW
  - FFE=900.00 PROPOSED FINISHED FLOOR ELEVATION
  - WETLAND BUFFER SIGN (3 TOTAL) ①
  - CONSTRUCT 10' WIDE MAINTENANCE ROAD SEE  $\frac{1}{9}$

- GENERAL NOTES:**
1. SEE SHEET 2 FOR INDEX OF CITY STANDARD PLATES (CSP).
  2. SEE SHEET 7 FOR SOIL NOTES AND LOT SECTIONS.
- REFERENCE NOTES:**
- ① WETLAND BUFFER SIGN AND INSTALLATION KIT SHALL BE PURCHASED FROM THE CITY OF ELK RIVER. THE CURRENT COST IS \$15 PER SIGN.
  - ② SEE CSP 2504 FOR TRASH GUARD CONSTRUCTION. A CONCRETE EROSION MAP SHALL BE CONSTRUCTED WITH THE FES. SEE CSP 2513 FOR CONSTRUCTION DETAILS.
  - ③ SEE  $\frac{1}{5}$  FOR RETAINING WALL CONSTRUCTION.



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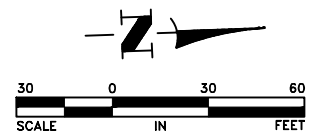


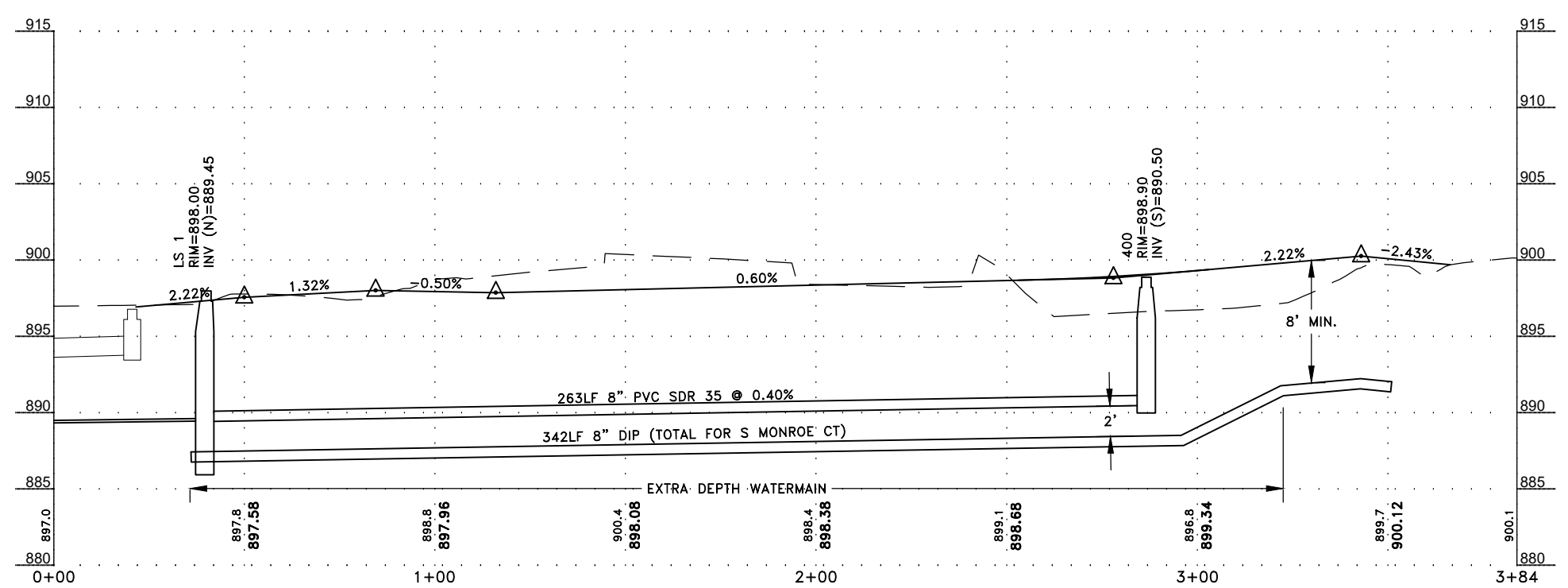
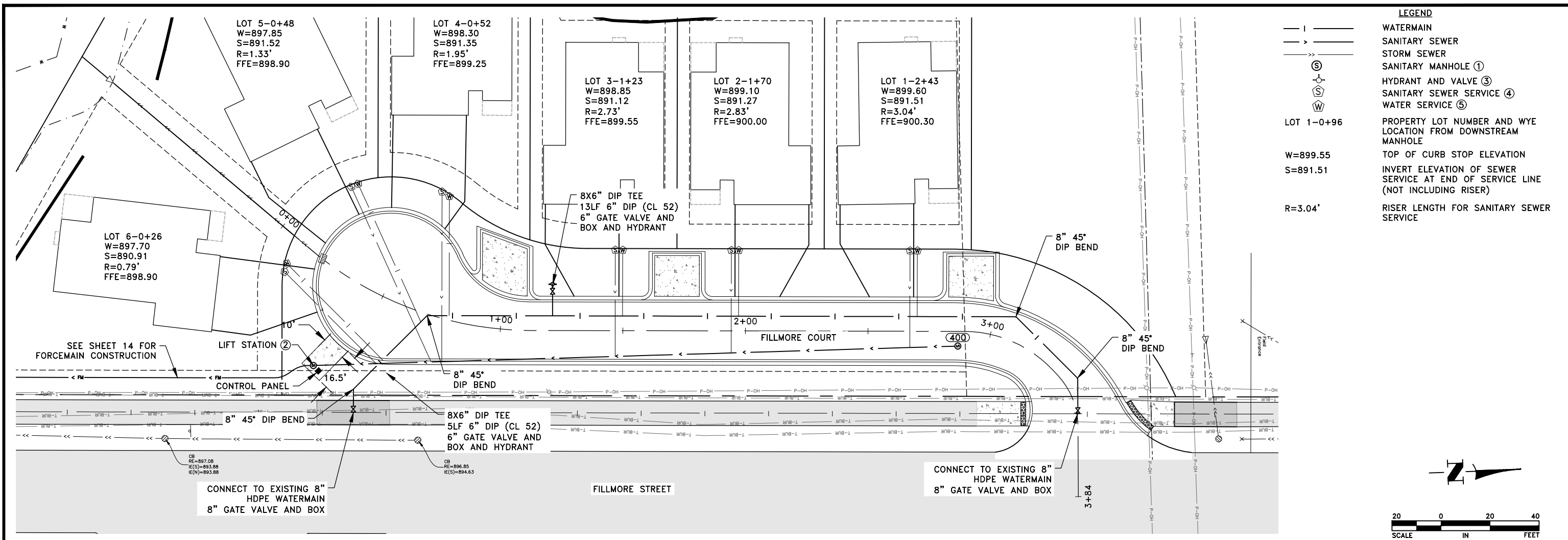
THE VILLAS AT FILLMORE THIRD ADDITION

GRADING AND EROSION CONTROL PLAN

CITY OF ELK RIVER, MINNESOTA

SHEET 9 OF 20 SHEETS





- GENERAL NOTES:**
1. WATERMAIN AND SANITARY SEWER SHALL BE BEDDED PER CSP 3406.
  2. SEE SHEET 2 FOR INDEX OF CITY STANDARD PLATES (CSP).
  3. ALL WATERMAIN TRUNK, WATER SERVICES, AND HYDRANTS WILL REQUIRE TRACER WIRE. SEE CSP 3402, 3408, AND 3409 FOR ADDITIONAL INFORMATION.
- REFERENCE NOTES:**
- ① CONSTRUCT MANHOLE PER CSP 4000.
  - ② SEE SHEET 6 FOR LIFT STATION DETAILS.
  - ③ CONSTRUCT HYDRANT PER CSP 3401 AND 3402.
  - ④ CONSTRUCT SANITARY SEWER SERVICE PER CSP 4007.
  - ⑤ CONSTRUCT WATER SERVICE PER CSP 3403.

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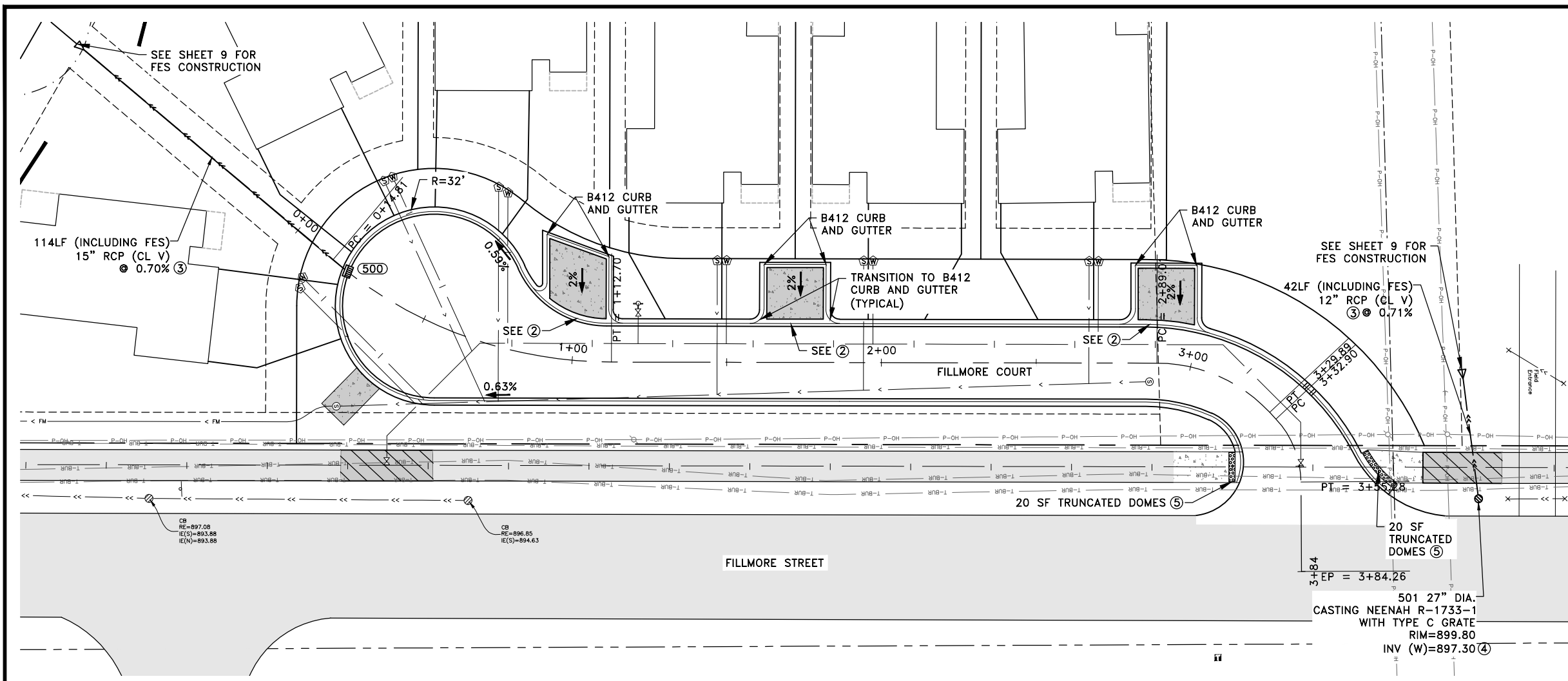


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THE VILLAS AT FILLMORE THIRD ADDITION

WATERMAIN AND SANITARY SEWER PLAN  
 FILLMORE COURT  
 CITY OF ELK RIVER, MINNESOTA

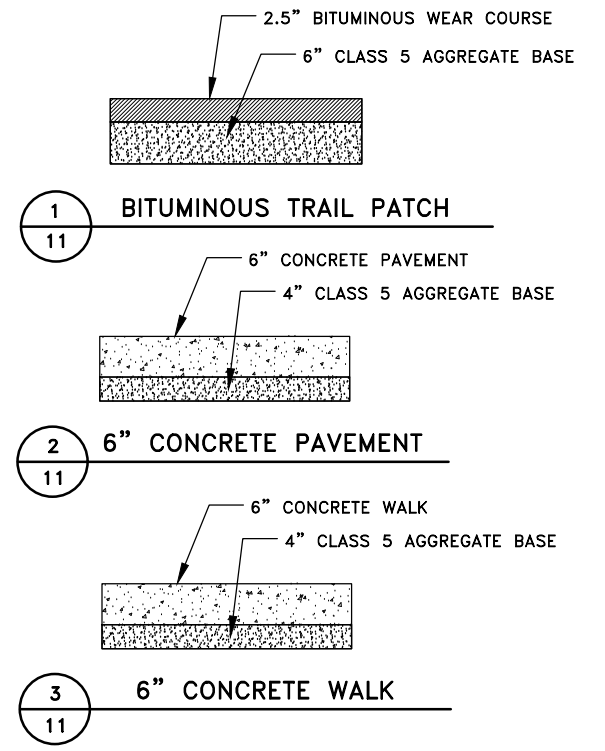
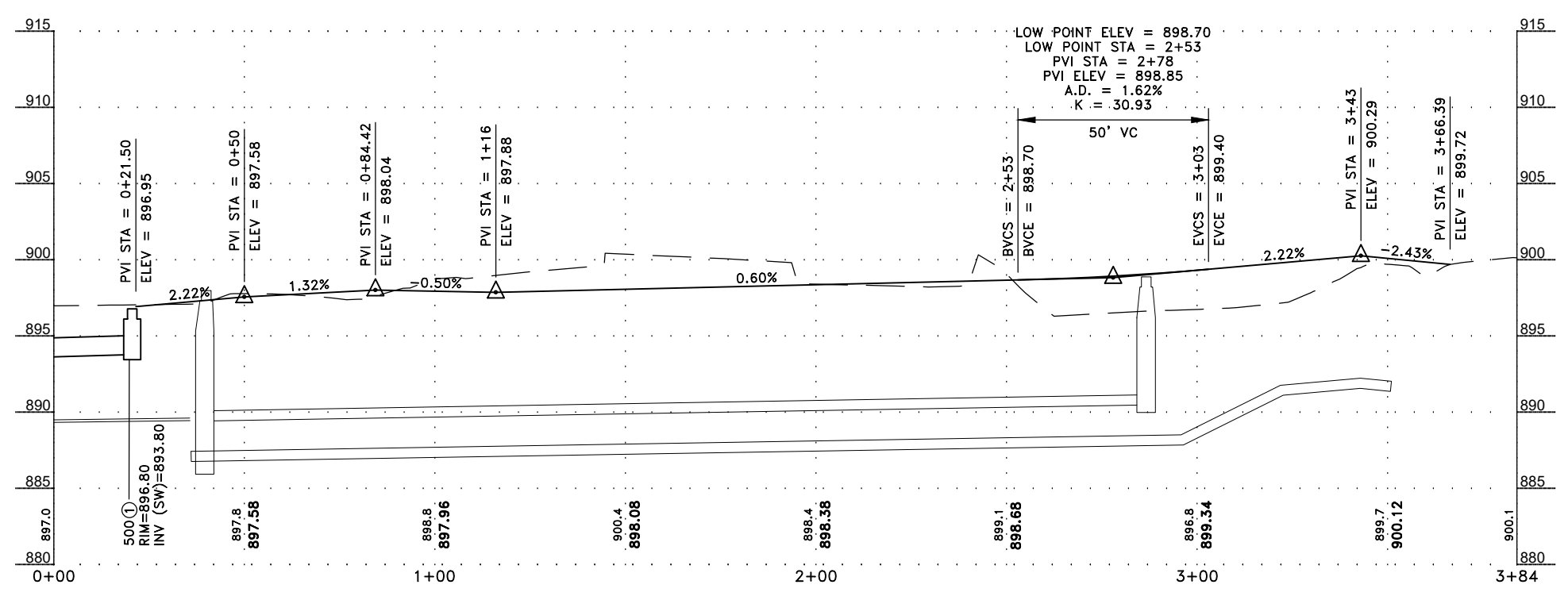
SHEET 10 OF 20 SHEETS



**LEGEND**

	BITUMINOUS TRAIL PATCH SEE ①/11
	6" CONCRETE PAVEMENT SEE ②/11
	6" CONCRETE WALK SEE ③/11
	EXISTING BITUMINOUS PAVEMENT

- GENERAL NOTES:**
- SEE ①/3 AND CSP 2000 FOR STREET AND CURB CONSTRUCTION.
  - CATCH BASIN FRAMES IN SURMOUNTABLE CURB SHALL BE PLACED PER CSP 2005.
  - SEE SHEET 2 FOR INDEX OF CITY STANDARD PLATES (CSP).
- REFERENCE NOTES:**
- CONSTRUCT STRUCTURE PER CSP 2500. CASTING WILL BE PER CSP 2507.
  - CONSTRUCT VALLEY GUTTER PER CSP 2001.
  - ALL PIPE JOINTS SHALL BE TIED PER CSP 2503.
  - CONSTRUCT STRUCTURE PER MNDOT STANDARD PLATE 4006L DESIGN H.
  - SHEETS 13 AND 15-20 FOR PEDESTRIAN CURB RAMP DETAILS.



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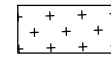


THE VILLAS AT FILLMORE THIRD ADDITION

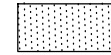
STREET AND STORM SEWER PLAN  
FILLMORE COURT  
CITY OF ELK RIVER, MINNESOTA

SHEET 11 OF 20 SHEETS

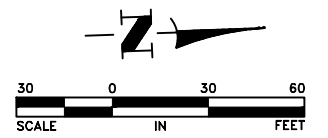
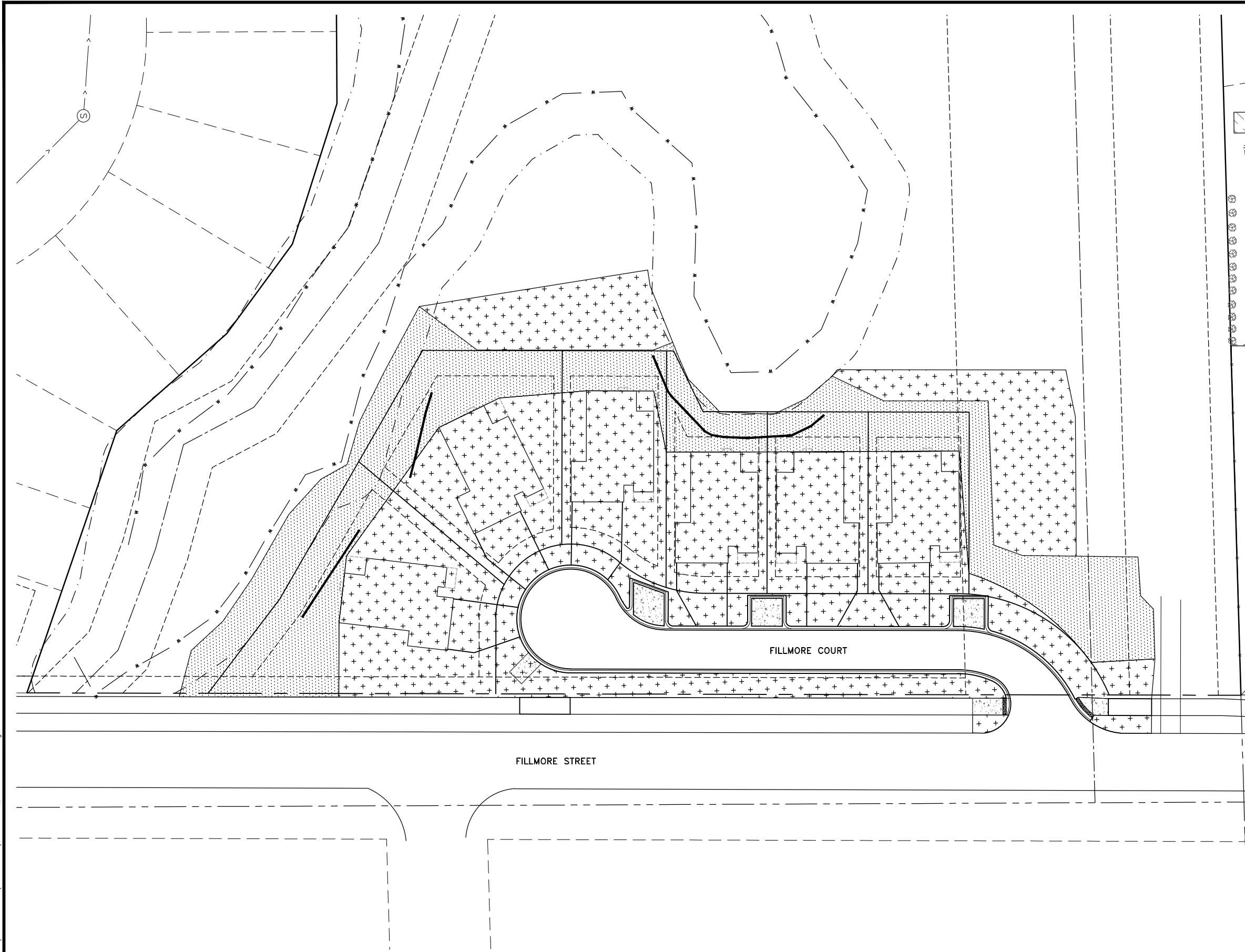
LEGEND



SEED SOUTHERN BOULEVARD (160 POUNDS/ACRE)  
TYPE 1 FERTILIZER (300 POUNDS/ACRE)  
MULCH TYPE 1 - DISK ANCHOR (2 TONS/ACRE)



SEED SOUTHERN (160 POUNDS/ACRE)  
TYPE 1 FERTILIZER (300 POUNDS/ACRE)  
ROLLED EROSION PREVENTION CATEGORY 25



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*[Signature]*  
**CRAM J. JOCHUM, P.E.**  
 Date 1/23/25 Lic. No. 23461

DESIGNED BY:  
CJJ

DRAWN BY:  
SGJ

CHECKED BY:  
TAE

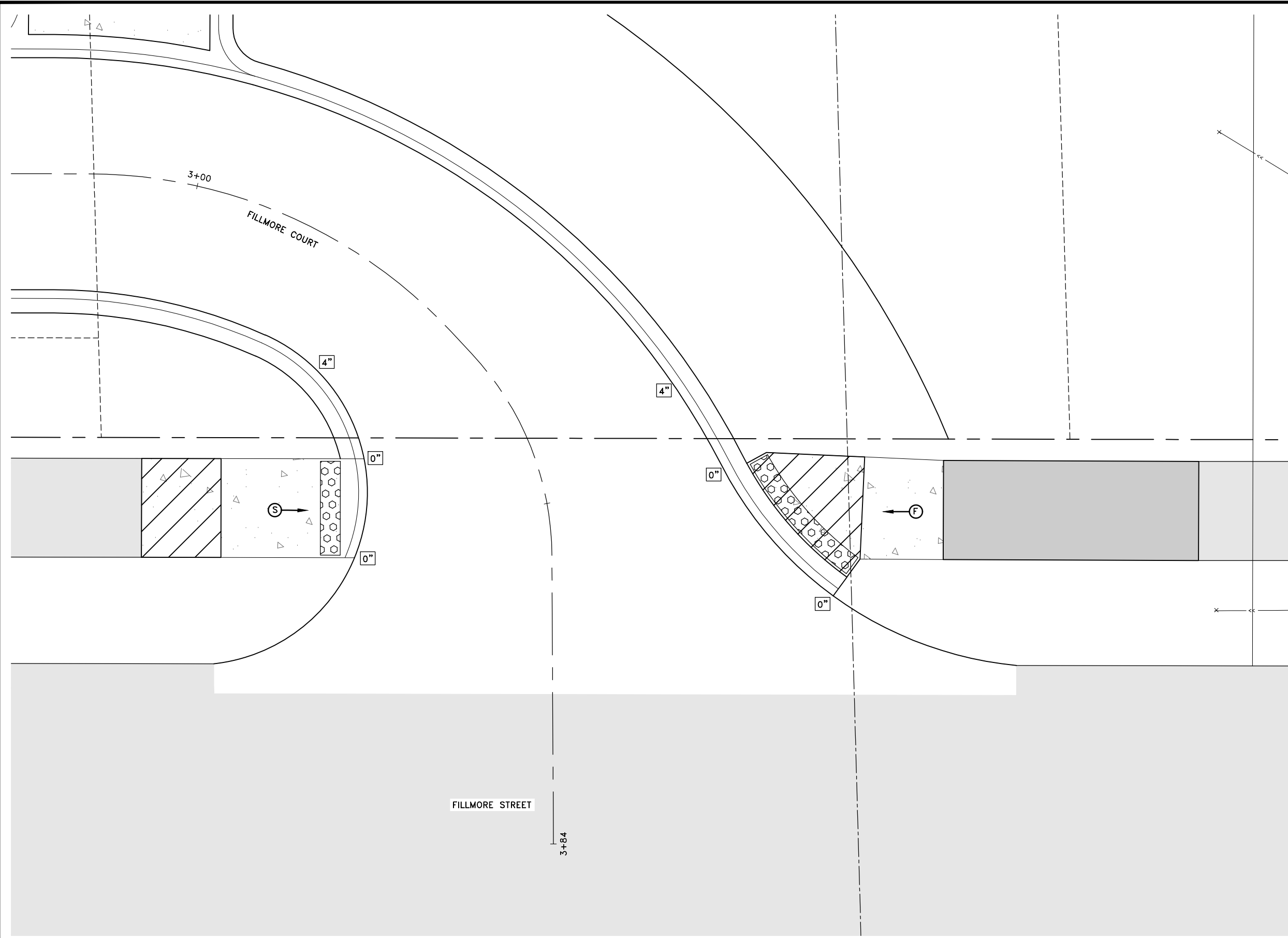






THE VILLAS AT FILLMORE THIRD ADDITION

RESTORATION PLAN  
 CITY OF ELK RIVER, MINNESOTA

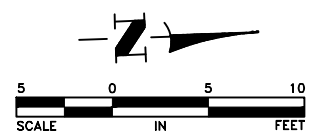
SHEET 12 OF 20 SHEETS

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- LEGEND**
-  LANDING AREA - 4'X4' MIN. DIMENSIONS. MAX. 2.0% CROSS SLOPE IN ALL DIRECTIONS.
  -  INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MIN. AND 8.3% MAX. IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
  -  INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
  -  CURB HEIGHT

**GENERAL NOTES:**  
1. SEE SHEETS 15-20 FOR PEDESTRIAN CURB RAMP DETAILS.



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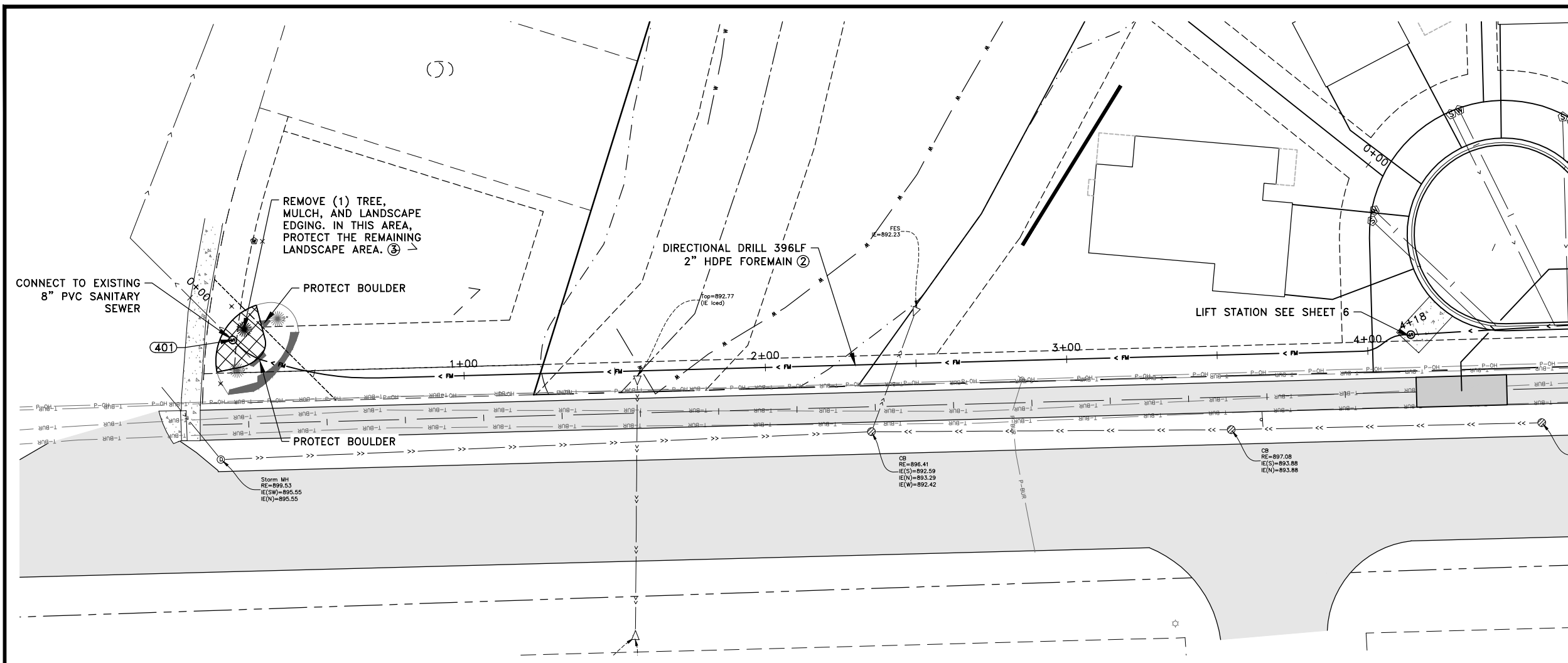
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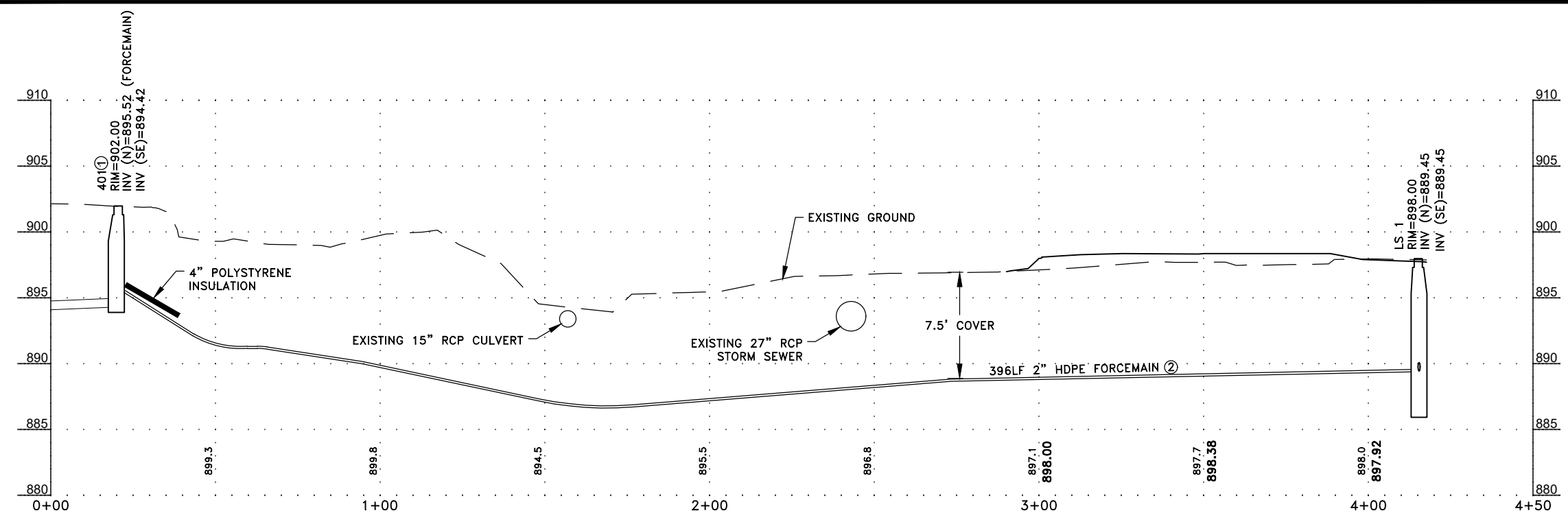
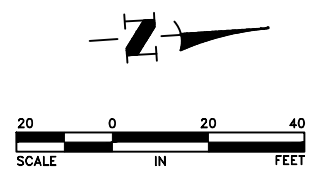
THE VILLAS AT FILLMORE THIRD ADDITION

PEDESTRIAN RAMP CONSTRUCTION PLAN  
 CITY OF ELK RIVER, MINNESOTA

SHEET 13 OF 20 SHEETS



- REFERENCE NOTES:
- ① CONSTRUCT MANHOLE PER CSP 4000.
  - ② MATERIAL OF FORCEMAIN PIPE SHALL BE HDPE, ANSI DR 11.0, 200 PSI, IPS(IN).
  - ③ ALL GREEN SPACE THAT IS DISTURBED IN THIS AREA DURING CONSTRUCTION SHALL BE RESTORED WITH 4" OF TOPSOIL, SEED SOUTHERN BOULEVARD, TYPE 1 FERTILIZER, AND MULCH TYPE 1. SEE SHEET 12 FOR RATE APPLICATIONS.



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CJJ

DRAWN BY:  
SGJ

CHECKED BY:  
TAE

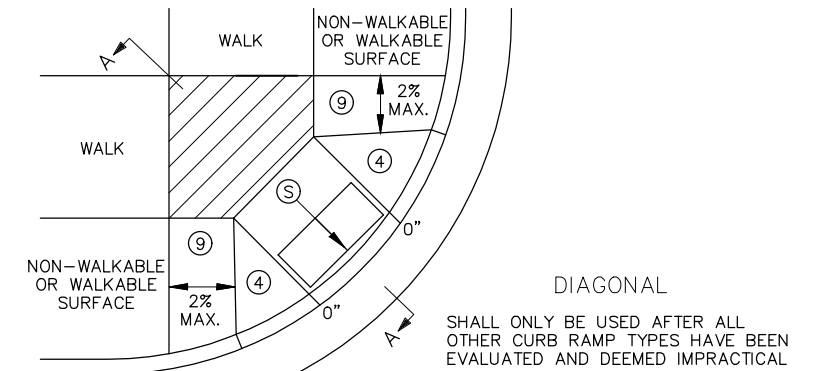
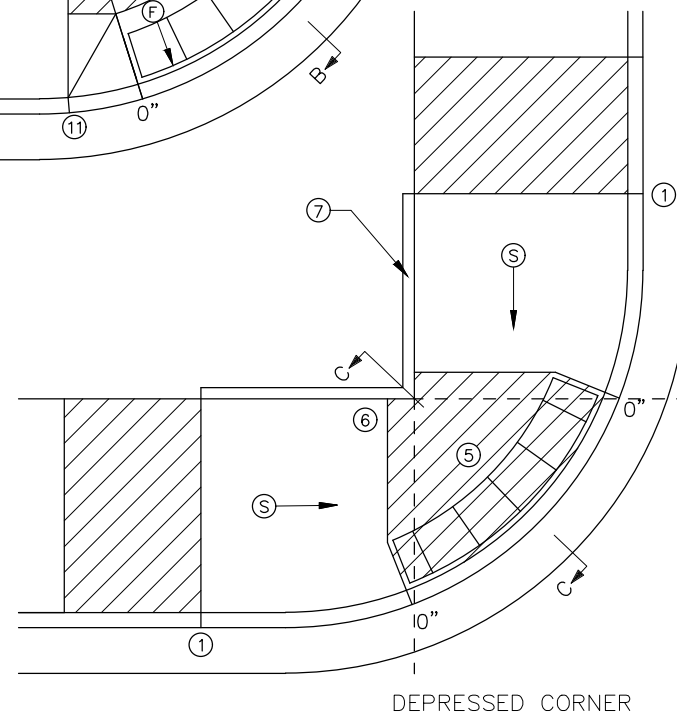
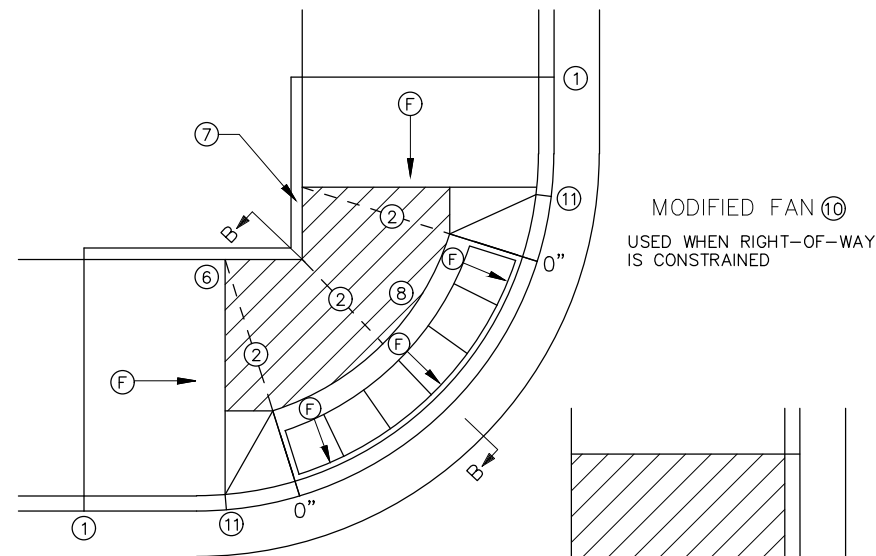
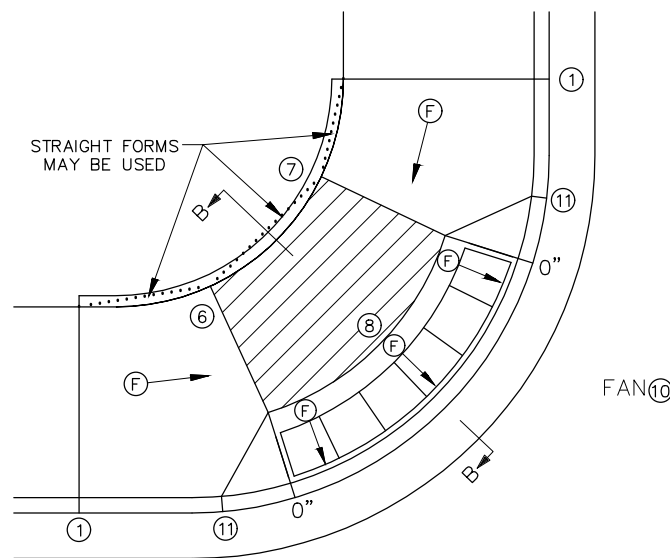
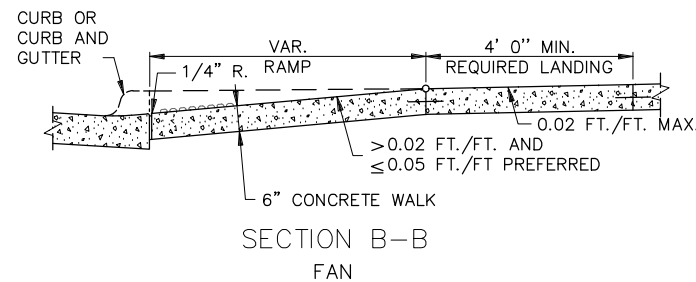
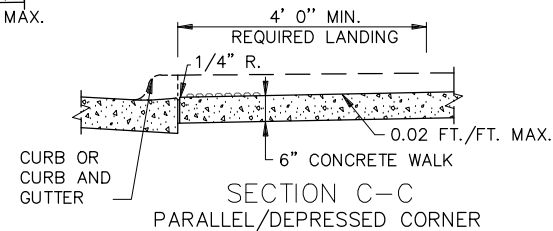
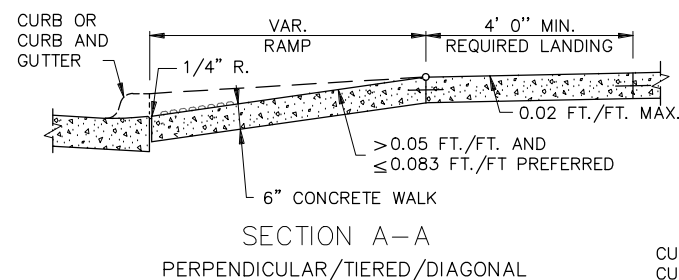
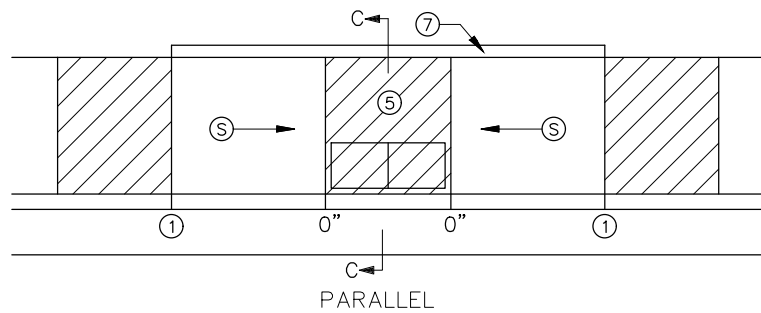
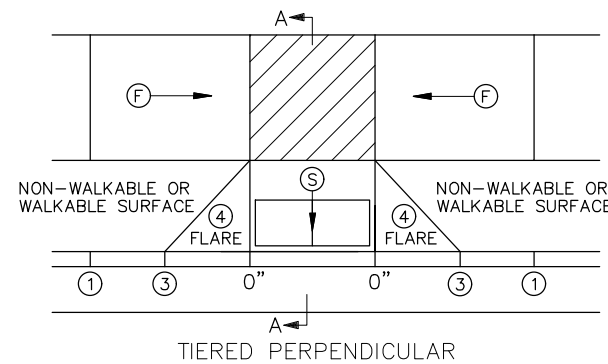
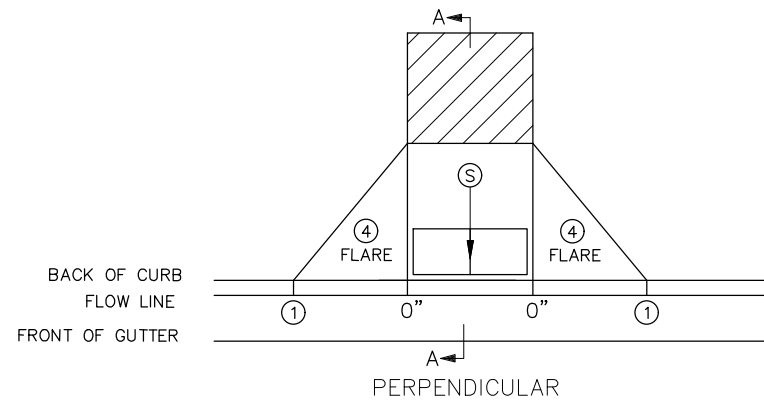


THE VILLAS AT FILLMORE THIRD ADDITION

FOREMAIN CONSTRUCTION PLAN  
 CITY OF ELK RIVER, MINNESOTA

SHEET 14 OF 20 SHEETS

Jan 24, 2025 11:10am K:\PRIVATE\213536\ENGINEERING\PLAN DWG\213536\_MNDOT PED RAMP DETAIL.dwg



NOTES:

- LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE GREATER THAN 2%.
- INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6" FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.
- SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30" OF VERTICAL RISE WHEN THE LONGITUDINAL RUNNING SLOPE IS GREATER THAN 5.0%.
- CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOPS OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.
- ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL, THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH. (EXCEPT AS STATED IN 6) BELOW.
- TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, ALL INITIAL LANDINGS AT A TOP OF A RAMPED SURFACE (RUNNING SLOPE GREATER THAN 2%) SHALL BE FORMED AND PLACED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 OF 6 FOR ALL SEPARATELY POURED INITIAL LANDINGS.
- WHEN SIDEWALK IS AT BACK OF CURB, TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE. MAINTAIN POSITIVE BOULEVARD DRAINAGE TO TOP OF CURB.
- ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.
- 4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER THE ENTIRE PAR WIDTH OF SHARED-USE PATHS AND THE ENTIRE PAR WIDTH OF THE WALK WITH THE EXCEPTION OF 3" MAXIMUM ON EACH OUTSIDE EDGE WHICH ENSURES THE DETECTABLE WARNINGS ARE ENCASED IN CONCRETE WHEN ADJACENT TO TURF. WHEN ADJACENT TO CONCRETE FLARES 0" - 3" OFFSET IS ALLOWED.
- WHEN DESIGNING OR ORDERING RECTANGULAR DETECTABLE WARNING SURFACES SHOULD BE 6" LESS THAN THE INCOMING PAR. ARC LENGTH OF THE RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.
- RECTANGULAR DETECTABLE WARNINGS SHALL BE SETBACK 3" FROM THE BACK OF CURB. RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB.

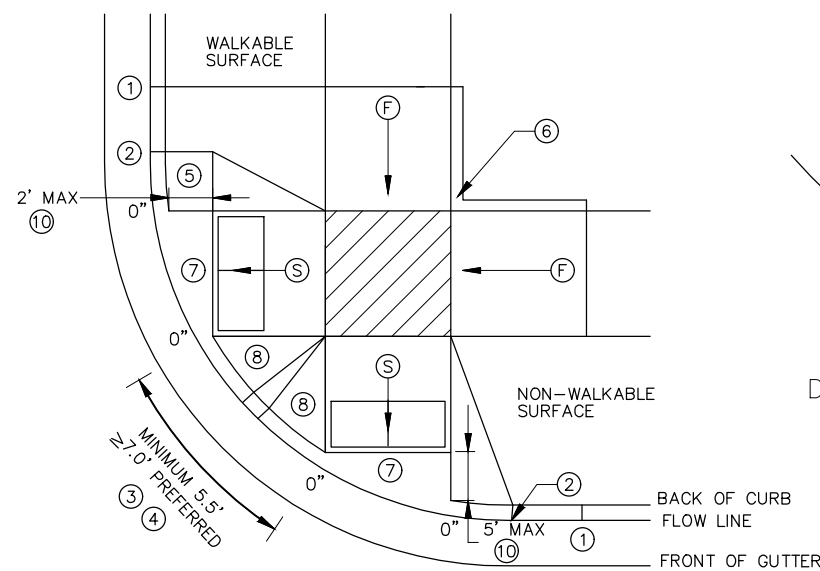
- 1 MATCH FULL HEIGHT CURB.
- 2 4' MINIMUM DEPTH LANDING REQUIRED ACROSS TOP OF RAMP.
- 3 3" HIGH CURB WHEN USING A 3' LONG RAMP, 4" HIGH CURB WHEN USING A 4' LONG RAMP.
- 4 SEE SHEET 4 OF 6, TYPICAL SIDE TREATMENT OPTIONS, FOR DETAILS ON FLARES AND RETURNED CURBS.
- 5 DETECTABLE WARNINGS MAY BE PART OF THE 4' X 4' MIN. LANDING AREA IF IT IS NOT FEASIBLE TO CONSTRUCT THE LANDING OUTSIDE OF THE DETECTABLE WARNING AREA.
- 6 THE GRADE BREAK SHALL BE PERPENDICULAR TO THE BACK OF WALK. THIS WILL ENSURE THAT THE GRADE BREAK IS PERPENDICULAR TO THE DIRECTION OF TRAVEL. (TYPICAL FOR ALL)
- 7 WHEN ADJACENT TO GRASS, GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS LESS THAN 5% RUNNING SLOPE SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
- 8 A 7' MIN TOP RADIUS GRADE BREAK IS REQUIRED TO BE CONSTRUCTIBLE.
- 9 PAVE FULL WALK WIDTH.
- 10 "S" SLOPES ON FANS SHALL ONLY BE USED WHEN ALL OTHER FEASIBLE OPTIONS HAVE BEEN EVALUATED AND DEEMED IMPRACTICAL.
- 11 INTERMEDIATE CURB HEIGHTS TAPER SHALL RISE AT 8-10% TO A MINIMUM 3" CURB HEIGHT. REDUCE INTERMEDIATE CURB HEIGHT TO 2+ INCHES IF NECESSARY TO MATCH ADJACENT BOULEVARD OR SIDEWALK GRADES.

LEGEND	
(S)	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
(F)	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
[Hatched Box]	LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PARS.
X"	CURB HEIGHT

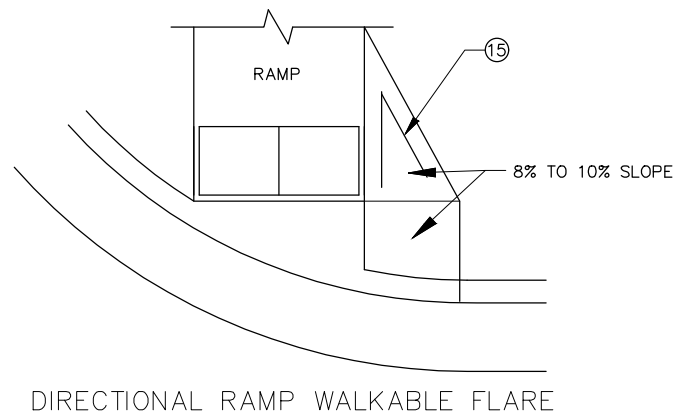
REVISION:  
APPROVED: 11-04-2021  
*Jeff J. Perkins*  
JEFFREY PERKINS  
OPERATIONS DIVISION

**m** MINNESOTA  
DEPARTMENT OF TRANSPORTATION  
STANDARD PLAN 5-297.250  
1 OF 6  
APPROVED: 11-04-2021  
REVISOR:  
*Tom Styrbicki*  
THOMAS STYRBICKI  
STATE DESIGN ENGINEER

PEDESTRIAN CURB RAMP DETAILS

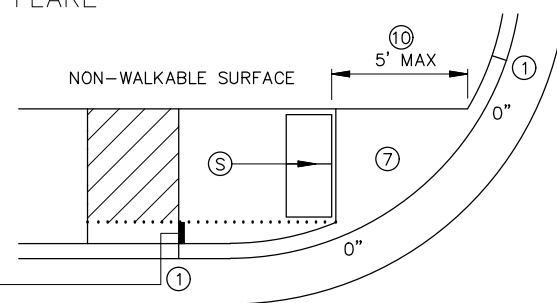


COMBINED DIRECTIONAL

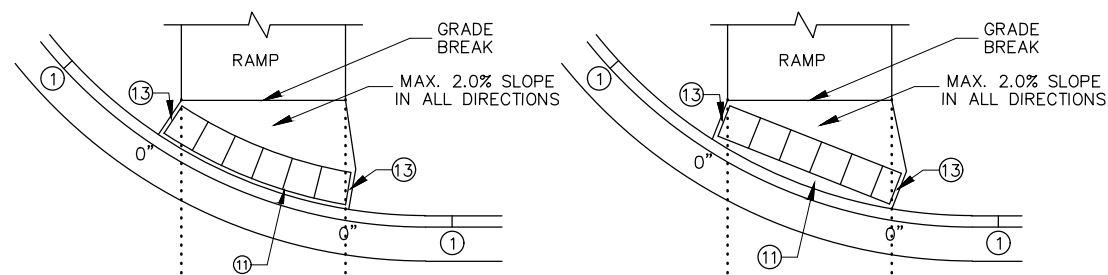


DIRECTIONAL RAMP WALKABLE FLARE

IF NON-CONCRETE BLVD. IS CONSTRUCTED AND IS LESS THAN 2' IN WIDTH AT TOP OF CURB TRANSITION, PAVE CONCRETE RAMP WIDTH TO ADJACENT BACK OF CURB.

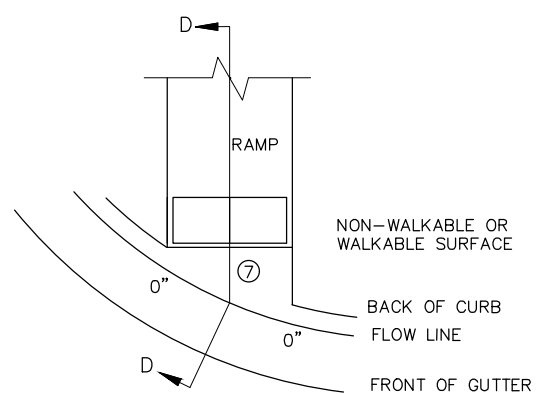


STANDARD ONE-WAY DIRECTIONAL ⑨

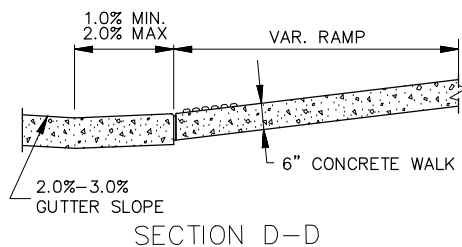


DETECTABLE WARNING PLACEMENT WHEN SETBACK CRITERIA IS EXCEEDED ⑫

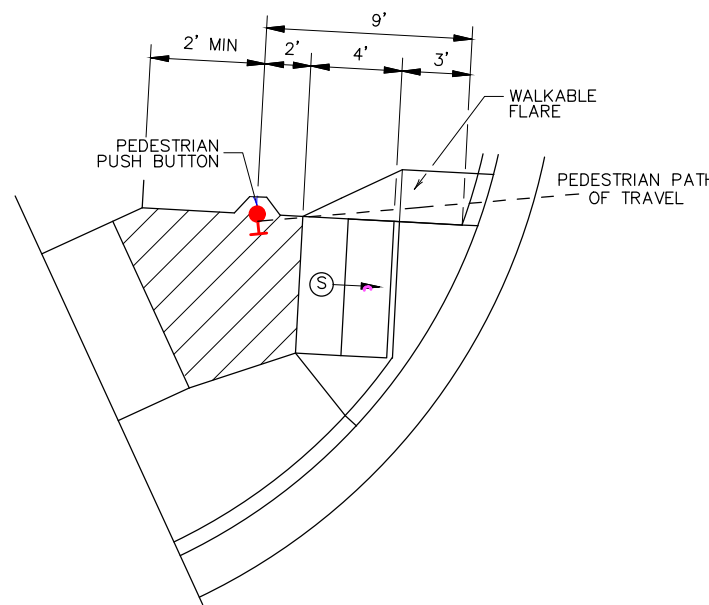
ONE-WAY DIRECTIONAL WITH DETECTABLE WARNING AT BACK OF CURB



CURB FOR DIRECTIONAL RAMPS ⑭



SECTION D-D



SEMI-DIRECTIONAL RAMP ③ ④ ⑨

3' DOME SETBACK, 4' LONG RAMP AND PUSH BUTTON 9' FROM THE BACK OF CURB  
 PRIMARILY USED FOR APS APPLICATIONS WHERE THE PAR DOES NOT CONTINUE PAST THE PUSH BUTTON (DEAD-END SIDEWALK)

NOTES:

LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE.

INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.

SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30' OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.

CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOP GRADE BREAK OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.

ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL. THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH.

TO ENSURE INITIAL RAMPS AND INITIAL LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS SHALL BE CAST SEPARATELY. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 AND THE ADA SPECIAL PROVISION (PROSECUTION OF WORK).

TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.

WHEN THE BOULEVARD IS 4' WIDE OR LESS, THE TOP OF CURB TAPER SHALL MATCH THE RAMP SLOPES TO REDUCE NEGATIVE BOULEVARD SLOPES FROM THE TOP BACK OF CURB TO THE PAR.

ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.

4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER THE ENTIRE PAR WIDTH OF SHARED-USE PATHS AND THE ENTIRE PAR WIDTH OF THE WALK WITH THE EXCEPTION OF 3" MAXIMUM ON EACH OUTSIDE EDGE WHICH ENSURES THE DETECTABLE WARNINGS ARE ENCASED IN CONCRETE WHEN ADJACENT TO TURF. WHEN ADJACENT TO CONCRETE FLARES 0' - 3' OFFSET IS ALLOWED.

WHEN DESIGNING OR ORDERING RECTANGULAR DETECTABLE WARNING SURFACES SHOULD BE 6" LESS THAN THE INCOMING PAR. ARC LENGTH OF THE RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.

RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB. SEE NOTES ⑩ & ⑪ FOR INFORMATION REGARDING RECTANGULAR DETECTABLE WARNING PLACEMENT.

- ① MATCH FULL CURB HEIGHT.
- ② 3" HIGH CURB WHEN USING A 3' LONG RAMP  
4" HIGH CURB WHEN USING A 4' LONG RAMP.
- ③ 3" MINIMUM CURB HEIGHT (5.5' MIN. DISTANCE REQUIRED BETWEEN DOMES)  
4" PREFERRED (7' MIN. DISTANCE REQUIRED BETWEEN DOMES).
- ④ THE "BUMP" IN BETWEEN THE RAMPS SHOULD NOT BE IN THE PATH OF TRAVEL FOR COMBINED DIRECTIONAL RAMPS. IF THIS OCCURS MODIFY THE RAMP LOCATION OR SWITCH RAMP TO A FAN/DEPRESSED CORNER.
- ⑤ WHEN USING CONCRETE PAVED FLARES ON THE OUTSIDE OF DIRECTIONAL RAMPS, AND ADJACENT TO A WALKABLE SURFACE, DIRECTIONAL RAMP FLARES SHALL BE USED. SEE THE DETAIL ON THIS SHEET.
- ⑥ GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
- ⑦ MAX. 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK AND DRAIN TO FLOW LINE. SHALL BE CONSTRUCTED INTEGRAL WITH CURB AND GUTTER.
- ⑧ 8% TO 10% WALKABLE FLARE.
- ⑨ PLACE DOMES AT THE BACK OF CURB WHEN ALLOWABLE SETBACK CRITERIA IS EXCEEDED.
- ⑩ FRONT EDGE OF DETECTABLE WARNING SHALL BE SET BACK 2' MAXIMUM WHEN ADJACENT TO WALKABLE SURFACE, AND 5' MAXIMUM WHEN ADJACENT TO NON-WALKABLE SURFACE WITH ONE CORNER SET 3' FROM BACK OF CURB. A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.
- ⑪ RECTANGULAR DETECTABLE WARNINGS MAY BE SETBACK UP TO 9" FROM THE BACK OF CURB WITH CORNERS SET 3" FROM BACK OF CURB. IF 9" SETBACK IS EXCEEDED USE RADIAL DETECTABLE WARNINGS.
- ⑫ FOR DIRECTIONAL RAMPS WITH THE DETECTABLE WARNINGS PLACED AT THE BACK OF CURB, THE DETECTABLE WARNINGS SHALL COVER THE ENTIRE WIDTH OF THE WALK/PATH. THIS ENSURES A DETECTABLE EDGE AND HELPS ELIMINATE THE CURB TAPER OBSTRUCTING THE PATH OF PEDESTRIAN TRAVEL.
- ⑬ THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE BACK OF CURB. MAINTAIN 3" BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- ⑭ TO BE USED FOR ALL DIRECTIONAL RAMPS, EXCEPT WHERE DOMES ARE PLACED ALONG THE BACK OF CURB.
- ⑮ PLACE 2 NO. 4 BARS 4 INCHES FROM SIDE OF FORMS WITH A MINIMUM 2 INCHES OF CONCRETE COVER ALONG EACH SIDE OF FLARE (INCIDENTAL).

LEGEND	
THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.	
Ⓢ	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
Ⓣ	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
▨	LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PAR.
x"	CURB HEIGHT

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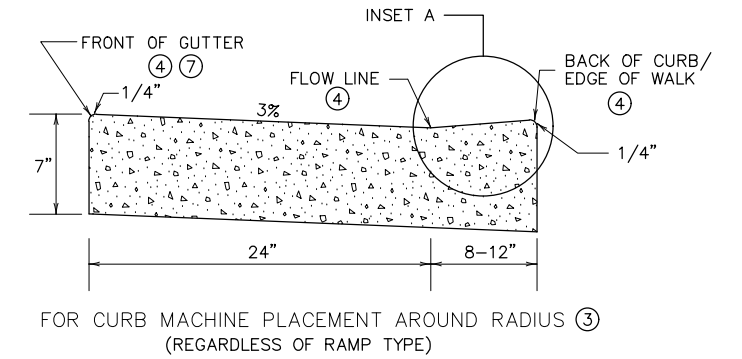
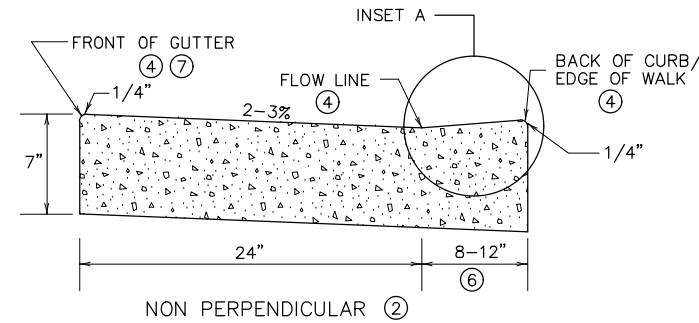
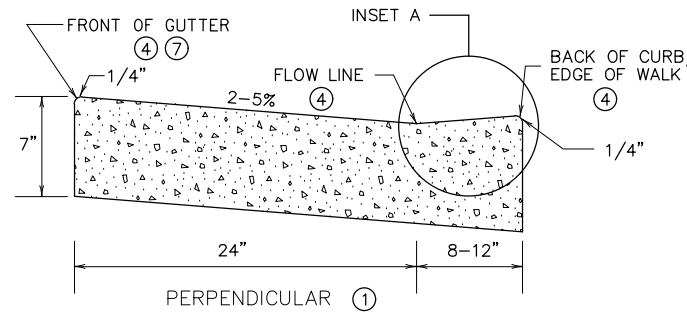
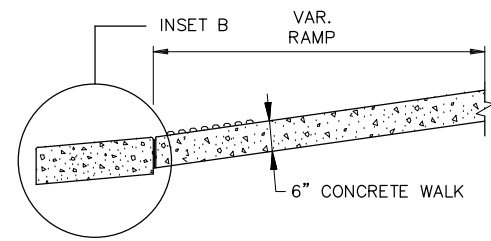
REVISION:  
 APPROVED: 11-04-2021  
*Jeff J. Pel...*  
 JEFFREY PERKINS  
 OPERATIONS DIVISION



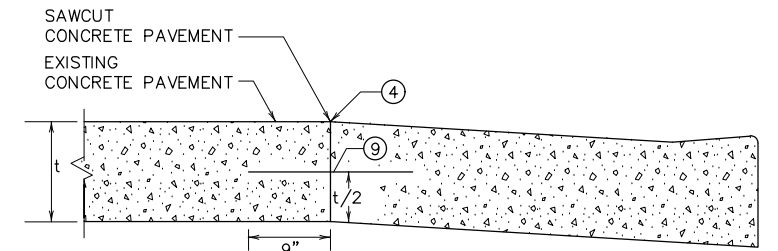
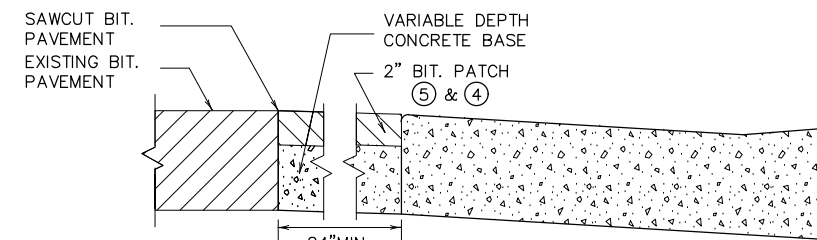
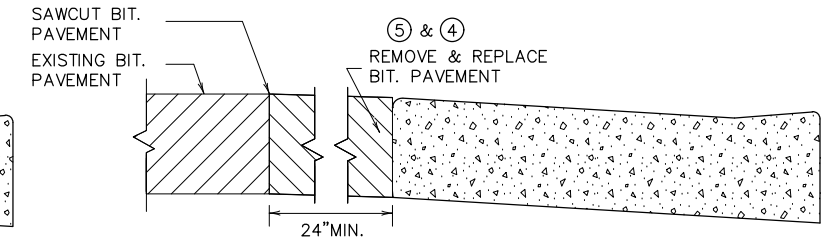
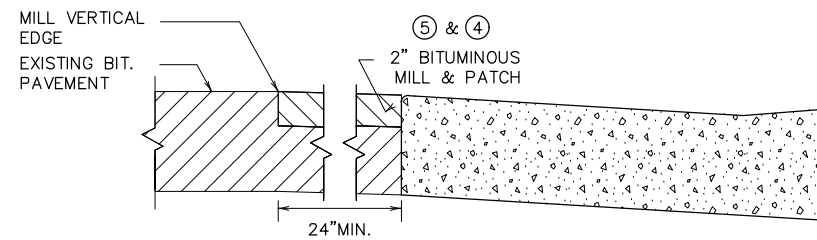
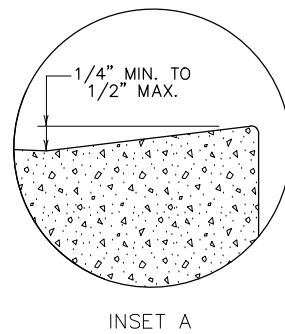
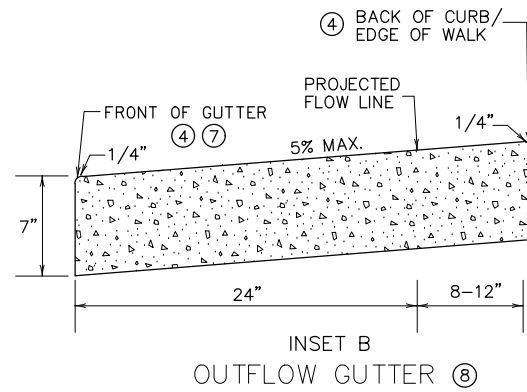
STANDARD PLAN 5-297.250 | 2 OF 6  
 APPROVED: 11-04-2021  
 REVISOR:  
 THOMAS STYRBICKI  
 STATE DESIGN ENGINEER  
 SP VALUE

PEDESTRIAN CURB RAMP DETAILS

Jan 24, 2025 - 11:10am  
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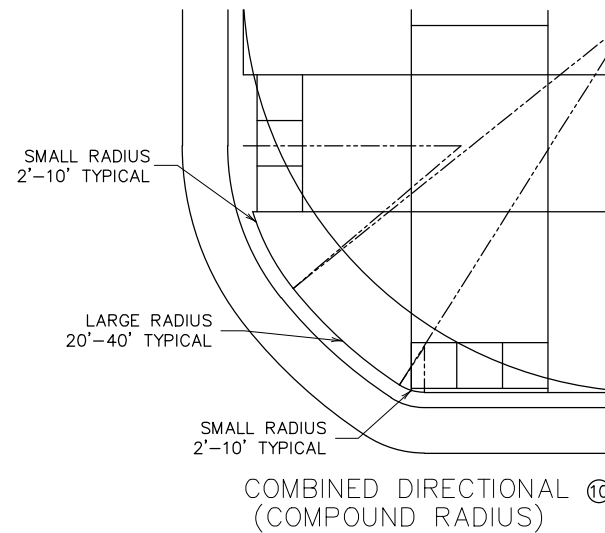
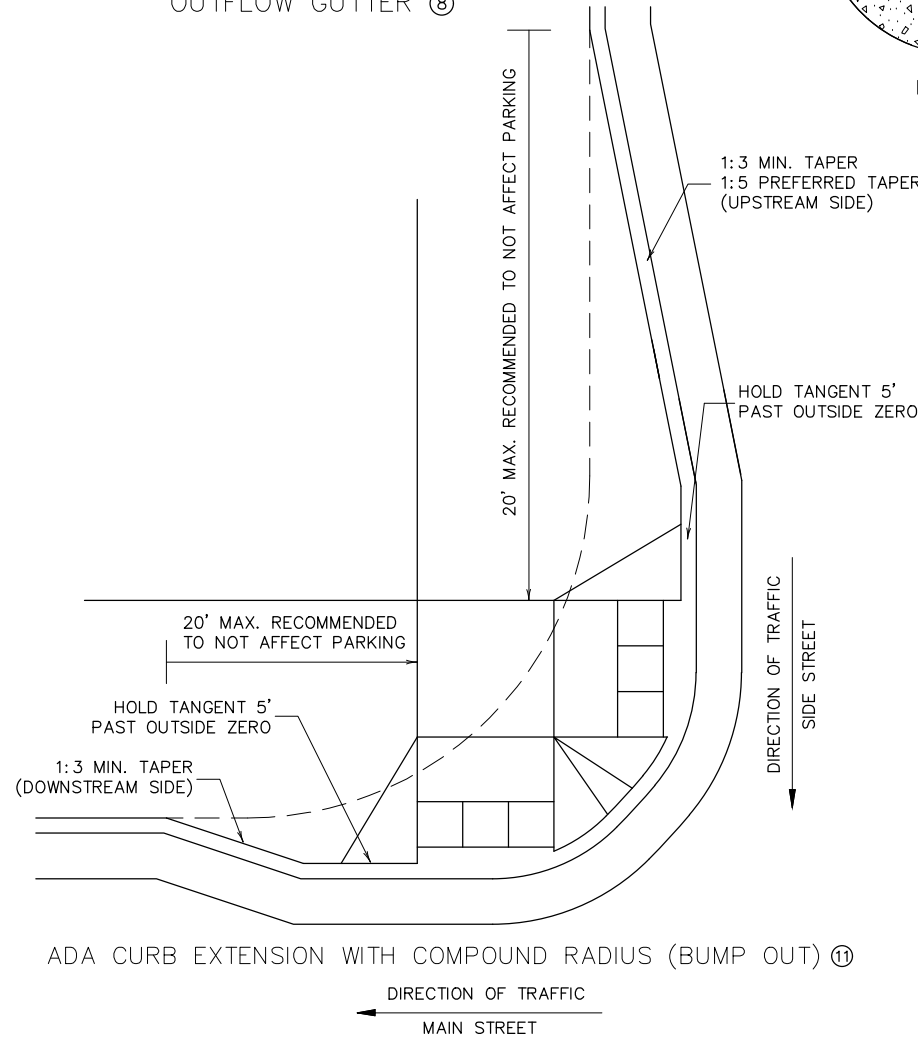


PEDESTRIAN ACCESS ROUTE CURB & GUTTER DETAIL



ONLY ALLOWED PER ENGINEER'S APPROVAL

PAVEMENT TREATMENT OPTIONS IN FRONT OF CURB & GUTTER  
FOR USE ON CURB RAMP RETROFITS



NOTES:

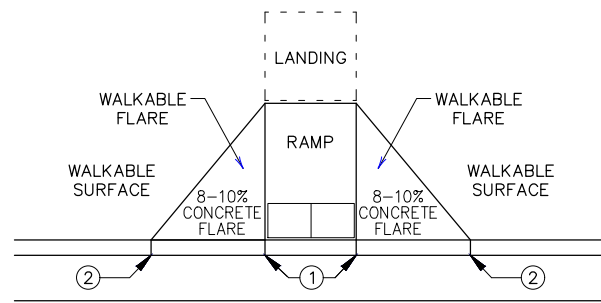
- POSITIVE FLOW LINE DRAINAGE SHALL BE MAINTAINED THROUGH THE PEDESTRIAN ACCESS ROUTE (PAR) AT A 2% MAXIMUM. NO PONDING SHALL BE PRESENT IN THE PAR.
- ANY VERTICAL LIP THAT OCCURS AT THE FLOW LINE SHALL NOT BE GREATER THAN 1/4 INCH.
- (1) FOR USE AT CURB CUTS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: PERPENDICULAR, TIERED PERPENDICULAR, PARALLEL, AND DIAGONAL RAMP.
- (2) FOR USE AT CURB RAMP WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED NON PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: FANS & DEPRESSED CORNERS.
- (3) BEGIN GUTTER SLOPE TRANSITION 10' OUTSIDE OF ALL CURB RAMP.
- (4) THERE SHALL BE NO VERTICAL DISCONTINUITIES GREATER THAN 1/4".
- (5) ELEVATION CHANGE TAKES PLACE FROM THE EXISTING TO NEW FRONT OF GUTTER. PATCH IS USED TO MATCH THE NEW GUTTER FACE INTO THE EXISTING ROADWAY.
- (6) VARIABLE WIDTH FOR DIRECTIONAL CURB APPLICATIONS. SEE SHEET 2 FOR DIRECTIONAL CURB SLOPE REQUIREMENTS.
- (7) TOP FRONT OF GUTTER SHALL BE CONSTRUCTED FLUSH WITH PROPOSED ADJACENT PAVEMENT ELEVATION. TOP 1.5" OF THE GUTTER FACE MUST BE A FORMED EDGE. PAR GUTTER SHALL NOT BE OVERLAID.
- (8) SHOULD BE USED AT VERTICALLY CONSTRAINED AREAS WHEN AT A DRAINAGE HIGH POINT OR SUPER ELEVATED ROADWAY SEGMENTS.
- (9) DRILL AND GROUT NO. 4 EPOXY-COATED 18" LONG TIE BARS AT 30" CENTER TO CENTER INTO EXISTING CONCRETE PAVEMENT 1' MINIMUM FROM ALL JOINTS.
- (10) HELPS PROVIDE TWO SEPARATE RAMPS, REDUCES THE DOME SETBACK LENGTH AND MINIMIZES DIRECTIONAL CURB. THIS RADIUS DESIGN CLOSELY FOLLOWS THE TURNING VEHICLE PATH WHILE OPTIMIZING CURB RAMP LENGTH.
- (11) CURB EXTENSIONS SHOULD BE USED IN VERTICALLY CONSTRAINED AREAS, USUALLY IN DOWNTOWN ROADWAY SEGMENTS WHERE ON-STREET PARKING IS AVAILABLE. CURB EXTENSIONS SHOULD BE CONSIDERED FOR APS INTERSECTIONS WHERE SPACE IS LIMITED. PUSH BUTTONS MUST MEET APS CRITERIA AS DESCRIBED IN THE PUSH BUTTON LOCATION DETAIL SHEET.

REVISION:  
APPROVED: 11-04-2021  
*Jeff J. Perkins*  
JEFFREY PERKINS  
OPERATIONS DIVISION

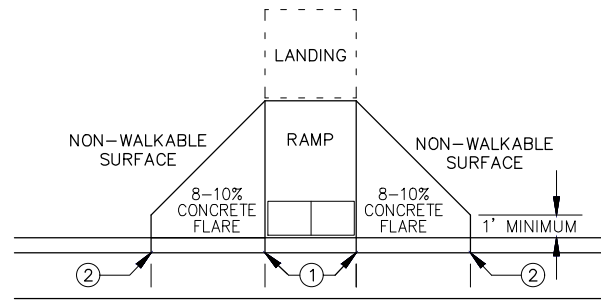


STANDARD PLAN 5-297.250 3 OF 6  
*Tom Styrbicki*  
THOMAS STYRBICKI  
STATE DESIGN ENGINEER  
APPROVED: 11-04-2021  
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SP VALUE

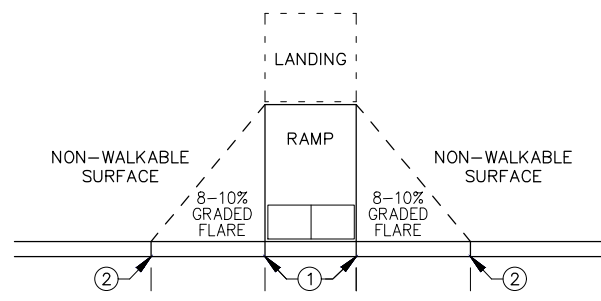
PEDESTRIAN CURB RAMP DETAILS



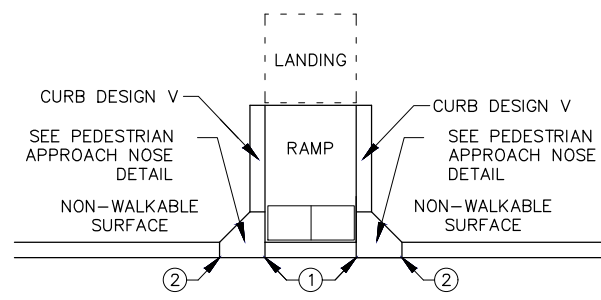
PAVED FLARES  
ADJACENT TO WALKABLE SURFACE



PAVED FLARES  
ADJACENT TO NON-WALKABLE SURFACE

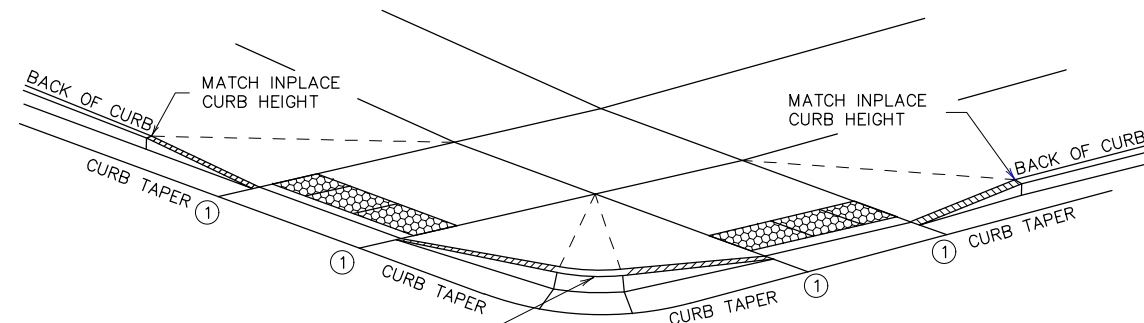


GRADED FLARES



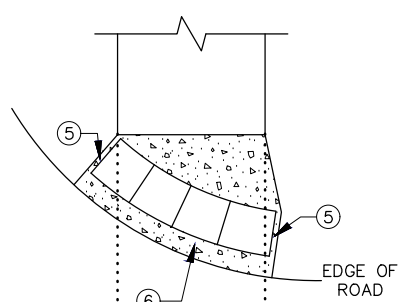
RETURNED CURB ④

TYPICAL SIDE TREATMENT OPTIONS ③ ⑩

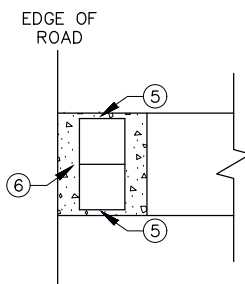


3" MINIMUM CURB HEIGHT, 4" PREFERRED  
(MEASURED AT FRONT FACE OF CURB)  
FOR A MIN. 6" LENGTH (MEASURED ALONG FLOW LINE)

DETECTABLE EDGE WITH CURB AND GUTTER ⑦

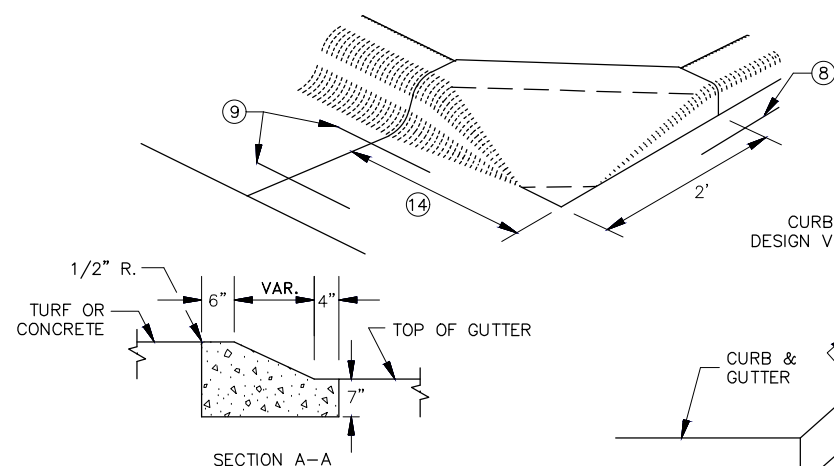


RADIAL DETECTABLE WARNING

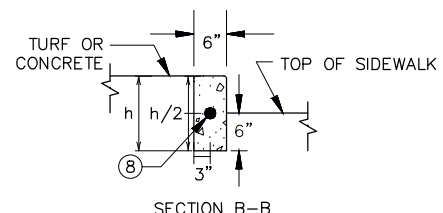


RECTANGULAR DETECTABLE WARNING

DETECTABLE EDGE WITHOUT CURB AND GUTTER

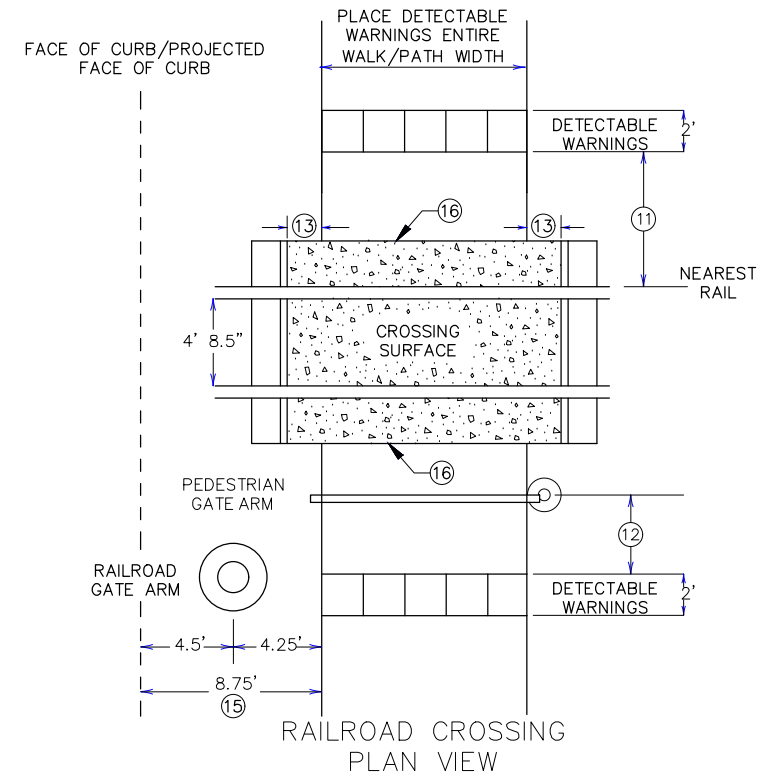


SECTION A-A



SECTION B-B

PEDESTRIAN APPROACH NOSE DETAIL (FOR RETURNED CURB SIDE TREATMENT)



RAILROAD CROSSING PLAN VIEW

- NOTES:
- INTERMEDIATE CURB HEIGHTS TAPER SHALL RISE AT 8-10% TO A MINIMUM 3 INCH CURB HEIGHT. INCREASE CURB TAPER LENGTH AT LESS THAN 8% OR REDUCE INTERMEDIATE CURB HEIGHT TO 2+ INCHES IF NECESSARY TO MATCH ADJACENT BOULEVARD OR SIDEWALK GRADES.
  - SEE STANDARD PLATE 7038 AND THIS SHEET FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.
  - A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.
  - CONCRETE FLARE LENGTHS ADJACENT TO NON-WALKABLE SURFACES SHOULD BE LESS THAN 8' LONG MEASURED ALONG THE RAMPS FROM THE BACK OF CURB.
  - ① 0" CURB HEIGHT. SEE INSET A ON SHEET 3 OF 6.
  - ② FULL CURB HEIGHT.
  - ③ SIDE TREATMENTS ARE APPLICABLE TO ALL RAMP TYPES AND SHOULD BE IMPLEMENTED AS NEEDED AS FIELD CONDITIONS DICTATE. THE ENGINEER SHALL DETERMINE THE RAMP SIDE TREATMENTS BASED ON MAINTENANCE OF BOTH ROADWAY AND SIDEWALK, ADJACENT PROPERTY CONSIDERATIONS, AND MITIGATING CONSTRUCTION IMPACTS.
  - ④ TYPICALLY USED FOR MEDIANS AND ISLANDS.
  - ⑤ WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE EDGE OF ROADWAY. MAINTAIN 3" MAX. BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
  - ⑥ IF NO CURB AND GUTTER IS PLACED IN RURAL SECTIONS, DETECTABLE WARNINGS SHALL BE PLACED 1' FROM THE EDGE OF BITUMINOUS ROADWAY AND/OR BITUMINOUS SHARED-USE PATH TO PROVIDE VISUAL CONTRAST.
  - ⑦ ALL CONSTRUCTED CURBS MUST HAVE A CONTINUOUS DETECTABLE EDGE FOR THE VISUALLY IMPAIRED. THIS DETECTABLE EDGE REQUIRES DETECTABLE WARNINGS WHEREVER THERE IS ZERO-INCH HIGH CURB. CURB TAPERS ARE CONSIDERED A DETECTABLE EDGE WHEN THE TAPER STARTS WITHIN 3" OF THE EDGE OF THE DETECTABLE WARNINGS AND UNIFORMLY RISES TO A 3-INCH MINIMUM CURB HEIGHT. ANY CURB NOT PART OF A CURB TAPER AND LESS THAN 3 INCHES IN HEIGHT IS NOT CONSIDERED A DETECTABLE EDGE AND THEREFORE IS NOT COMPLIANT WITH ACCESSIBILITY STANDARDS.
  - ⑧ DRILL AND GROUT 1 - NO. 4 12" LONG REINFORCEMENT BAR (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE V CURB.
  - ⑨ DRILL AND GROUT 2 - NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE CURB AND GUTTER.
  - ⑩ SIDE TREATMENT EXAMPLES SHOWN ARE WHEN THE INITIAL LANDING IS APPROXIMATELY LEVEL WITH THE FULL HEIGHT CURB (I.E. 6' LONG RAMP FOR 6" HIGH CURB). WHEN THE INITIAL LANDING IS MORE THAN 1" BELOW FULL HEIGHT CURB REFER TO SHEETS 1 & 2 TO MODIFY THE CURB HEIGHT TAPERS AND MAINTAIN POSITIVE BOULEVARD DRAINAGE. CONSTRUCT THESE TAPER AT 0"-3" AT 8-10%, THEN LESS THAN 5% FROM 3" CURB TO FULL CURB HEIGHT.
  - ⑪ NEAREST EDGE OF DETECTABLE WARNING SURFACES SHALL BE PLACED 12' MINIMUM TO 15' MAXIMUM FROM THE NEAREST RAIL. FOR SKEWED RAILWAYS IN NO INSTANCE SHALL THE DETECTABLE WARNING BE CLOSER THAN 12' MEASURED PERPENDICULAR TO THE NEAREST RAIL.
  - ⑫ WHEN PEDESTRIAN GATES ARE PROVIDED, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE SIDE OF THE GATES OPPOSITE THE RAIL, 2' FROM THE APPROACHING SIDE OF THE GATE ARM. THIS CRITERIA GOVERNS OVER NOTE ⑪.
  - ⑬ CROSSING SURFACE SHALL EXTEND 2' MINIMUM PAST THE OUTSIDE EDGE OF WALK OR SHARED-USE PATH.
  - ⑭ 3' FOR MEDIANS AND SPLITTER ISLANDS. NOSE CAN BE REDUCED TO 2' ON FREE RIGHT ISLANDS.
  - ⑮ SIDEWALK TO BE PLACED 8.75' MIN. FROM THE FACE OF CURB/PROJECTED FACE OF CURB. THIS ENSURES MIN. CLEARANCE BETWEEN THE SIDEWALK AND GATE ARM COUNTERWEIGHT SUPPORTS.
  - ⑯ CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPECIFICATION 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE.

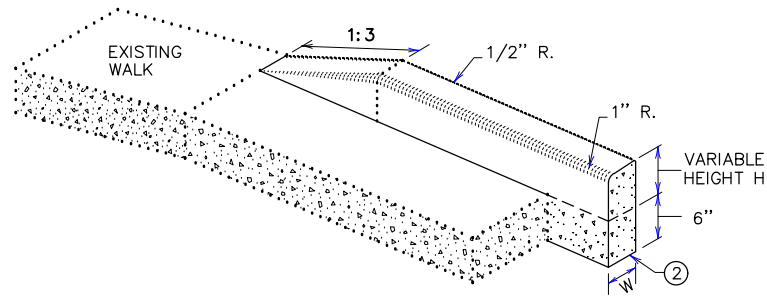
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OPERATIONS DIVISION

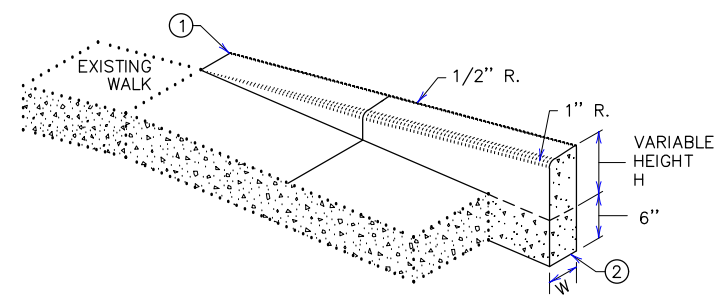
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DEPARTMENT OF TRANSPORTATION  
STANDARD PLAN 5-297.250 4 OF 6  
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REVISOR:  
THOMAS STYRBICKI  
STATE DESIGN ENGINEER  
SP VALUE

PEDESTRIAN CURB RAMP DETAILS

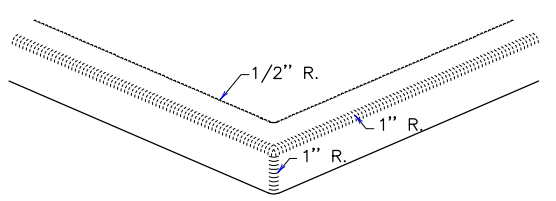
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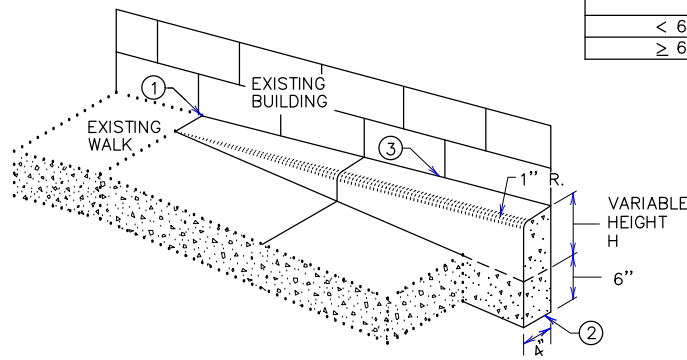
V CURB ADJACENT TO LANDSCAPE  
CURB WITHIN SIDEWALK LIMITS



V CURB ADJACENT TO LANDSCAPE  
CURB OUTSIDE SIDEWALK LIMITS

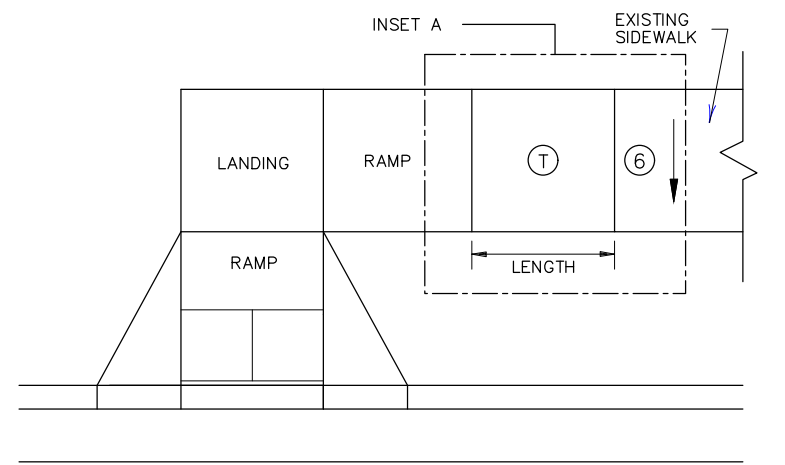


V CURB INTERSECTION

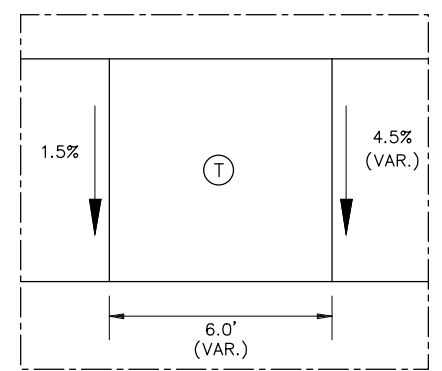


V CURB ADJACENT TO BUILDING  
OR BARRIER

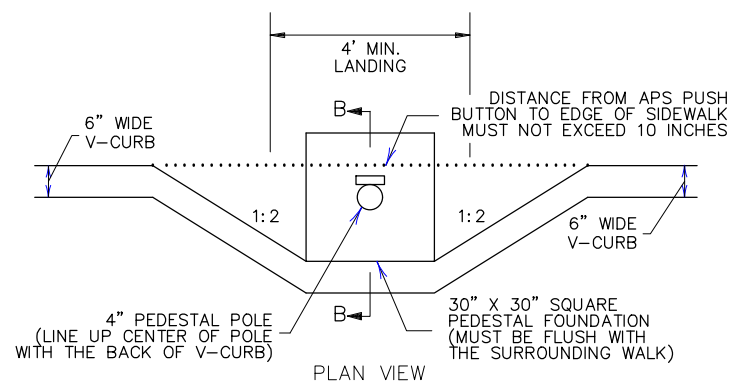
CONCRETE CURB DESIGN V	
CURB HEIGHT H	CURB WIDTH W
< 6"	4"
≥ 6"	6"



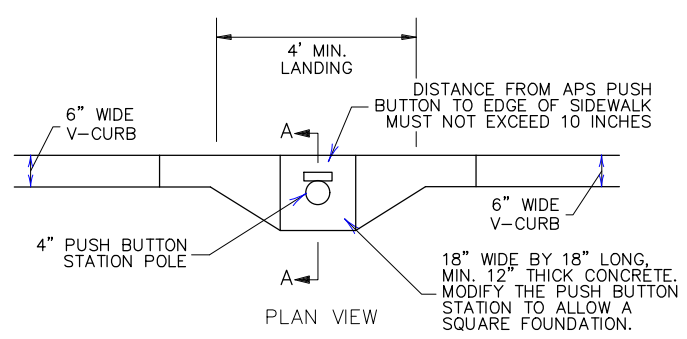
TRANSITION PANEL ④ ⑤



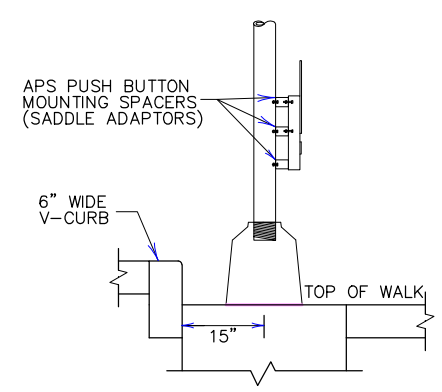
INSET A



PLAN VIEW

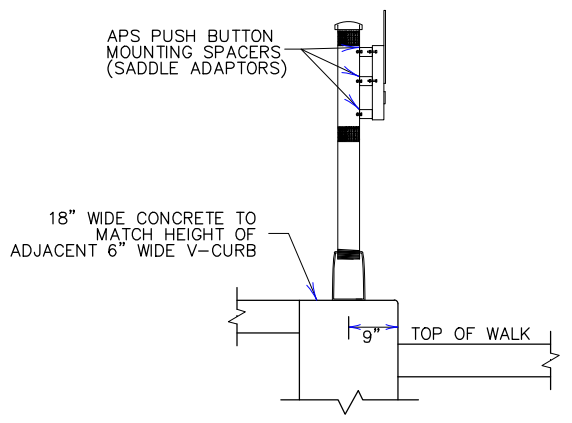


PLAN VIEW



SECTION B-B

SIGNAL PEDESTAL & PUSH BUTTON (V-CURB)



SECTION A-A

PUSH BUTTON STATION (V-CURB)

NOTES:

- A WALKABLE FLARE IS AN 8-10% CONCRETE FLARE THAT IS REQUIRED WHEN THE FLARE IS ADJACENT TO A WALKABLE SURFACE, OR WHEN THE PEDESTRIAN PATH OF TRAVEL OF A PUSH BUTTON TRAVERSES THE FLARE.
- ALL V CURB CONTRACTION JOINTS SHALL MATCH CONCRETE WALK JOINTS.
- WHERE RIGHT-OF-WAY ALLOWS, USE OF V CURB SHOULD BE MINIMIZED. GRADING ADJACENT TURF OR SLOPING ADJACENT PAVEMENT IS PREFERRED.
- V CURB SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS.
- V CURB NEXT TO BUILDING SHALL BE A 4" WIDTH AND SHALL MATCH PREVIOUS TOP OF SIDEWALK ELEVATIONS.
- ① END TAPERS AT TRANSITION SECTION SHALL MATCH INPLACE SIDEWALK GRADES.
- ② ALL V CURB SHALL MATCH BOTTOM OF ADJACENT WALK.
- ③ CONSTRUCT USING APPROVED EXPANSION MATERIAL PER MNDOT TYPE A-E EXPANSION. LEAVE A MINIMUM 1/2" TOP GAP AND SEAL WITH MNDOT APPROVED SILICONE PER MNDOT SPEC 3722.
- ④ THE MAX. RATE OF CROSS SLOPE TRANSITIONING IS 1' LINEAR FOOT OF SIDEWALK PER HALF PERCENT CROSS SLOPE. WHEN PAR WIDTH IS GREATER THAN 6' OR THE RUNNING SLOPE IS GREATER THAN 5%, DOUBLE THE CALCULATED TRANSITION LENGTH.
- ⑤ TRANSITION PANELS ARE TO ONLY BE USED AFTER THE RAMP, OR IF NEEDED, LANDING ARE AT THE FULL CURB HEIGHT (TYPICAL SECTION).
- ⑥ EXISTING CROSS SLOPE GREATER THAN 2.0%.

LEGEND	
↓	THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.
Ⓢ	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
▨	LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PARS.
Ⓣ	TRANSITION PANEL(S) - TO BE USED FOR TRANSITIONING THE CROSS-SLOPE OF A RAMP TO THE EXISTING WALK CROSS-SLOPE. RATE OF TRANSITION SHOULD BE 0.5% PER 1 LINEAR FOOT OF WALK. SEE THIS SHEET FOR ADDITIONAL INFORMATION.

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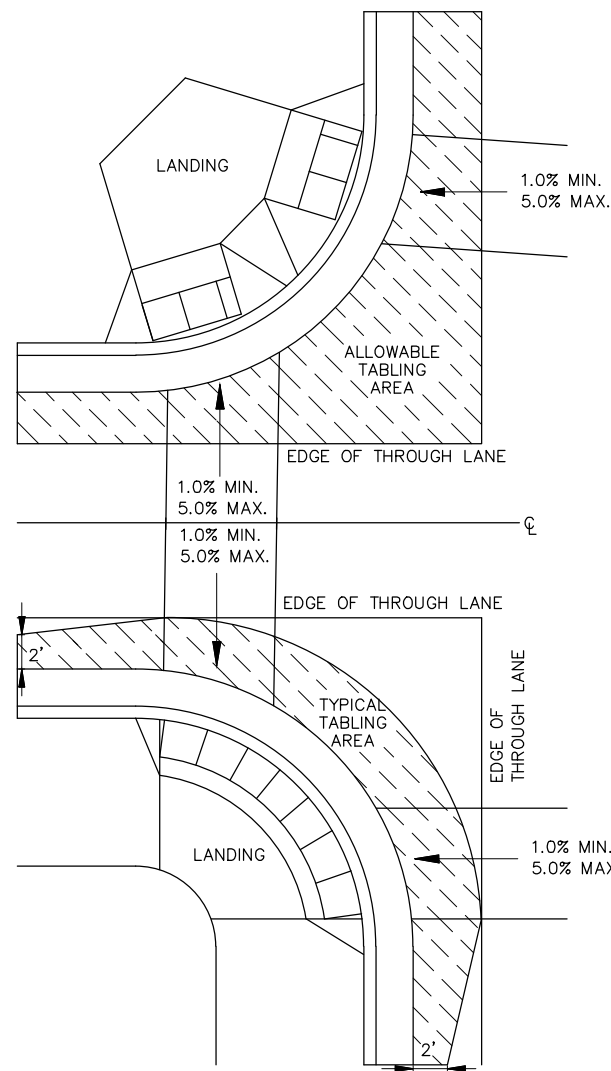
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APPROVED: 11-04-2021  
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JEFFREY PERKINS  
OPERATIONS DIVISION

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DEPARTMENT  
OF  
TRANSPORTATION

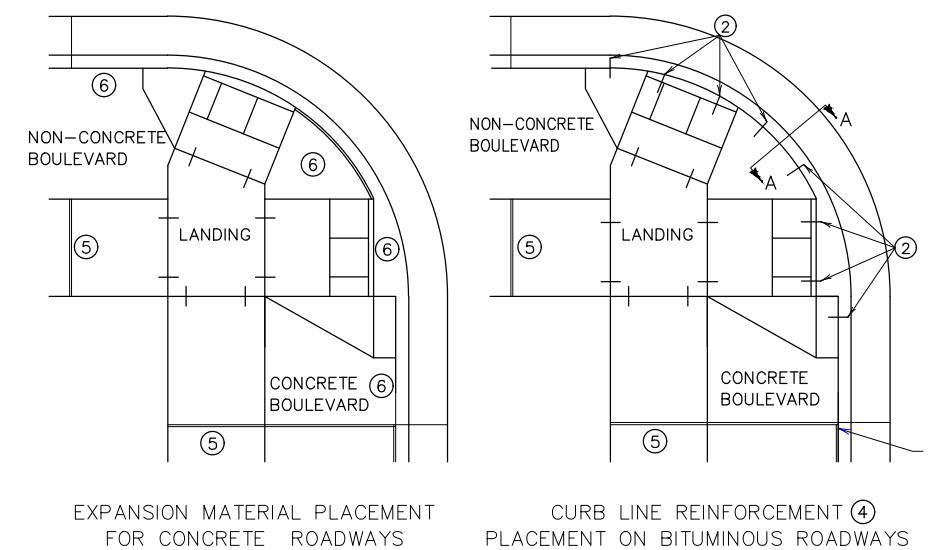
STANDARD PLAN 5-297.250 5 OF 6  
*Tom Styrbicki*  
THOMAS STYRBICKI  
STATE DESIGN ENGINEER  
APPROVED: 11-04-2021  
REVISED:  
SP VALUE

PEDESTRIAN CURB RAMP DETAILS

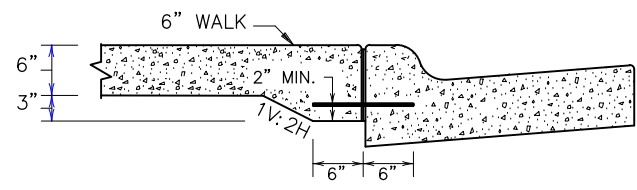
SHEET NO. 19 OF 20 SHEETS



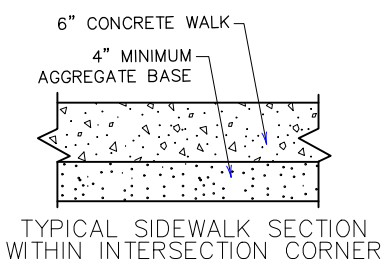
CURB LINE AND ROAD CROSSING ADJUSTMENTS



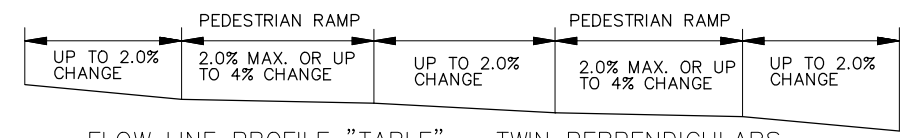
EXPANSION MATERIAL PLACEMENT FOR CONCRETE ROADWAYS CURB LINE REINFORCEMENT ④ PLACEMENT ON BITUMINOUS ROADWAYS



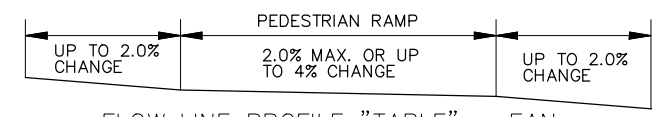
SECTION VIEW A-A THICKENED SECTION THROUGH CURB RAMP FLARES



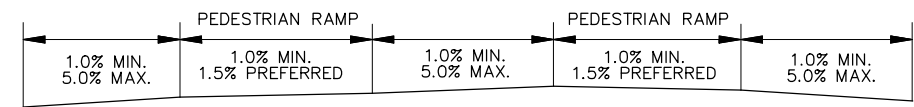
TYPICAL SIDEWALK SECTION WITHIN INTERSECTION CORNER



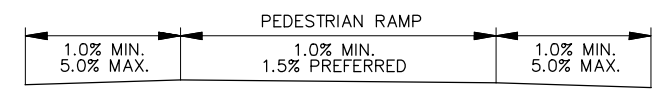
FLOW LINE PROFILE "TABLE" - TWIN PERPENDICULARS



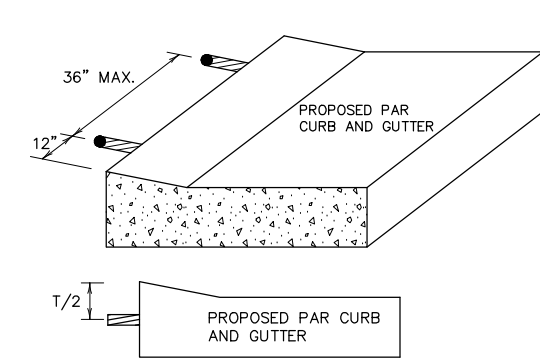
FLOW LINE PROFILE "TABLE" - FAN



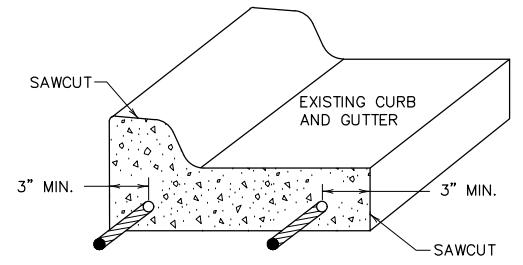
FLOW LINE PROFILE RAISE - TWIN PERPENDICULARS



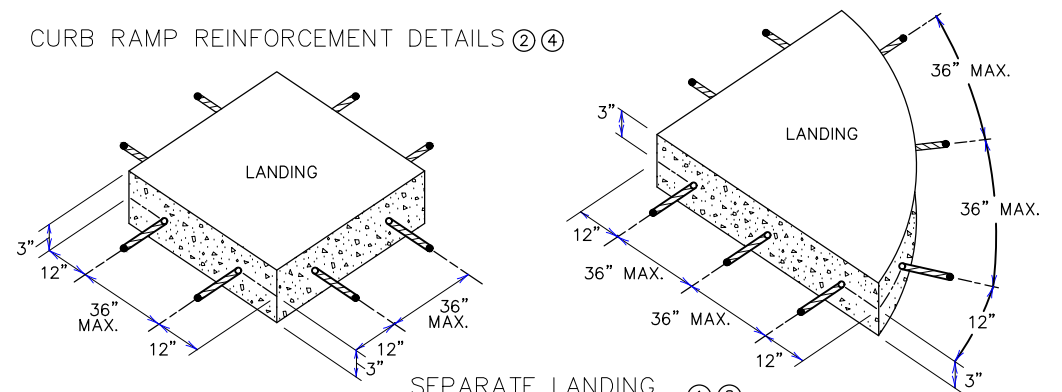
FLOW LINE PROFILE RAISE - FAN



CURB RAMP REINFORCEMENT DETAILS ② ④



CURB AND GUTTER REINFORCEMENT ③



SEPARATE LANDING POUR REINFORCEMENT ① ②

GENERAL NOTES:

- "TABLING" OF CROSSWALKS MEANS MAINTAINING LESS THAN 2% CROSS SLOPE WITHIN A CROSSWALK, IS REQUIRED WHEN A ROADWAY IS IN A STOP OR YIELD CONDITION AND THE PROJECT SCOPE ALLOWS.
- RECONSTRUCTION PROJECTS: ON FULL PAVEMENT REPLACEMENT PROJECTS "TABLING" OF ENTIRE CROSSWALK SHALL OCCUR WHEN FEASIBLE.
- MILL & OVERLAY PROJECTS: "TABLING" OF FLOW LINES, IN FRONT OF THE PEDESTRIAN RAMP, IS REQUIRED WHEN THE EXISTING FLOW LINE IS GREATER THAN 2%. WARPING OF THE BITUMINOUS PAVEMENT CAN NOT EXTEND INTO THE THROUGH LANE. TABLE THE FLOW LINE TO 2% OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA:
  - 1.0% MIN. CROSS-SLOPE OF THE ROAD
  - 5.0% MAX. CROSS-SLOPE OF THE ROAD
  - "TABLE" FLOW LINE UP TO 4% CHANGE FROM EXISTING SLOPE IN FRONT OF PEDESTRIAN RAMP
  - UP TO 2% CHANGE IN FLOW LINE FROM EXISTING SLOPE BEYOND THE PEDESTRIAN CURB RAMP
- STAND-ALONE ADA RETROFITS: FOLLOW MILL & OVERLAY CRITERIA ABOVE HOWEVER ALL PAVEMENT WARPING IS DONE WITH BITUMINOUS PATCHING ON BITUMINOUS ROADWAYS AND FULL-DEPTH APRON REPLACEMENT ON CONCRETE ROADWAYS.
- RAISING OF CURB LINES SHOULD OCCUR IN VERTICALLY CONSTRAINED AREAS. RAISE THE CURB LINES ENOUGH TO ALLOW COMPLIANT RAMPS OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA:
  - 1.0% MIN. AND 5.0% MAXIMUM CROSS-SLOPE OF THE ROAD
  - 1.0% MIN. FLOW LINE (ON EITHER SIDE OF PEDESTRIAN RAMP) TO MAINTAIN POSITIVE DRAINAGE
  - 5.0% RECOMMENDED MAX. FLOW LINE
  - LONGITUDINAL THROUGH LANE ROADWAY TAPERS SHOULD BE 1" VERTICAL PER 15' HORIZONTAL

NOTES:

- ① TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, ALL INITIAL LANDINGS AT A TOP OF A RAMPED SURFACE (RUNNING SLOPE GREATER THAN 2%) SHALL BE FORMED AND PLACED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON THIS SHEET FOR ALL SEPARATELY POURED INITIAL LANDINGS.
- ② DRILL AND GROUT NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) AT 36" MAXIMUM CENTER TO CENTER MINIMUM 12" SPACING FROM CONSTRUCTION JOINTS. BARS TO BE ADJUSTED TO MATCH RAMP GRADE. BARS TO BE PAID BY EACH.
- ③ DRILL AND GROUT 2 - NO. 4 X 12" LONG (6" EMBEDDED) REINFORCEMENT BARS (EPOXY COATED). REINFORCEMENT REQUIRED FOR ALL CONSTRUCTION JOINTS. BARS TO BE PAID BY EACH.
- ④ THIS CURB LINE REINFORCEMENT DETAIL SHALL BE USED ON BITUMINOUS ROADWAYS. FOR CONCRETE ROADWAYS, SEE NOTE 6.
- ⑤ CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPECIFICATION 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE.
- ⑥ USE AN APPROVED TYPE F (1/4 INCH THICK) SEPARATION MATERIAL. SEPARATION MATERIAL SHALL MATCH FULL HEIGHT DIMENSION OF ADJACENT CONCRETE.

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REVISION:

APPROVED: 11-04-2021

*Jeff J. Perkins*

JEFFREY PERKINS  
OPERATIONS DIVISION

**m** MINNESOTA DEPARTMENT OF TRANSPORTATION

STANDARD PLAN 5-297.250 6 OF 6

APPROVED: 11-04-2021

REVISOR:

SP VALUE

*Tom Styrbicki*  
THOMAS STYRBICKI  
STATE DESIGN ENGINEER

PEDESTRIAN CURB RAMP DETAILS

SHEET NO. 20 OF 20 SHEETS



# Request for Action

**To**  
Planning Commission

**Item Number**  
4.4

**Meeting Date**  
February 25, 2025

**Prepared By**  
Chris Leeseberg, Senior Planner

**Item Description**  
Ordinance Amendment (PUD): Signage Requirements and Codification, Elk River Crossing Planned Unit Development, Tractor Supply

**Reviewed by**  
Zack Carlton

## Action Requested

Recommend, by motion, approval of an ordinance establishing and outlining the development standards for the Elk River Crossing Planned Unit Development and updating the signage standards.

## Background/Discussion

The City Council approved the Planned Unit Development (PUD) for Elk River Crossing in 1999 and several subsequent amendments. The applicant, Tractor Supply, has requested an Ordinance Amendment to codify the PUD and allow additional signage for their outdoor garden center. The city code now requires that PUDs be established through an ordinance rather than the Conditional Use Permit (CUP) process. This change enhances access to PUD information for residents and developers while streamlining future amendments.

City staff have drafted an ordinance maintaining the current permitted and conditional uses, along with architectural standards. It is recommended that signage for all freestanding and multi-tenant buildings complies with the Highway Commercial District (C3) Sign Ordinance. However, major tenants (Wal-Mart and Home Depot), will retain the signage allowances specified in the original PUD. Development and Entry signs will also remain consistent with the original PUD agreement.

Attached is a graphic illustrating the affected areas, along with the original PUD agreement and amendments.

## Financial Impact

None

## Mission/Policy/Goal

Simplify city ordinances and policies.

Support the growth and development of the residential community.

## Attachments

1. Ordinance Document - Elk River Crossing
2. Signage Exhibits

The Elk River Vision

*A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity*

3. Sign Site Plan
4. Area Signage Exhibit
5. PUD Agreements and Amendments

## Ordinance 25 - \_\_\_\_

### **An Ordinance Amending Chapter, 30, Division 9, Other Nonoverlay Districts, of the City of Elk River, Minnesota, City Code**

The City Council of the City of Elk River does hereby ordain as follows:

**SECTION 1.** That **Chapter 30, Division 9, Other Nonoverlay Districts**, of the City of Elk River Code of Ordinances shall be amended to add the following Section:

Section 30-1602. – Elk River Crossing PUD

- (a) Legal Description. The following described property located within the City of Elk River, Sherburne County, Minnesota is hereby zoned PUD, Planned Unit Development:

Lot 1, Block 2, ELK RIVER CROSSING, Sherburne County, Minnesota

Lot 2, Block 2, ELK RIVER CROSSING, Sherburne County, Minnesota

Lot 1, Block 1, ELK RIVER CROSSING THIRD ADDITION, Sherburne County, Minnesota

Lot 2, Block 1, ELK RIVER CROSSING THIRD ADDITION, Sherburne County, Minnesota

Lot 3, Block 1, ELK RIVER CROSSING THIRD ADDITION, Sherburne County, Minnesota

Lot 1, Block 1, ELK RIVER CROSSING FOURTH ADDITION, Sherburne County, Minnesota

Lot 2, Block 1, ELK RIVER CROSSING FOURTH ADDITION, Sherburne County, Minnesota

Lot 1, Block 1, ELK RIVER CROSSING FIFTH ADDITION, Sherburne County, Minnesota

Lot 1, Block 1, ELK RIVER CROSSING SIXTH ADDITION, Sherburne County, Minnesota

Lot 2, Block 1, ELK RIVER CROSSING SIXTH ADDITION, Sherburne County, Minnesota

Lot 1, Block 1, ELK RIVER CROSSING SEVENTH ADDITION, Sherburne County, Minnesota

Outlot A, ELK RIVER CROSSING SEVENTH ADDITION, Sherburne County, Minnesota

Lot 1, Block 1, ELK RIVER CROSSING EIGHTH ADDITION, Sherburne County, Minnesota

Lot 2, Block 1, ELK RIVER CROSSING EIGHTH ADDITION, Sherburne County, Minnesota

Lot 1, Block 1, THE HOME DEPOT ADDITION, Sherburne County, Minnesota

(b) PUD Development Standards. The property shall be subject to the following standards.

(1) Uses.

a. Permitted Uses

1. Convenience stores.
2. Financial institutions.
3. Garden centers, which may include outdoor display.
4. Governmental, business and professional offices.
5. Non-profit clubs, lodges or halls.
6. Personal service establishments.
7. Retail shops and stores.
8. Veterinary clinics.

b. Conditional uses.

1. Amusement and recreation uses.
2. Car washes.
3. Educational uses.
4. Hotel and motels.
5. Licensed day care facilities.
6. Motor vehicle service stations.
7. Motor vehicle specialty shops.
8. On-sale liquor establishments.
9. Restaurants (Class I and II).
10. Retail sale of gasoline.
11. Zero lot line development.

(2) Architectural standards.

a. Facades and exterior walls.

1. The Elk River Crossing development will represent the eastern terminus of the Main Street corridor which connects Highway 169 and the Central Business District. As such, the exterior appearance of the buildings should include elements of buildings found along the corridor and in the Central Business District.

Facades should be articulated to reduce the massive scale and uniform impersonal appearances of large retail buildings and provide visual interest. Buildings over 100 feet in length should incorporate recesses, off-sets, pilasters or angular forms so that no uninterrupted

length exceeds 50 feet. The wall projection or recess should be a depth of approximately 3% of the length of the uninterrupted building length. Buildings should have architectural features and patterns that provide visual interest at a pedestrian scale. The features should be an integral part of the building fabric, and not be applied trim, graphics or paint. Variations to roof lines and cornice details should also be used to add interest to buildings.

Exterior building materials and colors comprise a significant part of the visual impact of a building. Therefore, they should be aesthetically pleasing and compatible with materials and colors used in the Central Business District and along east Main Street. Facade colors should be subtle, neutral or earth tone colors. The development shall use a consistent palate of materials and colors throughout. Facades of a building which are visible from adjoining properties or public streets shall include certain design elements found on the front facades.

Predominant exterior facade materials should include, but not limited to, one or more of the following:

1. Brick
2. Native stone
3. Tinted, textured, integrally colored concrete masonry units.
4. Stucco
5. Tilt-up concrete panels with exterior facades as referred to in these standards

b. Entryways

1. Entrances should be clearly defined and visible. They should incorporate devices such as canopies, overhangs, recesses/projections, arcades, raised parapets over the doors, peaked roof forms, display windows, architectural details such as tile works and moldings which are integrated into the building design.

c. Lighting

1. General site and street lighting (excluding general parking lot lighting) shall be a design that is compatible and connects with the lighting utilized in the Central Business District. It shall be at a pedestrian scale. Wall lighting fixtures shall be consistent with the pedestrian and general site lighting.

General parking lot lighting shall be at a height no greater than 30 feet. Poles and fixtures shall be a design that compliments the street and general site lighting standards and fixtures.

d. Loading Areas, Trash Collection and Outdoor Storage

1. Loading docks, truck parking, HVAC equipment, and other service functions should be incorporated into the overall design of the building and screened from view of adjoining properties and streets. Trash compactors and dumpsters should be within an enclosure constructed of the same materials and colors as those used in the building. Outdoor areas used for the sale and/or display of seasonal inventory shall be permanently defined and screened with walls and/or fences and landscaping. Outdoor sales and display areas, except for garden centers, shall not exceed 5% of the ground floor area of the building.

e. Canopies

1. Canopies located over pump islands are allowed provided the architectural design, colors and character of the canopy shall be consistent with the principle building on site. The fascia of the canopy shall not be illuminated. Canopy lighting shall consist of canister spotlights recessed into the canopy. No portion of the light source or fixture may extend below the bottom face of the canopy.

- (3) Signage. Signs shall be an integral part of the overall architectural design of a building and the project, rather than contrasting with it. They must consist of materials and design which compliments the architectural elements of the building and project. Signs must not be visually distracting or tend to dominate by means of location, size, color or lighting. All signs shall be internally illuminated.

All freestanding signs shall be setback a minimum of 10 feet from all property lines and shall not obstruct visibility or encroach in any drainage or utility easements.

a. Development Sign

1. One freestanding development sign identifying Elk River Crossing will be permitted along Highway 169. The sign shall be a monument style sign with a maximum height of 30 feet and a width of 16 feet. The number of tenants allowed on this sign shall be limited to 4. Total sign area shall not exceed 225 square feet and shall not exceed 10 feet in width. The sign shall be constructed of brick, face brick or

colored rock faced block and may include decorative brick and/or decorative glazed block as accents.

- b. Entry Signs
  - 1. Two entry signs shall be allowed, one at the Main Street and Zane Street intersection and one at the Tyler Street and 179th Avenue intersection. These signs shall be a monument style sign with a maximum height of 8 feet and a width of 16 feet. Total sign area shall not exceed 128 square feet. The sign shall be constructed of brick, face brick or colored rock faced block and in may include decorative brick and/or decorative glazed block as accents. These entry signs may identify no more than 4 tenants.
  
- c. Monument Signs for Freestanding Sites
  - 1. Each of the freestanding buildings within Elk River Crossing will be allowed to have a freestanding sign. The freestanding sign must be a monument style with a maximum height of 10 feet and a maximum area of 100 square feet. The sign shall be constructed of brick or face brick and in may include decorative brick and/or decorative glazed block as accents.
  
- d. Wall Signs for All Sites
  - 1. All parcels within Elk River Crossing and not specifically mentioned herein shall comply with the City of Elk River High Commercial District Sign Ordinance Section 30-871
  
- e. Wall Signs on North Side of Retail Building (L1, B1 The Home Depot Addition)
  - 1. Wall signs for the retail building attached to the north side of the proposed home improvement store shall consist of individual letters. Capital letters shall not exceed 36" high, lower-case letters shall not exceed 24" high. Signs shall be for store identity only and shall not be allowed on the rear or side of building. Signs shall not extend closer than 3 or 4 feet from the projected lease line. If the space is leased by a single tenant, sign area shall not exceed 10 percent of the area of the building wall, including doors and windows, to which the sign is to be affixed.
  
- f. Major Tenant Wall Signage (L2, B2 Elk River Crossing & L1, B1 The Home Depot Addition)

1. Total wall sign area for major tenants should not exceed 7% of the front facade of the principal structure and must consist of individually mounted letters and corporate logos. Letters shall not exceed 6 feet in height and logos shall not exceed 10 feet in height. All signage shall be internally illuminated.

g. ~~Other Signs~~

1. ~~All other signage that may occur within Elk River Crossing and not specifically mentioned herein shall comply with the City of Elk River Sign Ordinance. All requests which are not consistent with conditions, character and intent of the Elk River Crossing PUD may be denied by the City.~~



📅 December 4, 2024

📍 Elk River, MN



**Notes**

- Internally Illuminated Channel Letters With Black Day/ Red Night Faces
- Illuminated With White LEDs
- Logo is Flush Mounted
- Logo Has White Lexan Face With 3M 3630-33 Red Vinyl w/ White Copy
- 1" Red Retainer
- 6" Deep

THIS DRAWING IS FOR CONCEPTUAL PURPOSES ONLY. DUE TO CONSTRUCTION CONSTRAINTS, SIZES AND/OR LAYOUTS MAY CHANGE SLIGHTLY.

CLIENT: TRACTOR SUPPLY CO	STORE #: 610	REV: R1 11/14/24 BRC	REV:	REV:
LOCATION: ELK RIVER, MN	DATE: 10/28/24	REV: R2 11/15/24 BRC	REV:	REV:
ACCOUNT REP: DEANNA PAYNE	DRAWN BY: BRC	REV: R3 12/04/24 BRC	REV:	REV:
DRAWING NO: TSC - ELK RIVER, MN #610 - SIGN EXHIBIT				

EXHIBIT APPROVED BY:

SIDE ELEVATION REMOTE CHANNEL LETTERS



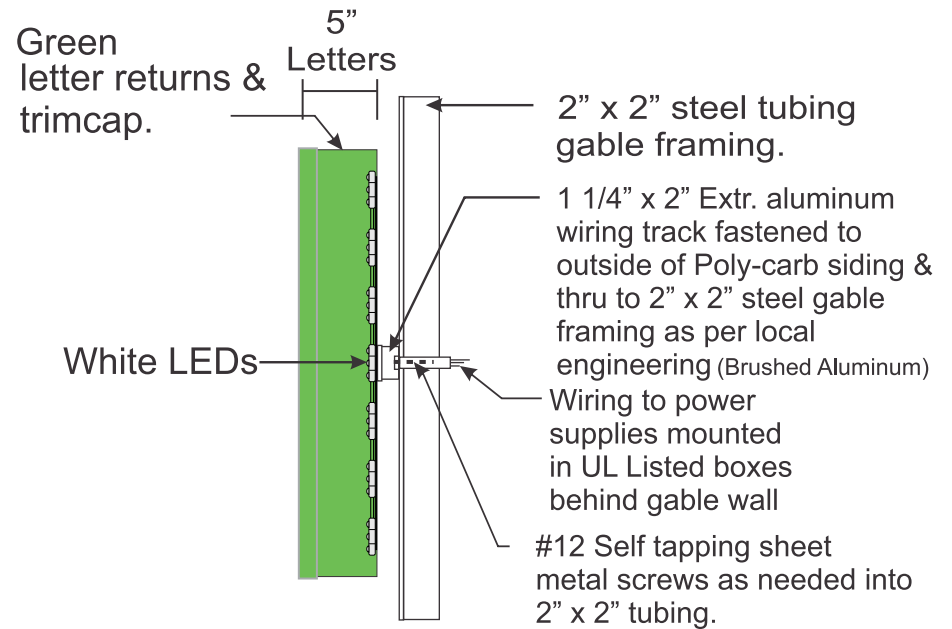
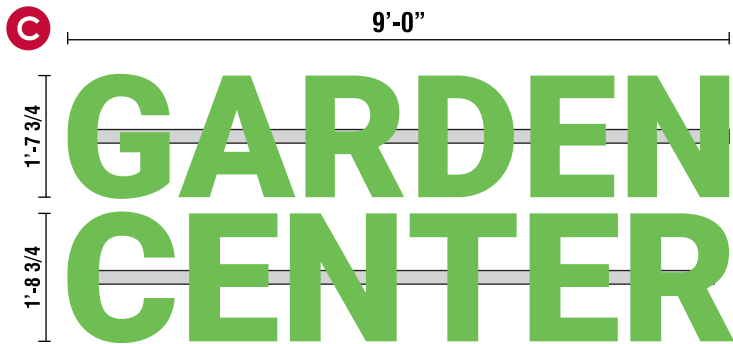
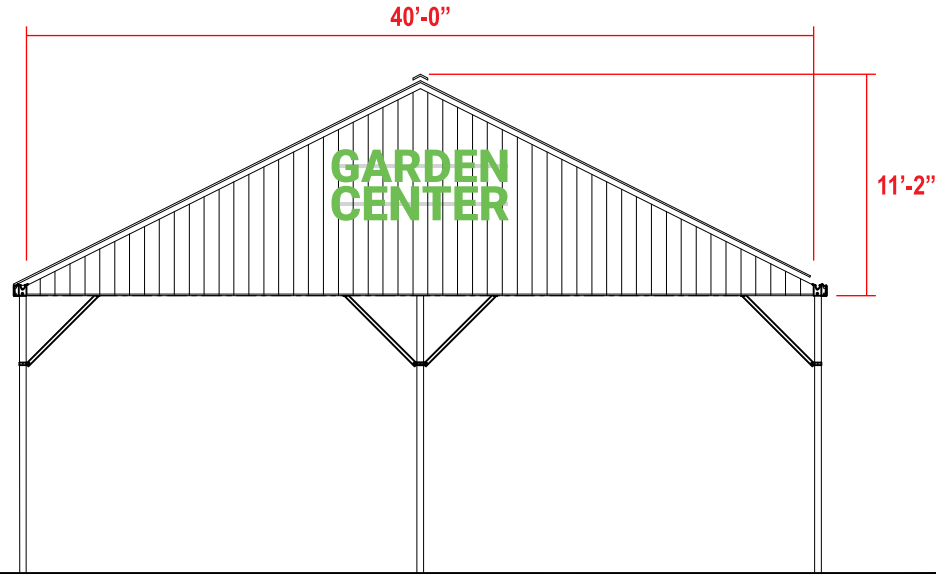
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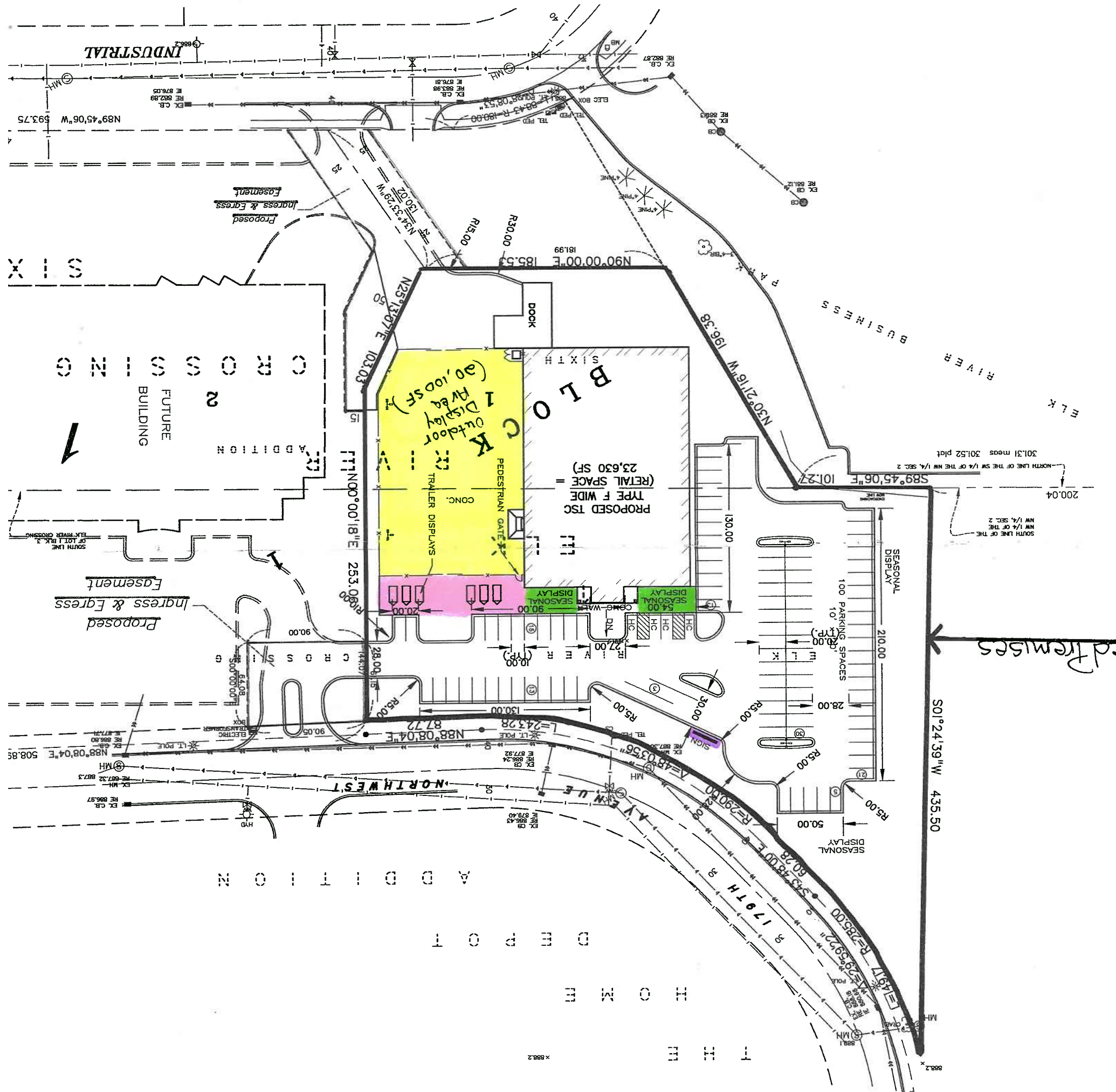
EXHIBIT APPROVED BY:



THIS DRAWING IS FOR CONCEPTUAL PURPOSES ONLY. DUE TO CONSTRUCTION CONSTRAINTS, SIZES AND/OR LAYOUTS MAY CHANGE SLIGHTLY.

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ACCOUNT REP: DEANNA PAYNE	DRAWN BY: BRC	REV: R3 12/04/24 BRC	REV:	REV:
DRAWING NO: TSC - ELK RIVER, MN #610 - SIGN EXHIBIT				

EXHIBIT APPROVED BY:



ELK RIVER, MN #610

Development Sign Removed  
30' tall x 16' wide  
225 sqft  
4 tenants

Main St

Entry Sign #1

75-00644-0002

Co Rd 12

75-00644-0235

75-00404-0126

75-00134-4406

Highway 169

Zane St

75-00003-1000

75-00702-0160

-  Monument & Wall Signs for Freestanding Sites
-  Major Tenant Wall Signage
-  Multi-Tenant Sites

Zane St

Twin Lakes Road

Entry Sign #2

179th Ave

75-00710-6000

75-00003-1200

**PLANNED UNIT DEVELOPMENT AGREEMENT  
ELK RIVER CROSSING**

THIS PLANNED UNIT DEVELOPMENT AGREEMENT ("PUD Agreement"), made and entered into this 21st, day of December, 1999, by and between the CITY OF ELK RIVER, a municipal corporation organized under the laws of the State of Minnesota (the "City"); and Associated Investors of Elk River, Inc., a Minnesota corporation, (the "Developer");

WITNESSETH:

WHEREAS, Developer has made application to City for approval of a Conditional Use Permit (the "Conditional Use Permit") to develop a shopping center project (the "Project") as a Planned Unit Development, pursuant to Sections 900.12(15) and 900.42 of the Elk River City Code of Ordinances, on certain property located within the City and described on Exhibit A attached hereto (the "Property"); and

WHEREAS, the City Council of the City of Elk River (the "City Council") adopted Resolution No. 99-\_\_\_ (attached as Exhibit B hereto) on May 17, 1999, approving the Conditional Use Permit for the Project, subject to a number of conditions; and

WHEREAS, one of the conditions of the approval of the Conditional Use Permit is that Developer enter into a Planned Unit Development Agreement (“PUD Agreement”) with City to be approved by the City Council; and

WHEREAS, the purposes of the PUD Agreement are to set forth the standards, uses and procedures for development of the Project as a Planned Unit Development;

NOW, THEREFORE, in consideration of the premises and the mutual promises and conditions hereafter contained, it is hereby agreed as follows:

1. Warranty of Developer - Developer hereby warrants and represents to City, as inducement to City’s entering into this PUD Agreement, that:

- (a) Developer is the fee owner of that portion of the Property so identified on Exhibit A hereto.
- (b) Developer will acquire the remaining parcels located within the Property, as identified on Exhibit A hereto. This PUD Agreement is contingent on Developer acquiring all of the parcels identified as the Property on Exhibit A hereto.
- (c) Developer intends, following approval of a final plat of the Property, to sell some parcels within the Property to third parties unrelated to Developer (Third Parties). This PUD Agreement shall be binding on Third Parties.

2. Development - The Project shall be developed in accordance with the plans attached as Exhibit C hereto, as the same may be amended as part of future amendments to the Conditional Use Permit for the Project, subject to obtaining site plan approval for each parcel within the Property as provided by this PUD Agreement:

Exhibit C - Plans:

- Site Plan
- Landscape Plan
- Grading Plan
- Setback Plan
- Sign Plan
- Development Standards, including:
- Architectural Theme, including Building Materials

3. Uses -

(a) Permitted Uses - The following uses shall be allowed as permitted uses within the Project, subject to the approval process set forth in paragraph 4:

- Governmental, business and professional offices
- Retail shops and stores
- Personal service establishments
- Financial institutions
- Non-profit clubs, lodges or halls
- Veterinary clinics
- Convenience stores
- Garden centers, which may include outdoor display

(b) Conditional Uses - The following uses shall be allowed as conditional uses within the Project:

- Amusement and recreation uses
- Hotel and motels
- Licensed day care facilities
- Educational uses
- Motor vehicle service stations
- Motor vehicle specialty shops
- Car washes
- Restaurants (Class I and II)
- On-sale liquor establishments
- Retail sale of gasoline
- Zero lot line development

4. Approval by City - City hereby approves the Project as shown in the attached exhibits provided, however, that insofar as the exhibits vary from the written terms of this PUD Agreement, the written terms shall govern. This approval is subject to individual site and building plan review and approval as follows:

(01) Site Plan Approval -

(a) Site plan approval shall be required prior to issuance of a building permit for any lot within the Project, both for initial construction and for any subsequent expansion, exterior remodeling of a structure, redevelopment of a site, or substantial revision of an approved site plan.

(b) Each site plan approval request shall be accompanied by such plans and supporting materials as City staff shall require, and shall be reviewed by the Planning Commission and City Council in a public hearing held thereon before approval.

(c) Site plan approval shall be granted only if the City Council finds that the proposed site plan is consistent with the terms and conditions of the Conditional Use Permit for the Project, this PUD Agreement, applicable City Ordinance requirements and all other legal requirements. City reserves the right to refuse approval of an individual site plan if, in the City Council's judgment, this condition is not met.

(d) The City Council may, when approving individual site plans, impose such conditions as it shall deem necessary to insure that the Project complies with the requirements of this PUD Agreement, as amended from time to time, the Conditional Use Permit, as amended from time to time, and the City Code of Ordinances.

(02) Conditional Use Permit Approval - In addition to the site plan approval process set forth above, a conditional use permit shall be applied for and approved, pursuant to the Elk River Zoning Ordinance, for any conditional use in the Project. Each such conditional use shall be considered and acted upon by the City Council pursuant to the standards set forth in the Zoning Ordinance for conditional uses, as well as the standards and requirements set forth in this PUD Agreement and the attachments hereto.

(03) Planned Unit Development Amendment - In addition to site plan approval and conditional use permit approval for individual lots and sites within the Project, any substantial revision to the Plans identified in paragraph 2 of this PUD Agreement and attached as Exhibit C hereto which affects more than one lot or site shall require an amendment, by Conditional Use Permit, of the Planned Unit Development.

5. Public Improvements - Public Improvements for the Project shall be installed and paid for as provided by the terms of the Developer Agreement entered into between Developer and City attached as Exhibit D to this PUD Agreement. Any violation by Developer of the terms of the Developer Agreement shall be a violation of this PUD Agreement, and shall be grounds for City invoking against Developer the remedies provided for by this PUD Agreement, including denial of site plan review or other approval for one or more of the lots in the Project.

6. Maintenance of Project Areas -

(0) Litter Control - Developer and all Third Party owners of lots within the Subdivision shall pick up litter as needed, at least once per month (except during winter weather), from:

( ) all parking and landscaped areas on their property

(a) public rights-of-way adjacent to their property, including the Highway 169 right-of-way east of the northbound lane, the Main Street Right-of-Way South of Main Street, the entire rights-of-way of Tyler Street and Crossing Avenue, and all ponding and wetland areas within the Project.

(1) Maintenance Agreements - Developer shall provide evidence of satisfactory provision for control and maintenance of jointly used or commonly owned parking, drainage and landscaped areas within the Project. Reciprocal parking and access easements and maintenance agreements shall be provided to and approved by the City Attorney prior to the issuance of certificates of occupancy so as to insure satisfactory control, ownership and maintenance. These agreements shall provide that Developers and their successors shall be obligated to maintain all common areas, drainage areas, outlots and landscaping in the Project, including all wetland and ponding areas, in perpetuity.

(2) Enforcement - Failure of Developer or a Third Party to meet its maintenance obligations under this paragraph will be grounds for City to invoke against that party any remedy which City has under this PUD Agreement.

7. Remedies - Should Developer or a Third Party breach any of the terms and conditions of this PUD Agreement, and if such breach is not cured within thirty (30) days after written notice, or if such cure shall reasonably require longer than thirty (30) days to cure within such longer period as shall be reasonably necessary to cure such breach provided Developer or the Third Party commences cure within such thirty (30) day period and diligently proceeds to cure such breach thereafter, City shall have any or all of the following remedies against the party who has breached:

(1) Withhold site plan approval for undeveloped lots;

(2) Withhold issuance of building permits or certificates of occupancy for any structure within the Project;

(3) Halt construction on public improvements not commenced or not completed;

(4) Institute prosecution of Developer or the Third Party under the relevant provisions of the City Zoning Ordinance;

(5) Draw upon any financial security provided the City to assure compliance;

(6) Institute a civil action to compel compliance or recover damages or both.

8. Additional Specific Conditions - The conditions set forth in Resolution No. 99-26 approving the Conditional Use Permit for the Project are specifically incorporated into this PUD Agreement and made a part hereof. Violation of any of the conditions set forth in that Resolution shall be a violation of the terms of this PUD Agreement.

9. General Conditions - If Developer or a Third Party fails to comply with any of the provisions hereof, City may refuse to issue to the Party in breach a building permit or certificate of occupancy for any lot or parcel in the Project. Developer agrees to advise purchasers of any lot or parcel within the Project of the existence of this PUD Agreement, and the obligations and restrictions created herein prior to the completion of a sale. Further, Developer agrees to save and hold City harmless from any and all claims or actions arising from the right to withhold the issuance of such permits and certificates and from other provisions hereof.

10. Binding Effect - The terms and provisions hereof shall be binding upon and inure to the benefit of the heirs, representatives, successors and assigns of the parties hereto and shall be binding upon all Third Parties or other future owners of all or any part of the Project, and shall be deemed covenants running with the land. This agreement shall be placed of record so as to give notice hereof to subsequent purchasers and encumbrances of all or any part of the Project, and all recording fees, if any, shall be paid by Developer.

11. Severable - In the event that any portion of this PUD Agreement shall be held invalid for any reason, the same shall not affect in any respect whatsoever the validity of the remainder of this PUD Agreement.

12. Waiver - Any waiver, whether expressed or implied, by any party or a breach of any provision of this Agreement, will not operate as or be construed to be a waiver of any subsequent breach of this Agreement.

13. Notices - All notices and demands specified herein shall be deemed appropriately and timely given when delivered personally or deposited in the United States Mail to the addresses hereinafter set forth by certified mail (return receipt requested). The addresses of the parties hereto are as follows until changed by written notice given as above:

If to the City at: City of Elk River  
Attn: City Administrator  
P.O. Box 490  
13065 Orono Parkway  
Elk River, Minnesota 55330

If to the Developer at: Associated Investors Of Elk River, Inc.  
Attn: Mathias Fischer  
14698 Galaxie Avenue  
Apple Valley, MN 55124

With a copy to: Anthony J. Gleekel, Esq.  
Siegel, Brill, Greupner, Duffy & Foster, P.A.  
1300 Washington Square  
100 Washington Avenue South  
Minneapolis, MN 55401

Wal-Mart Stores, Inc.  
2001 S.E. 10<sup>th</sup> St.  
Bentonville, Arkansas, 72716  
Attention: Mn. Real Estate Mgr.-Store # 90192

Wal-Mart Stores, Inc.  
2001 S.E. 10<sup>th</sup> St.  
Bentonville, Arkansas, 72716  
Attention: General Counsel – Store # 90192

14. Incorporation by Reference - All plans, special provisions, proposals, specifications and contracts made or to be made pursuant to this PUD Agreement shall be and hereby are made a part of this PUD Agreement by reference as fully as if set forth herein in full.

15. Entire Agreement - This PUD Agreement, together with the exhibits attached hereto, sets forth the entire understanding between the parties, there being no terms, conditions, warranties or representations other than those contained herein, including the exhibits hereto, and no amendments hereto shall be valid unless made in writing and signed by all the parties hereto.





EXHIBIT A

LEGAL DESCRIPTION

Lots 1-5, inclusive, Block 1, Lots 1-3, inclusive,  
Block 2, and Lot 1, Block 3, Elk River Crossing

396872

396872

EXHIBIT B

COUNTY CLERK  
SHERBURNE CO. MINN.  
MICHELLE ASHE  
BY Chellier DEPUTY

'99 AUG 25 PM 12 25

me me

STATE OF MINNESOTA

CITY OF ELK RIVER CITY COUNCIL

COUNTY OF SHERBURNE

CONDITIONAL USE PROCEEDINGS

CITY OF ELK RIVER

In the matter of: Associated Developers of the Twin Cities, Inc.

Request: Conditional Use Permit for Commercial/Retail Development  
Project (Elk River Crossing), Case No. CU 99-26

ORDER GRANTING

CONDITIONAL USE

Earl H. Hohlen and Lorraine Hohlen;  
James Mevissen, and Delton Warden and Joan Warden, Owners

The above entitled matter came to be heard before the City Council on the 17<sup>th</sup> day of May, 1999, on a petition for a Conditional Use pursuant to the City of Elk River Zoning Ordinance, for the following described property:

See Attached Exhibit A  
(PID #'S 75-002-2200, 75-135-3310, and 75-135-3315)

IT IS ORDERED that a Conditional Use be granted as upon the following conditions or reasons:

1. A FINAL LANDSCAPE PLAN AND LIGHTING PLAN BE SUBMITTED AND APPROVED BY THE CITY.
2. DESIGN GUIDELINES BE ESTABLISHED AND APPROVED BY THE CITY.
3. THE PEDESTRIAN ACCESS BETWEEN LOT 2, BLOCK 2 AND LOT 3, BLOCK 2, PARKING LOTS BE 24 FEET.
4. THE LANDSCAPED ISLANDS IN THE LOT 2, BLOCK 2, PARKING LOT BE MORE CENTRALLY LOCATED.

DATED this 17<sup>th</sup> day of May, 1999.

STATE OF MINNESOTA )  
 )  
COUNTY OF SHERBURNE ) ss.  
 )  
CITY OF ELK RIVER )

OFFICE OF CITY PLANNING AND ZONING  
  
CITY OF ELK RIVER

I, Michele McPherson, Director of Planning for the City of Elk River with and in for said City, do hereby certify that I have compared the foregoing copy and Order granting a Conditional Use with the original record thereof preserved in my office, and have found the same to be a true transcript of the whole thereof.

If the proposed work described in a conditional use permit has not been substantially completed within two (2) years after its date of issuance, the permit shall expire and become void except that the Council may, following recommendations of the Planning Commission, extend the permit for an additional period determined by the council on the receipt of a request for a permit extension prior to its expiration. A Conditional Use permit authorizes only the use specified in the permit and shall expire if, for any reason, the authorized use ceases for more than one (1) year.

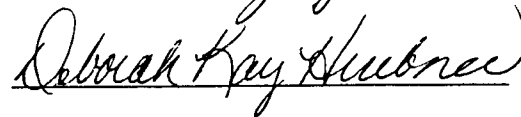
  
Michele McPherson  
Director of Planning

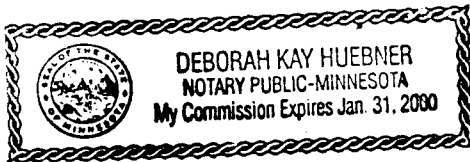
IN TESTIMONY WHEREOF, I have hereunto subscribed my hand at Elk River, Minnesota, in the County of Sherburne, in the City of Elk River, on the 9<sup>th</sup> day of July, 1999.

DRAFTED BY:

City of Elk River  
Elk River, Minnesota 55330

Signed before me a NOTARY PUBLIC  
this 9<sup>th</sup> day of July, 1999.





## Exhibit A

## PARCEL 1

The Northwest Quarter of the Northwest Quarter of Section 2, Township 32, Range 26, Sherburne County, Minnesota, EXCEPT that part thereof which lies westerly of a line run parallel with and distant 200 feet easterly of the west line of said Section 2.

## PARCEL 3

That part of the Southwest Quarter of the Southwest Quarter of Section 35, Township 33, Range 26, Sherburne County, Minnesota, lying south of County State Aid Highway No. 12, east of the west 200 feet thereof.

## PARCEL 4

The west 200 feet of the Southwest Quarter of Section 35, Township 33, Range 26, Sherburne County, Minnesota lying south of road.

## PARCEL 5

That part of the Southeast Quarter of the Southeast Quarter of Section 34, Township 33, Range 26, Sherburne County, Minnesota, lying east of Highway 169.

**EXHIBIT C**

Plans, including:

Site Plan

Landscape Plan

Grading Plan

Setback Plan

Sign Plan

Development Standards, including:

Architectural Theme, including Building Materials

**EXHIBIT D**

Developer Agreement for Elk River Crossing, has been recorded as document number \_\_\_\_ and is hereby incorporated by reference in this Agreement.

GP:606561 v1

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## DEVELOPMENT STANDARDS

### Architectural Character

#### 1. Facades and Exterior Walls

The Elk River Crossing development will represent the eastern terminus of the Main Street corridor which connects Highway 169 and the Central Business District. As such, the exterior appearance of the buildings should include elements of buildings found along the corridor and in the Central Business District.

Facades should be articulated to reduce the massive scale and uniform impersonal appearances of large retail buildings and provide visual interest. Buildings over 100 feet in length should incorporate recesses, off-sets, pilasters or angular forms so that no uninterrupted length exceeds 50 feet. The wall projection or recess should be a depth of approximately 3% of the length of the uninterrupted building length. Buildings should have architectural features and patterns that provide visual interest at a pedestrian scale. The features should be an integral part of the building fabric, and not be applied trim, graphics or paint. Variations to roof lines and cornice details should also be used to add interest to buildings.

Exterior building materials and colors comprise a significant part of the visual impact of a building. Therefore, they should be aesthetically pleasing and compatible with materials and colors used in the Central Business District and along east Main Street. Facade colors should be subtle, neutral or earth tone colors. The development shall use a consistent palate of materials and colors throughout. Facades of a building which are visible from adjoining properties or public streets shall include certain design elements found on the front facades.

Predominant exterior facade materials should include, but not limited to, one or more of the following:

- brick
- native stone
- tinted, textured, integrally colored concrete masonry units.
- stucco
- tilt-up concrete panels with exterior facades as referred to in these standards

Predominant exterior building materials shall not include the following:

- smooth faced concrete block

- smooth faced or raked faced tilt up concrete panels
- prefabricated steel panels.

## 2. Entryways

Entrances should be clearly defined and visible. They should incorporate devices such as canopies, overhangs, recesses/projections, arcades, raised parapets over the doors, peaked roof forms, display windows, architectural details such as tile works and moldings which are integrated into the building design.

### Lighting

General site and street lighting (excluding general parking lot lighting) shall be a design that is compatible and connects with the lighting utilized in the Central Business District. It shall be at a pedestrian scale. Wall lighting fixtures shall be consistent with the pedestrian and general site lighting.

General parking lot lighting shall be at a height no greater than 30 feet. Poles and fixtures shall be a design that compliments the street and general site lighting standards and fixtures.

### Loading Areas, Trash Collection and Outdoor Storage

Loading docks, truck parking, HVAC equipment, and other service functions should be incorporated into the overall design of the building and screened from view of adjoining properties and streets. Trash compactors and dumpsters should be within an enclosure constructed of the same materials and colors as those used in the building. Outdoor areas used for the sale and/or display of seasonal inventory shall be permanently defined and screened with walls and/or fences and landscaping. Outdoor sales and display areas, except for garden centers, shall not exceed 5% of the ground floor area of the building.

### Canopies

Canopies located over pump islands are allowed provided the architectural design, colors and character of the canopy shall be consistent with the principle building on site. The fascia of the canopy shall not be illuminated. Canopy lighting shall consist of canister spot lights recessed into the canopy. No portion of the light source or fixture may extend below the bottom face of the canopy.

### Signage

Signs shall be an integral part of the overall architectural design of a building and the project, rather than contrasting with it. They must consist of materials and design which compliments the architectural elements of the building and project. Signs must not be visually distracting or tend to dominate by means of location, size, color or lighting. All signs shall be internally illuminated.

All freestanding signs shall be setback a minimum of 10 feet from all property lines and shall not obstruct visibility or encroach in any drainage or utility easements.

### Development Sign

One freestanding development sign identifying Elk River Crossing will be permitted at the corner of Main Street and Highway 169. The sign shall be a monument style sign with a maximum height of 30 feet and a width of 16 feet. The number of tenants allowed on this sign shall be limited to 4. Total sign area shall not exceed 225 square feet and shall not exceed 10 feet in width. The sign shall be constructed of brick, face brick or colored rock faced block and may include decorative brick and/or decorative glazed block as accents.

### Entry Signs

Two entry signs shall be allowed, one at the Main Street and Zane Street intersection and one at the Tyler Street and 179<sup>th</sup> Avenue intersection. These signs shall be a monument style sign with a maximum height of 8 feet and a width of 16 feet. Total sign area shall not exceed 128 square feet. The sign shall be constructed of brick, face brick or colored rock faced block and in may include decorative brick and/or decorative glazed block as accents. These entry signs may identify no more than 4 tenants.

### Monument Signs for Freestanding Sites

Each of the freestanding buildings within Elk River Crossing will be allowed to have a freestanding sign. The freestanding sign must be a monument style with a maximum height of 10 feet and a maximum area of 100 square feet. The sign shall be constructed of brick or face brick and in may include decorative brick and/or decorative glazed block as accents.

### Wall Signs for Freestanding Sites

Wall signs for each freestanding building must consist of individual letters and shall not have more than 1 wall sign for each principle building, except that where a principal building abuts 2 or more public streets, 2 wall signs

will be allowed. Awning signs and canopy signs, including canopies over gas pumps, shall be included as wall signs. Awning and canopy signs shall not be illuminated. The sign area shall not exceed seven (7) percent of the area of the building wall, including doors and windows, to which the sign is to be affixed

#### Wall Signs Retail Building North Side of Home Improvement Store

Wall signs for the retail building attached to the north side of the proposed home improvement store shall consist of individual letters. Capital letters shall not exceed 36" high, lower case letters shall not exceed 24" high. Signs shall be for store identity only and shall not be allowed on the rear or side of building. Signs shall not extend closer than 3 or 4 feet from the projected lease line. If the space is leased by a single tenant, sign area shall not exceed 10 percent of the area of the building wall, including doors and windows, to which the sign is to be affixed

#### Major Tenant Wall Signage

Total wall sign area for major tenants should not exceed 7% of the front facade of the principle structure and must consist of individually mounted letters and corporate logos. Letters shall not exceed 6 feet in height and logos shall not exceed 10 feet in height. All signage shall be internally illuminated.

#### Other Signs

All other signage that may occur within Elk River Crossing and not specifically mentioned herein shall comply with the City of Elk River Sign Ordinance. All requests which are not consistent with conditions, character and intent of the Elk River Crossing PUD may be denied by the City.

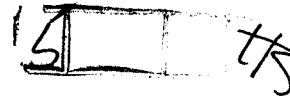
439633

439633

SHERBURNE CO. MINN.

MICHELLE ASHE  
BY *Michelle Ashe* DEPUTY

'01 JUN 21 PM 4 27



**AMENDMENT NO. 1  
to  
ELK RIVER CROSSING  
PLANNED UNIT DEVELOPMENT AGREEMENT  
CITY OF ELK RIVER, SHERBURNE COUNTY, MINNESOTA**

THIS AMENDMENT NO. 1 is made as of the 26 day of March, 2001, by and between the CITY OF ELK RIVER, Sherburne County, Minnesota (the City), and Gerald and Angie Koschney, husband and wife.

**WITNESSETH:**

WHEREAS, on December 21, 1999, the City of Elk River and Elk River Associated Investors, Inc. hereto entered into the Elk River Crossing Planned Unit Development Agreement; and

WHEREAS, Gerald and Angie Koschney purchased Lot 1, Block 1, Elk River Crossing 2<sup>nd</sup> Addition, and

WHEREAS, on February 20, 2001, the Elk River City Council approved an amendment to allow a third wall sign (2 foot by 8 foot illuminated canopy sign) for Wash-n-Fill located on Lot 1, Block 1, Elk River Crossing 2<sup>nd</sup> Addition.

NOW, THEREFORE, it is hereby agreed by and between the parties here as follows:

1.) The Elk River Crossing Planned Unit Development Agreement is hereby amended to allow a third wall sign (2 foot by 8 foot illuminated canopy sign) for Wash-n-Fill.

2.) The words, phrases, terms, and conditions used herein shall be interpreted the same as those used in the Elk River Crossing Planned Unit Development Agreement.

3.) All other terms and conditions of the Elk River Crossing Planned Unit Development Agreement, as amended, shall remain in full force and effect and shall be binding upon the parties hereto, except to the extent clearly inconsistent with the terms and conditions of this Amendment No. 1.

IN WITNESS WHEREOF, the parties have caused this Amendment No. 1 to be duly executed in their names and on their behalf on or as of the date first above written.

ATTEST:

Patrick D. Klaers  
Patrick D. Klaers  
Its: City Administrator

CITY OF ELK RIVER  
Stephanie Klinzing  
Stephanie Klinzing  
Its: Mayor

DEVELOPER:

Drafted by:

City of Elk River  
13065 Orono Parkway  
Elk River MN 55330

By: Nick Gols  
Its: president

By: \_\_\_\_\_  
Its: \_\_\_\_\_

STATE OF MINNESOTA )  
 ) ss.  
COUNTY OF SHERBURNE )

The foregoing instrument was acknowledged before me this 15<sup>th</sup> day of May, 2001, by Stephanie Klinzing and Patrick D. Klaers, the Mayor and City Administrator, respectively, for the City of Elk River, Minnesota.

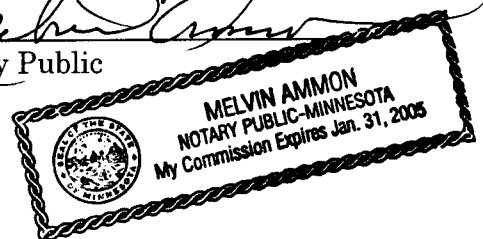


Deborah Kay Huebner  
Notary Public

STATE OF MINNESOTA )  
 ) ss.  
COUNTY OF SHERBURNE )

The foregoing instrument was acknowledged before me this 26<sup>th</sup> day of March, 2001, by Liza Robson and \_\_\_\_\_ of Associated Investors of Elk River Inc

Melvin Ammon  
Notary Public



AMENDMENT NO. 2  
to  
ELK RIVER CROSSING  
PLANNED UNIT DEVELOPMENT AGREEMENT  
CITY OF ELK RIVER, SHERBURNE COUNTY, MINNESOTA

THIS AMENDMENT NO. 2 is made as of the 2<sup>nd</sup> day of September, 2003, by and between the CITY OF ELK RIVER, Sherburne County, Minnesota (the City), and Wash-N-Fill Properties of Minnesota, LLC, a Minnesota Limited Liability Corporation, 3954 Lexington Avenue North, Shoreview, Minnesota 55126.

WITNESSETH:

WHEREAS, on December 21, 1999, the City of Elk River and Elk River Associated Investors, Inc. hereto entered into the Elk River Crossing Planned Unit Development Agreement; which agreement was amended by Amendment No. 1 effective February 20, 2001;

WHEREAS, on March 19, 2001, the Elk River City Council approved an amendment to allow a free-standing monument sign for Jiffy Lube also located on Lot 1, Block 1, Elk River Crossing 2<sup>nd</sup> Addition; and

WHEREAS, on August 18, 2003, the Elk River City Council approved an amendment to allow two additional wall signs, two additional canopy signs, and an illuminated canopy for the Wash-N-Fill site only.

NOW, THEREFORE, it is hereby agreed by and between the parties here as follows:

1.) The Elk River Crossing Planned Unit Development Agreement is hereby amended to allow a free-standing monument sign for Jiffy Lube also located on Lot 1, Block 1, Elk River Crossings 2<sup>nd</sup> Addition; and,

to allow two additional wall signs, two additional canopy signs, and an illuminated canopy for the Wash-N-Fill site only.

2.) The words, phrases, terms, and conditions used herein shall be interpreted the same as those used in the Elk River Crossing Planned Unit Development Agreement.

3.) All other terms and conditions of the Elk River Crossing Planned Unit Development Agreement, as amended, shall remain in full force and effect and shall be binding upon the parties hereto, except to the extent clearly inconsistent with the terms and conditions of this Amendment No. 2.

IN WITNESS WHEREOF, the parties have caused this Amendment No. 2 to be duly executed in their names and on their behalf on or as of the date first above written.

ATTEST:

Patrick D. Klaers  
Patrick D. Klaers  
Its: City Administrator

CITY OF ELK RIVER  
Stephanie Klinzing  
Stephanie Klinzing  
Its: Mayor

DEVELOPER:

Wash-N-Fill Properties of Minnesota, LLC

By: David Kroona  
David Kroona

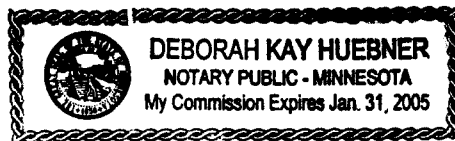
Its: Greg Muegler

STATE OF MINNESOTA )  
COUNTY OF SHERBURNE ) ss.

The foregoing instrument was acknowledged before me this 9<sup>th</sup> day of September, 2003, by Stephanie Klinzing and Patrick D. Klaers, the Mayor and City Administrator, respectively, for the City of Elk River, Minnesota.

Deborah Kay Huebner  
Notary Public

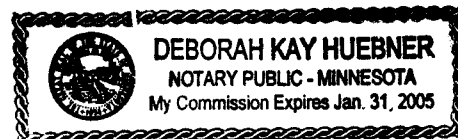
STATE OF MINNESOTA )  
COUNTY OF SHERBURNE ) ss.



The foregoing instrument was acknowledged before me this 2<sup>nd</sup> day of September, 2003, by David Kroona and of Wash-N-Fill Properties of Minnesota, LLC.

THIS INSTRUMENT DRAFTED BY:  
City of Elk River  
13065 Orono Parkway  
Elk River, Minnesota 55330

Deborah Kay Huebner  
Notary Public



- (03) Indoor and outdoor recreational facilities;
- (04) Parking ramps;
- (05) Taverns and bars;
- (06) Hotels;
- (07) Laundromats;
- (08) Theatres;
- (09) Adult book stores;
- (10) Restaurants;
- (11) Department stores.

provided that a site plan be submitted to the Planning Commission and Council for review and approval and that signage lettering be consistent with other signage within the Elk River Plaza Planned Development.

2.) The final landscaping of building plans for each lot in Elk River Plaza and Elk River Plaza Second Addition must be submitted for approval by the Elk River City Engineer prior to any construction on any part thereof.

3.) The words, phrases, terms, and conditions used herein shall be interpreted the same as those used in the Elk River Plaza Planned Development Agreement.

4.) All other terms and conditions of the Elk River Plaza Planned Development Agreement, as amended, shall remain in full force and effect and shall be binding upon the parties hereto, except to the extent clearly inconsistent with the terms and conditions of this Amendment No. 2.

IN WITNESS WHEREOF, the parties have caused this Amendment No. 2 to be duly executed in their names and on their behalf on or as of the date first above written.

ATTEST:

Patrick D. Klaers  
~~Thomas R. Publika~~ Patrick Klaers  
 Its: Clerk-Administrator

CITY OF ELK RIVER:

By: Estelle E. Gunkel  
 Estelle Gunkel  
 Its: Mayor

ELK RIVER SHOPPING CENTER  
COMPANY

By: SCHERER LIMITED  
PARTNERSHIP, Partner

By: [Signature]  
Its: General Partner

BARTHEL CONSTRUCTION, INC.

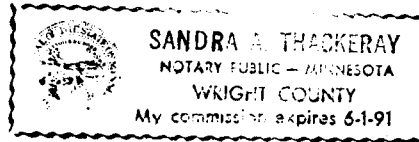
By: [Signature]  
Kenneth A. Barthel  
Its: President  
[Signature]  
Kenneth A. Barthel

STATE OF MINNESOTA  
SS.  
COUNTY OF HENNEPIN

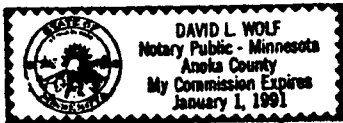
The foregoing instrument was acknowledged before me this  
15 day of December, 1987, by ~~Thomas R. Publitz and Patrick Klacs~~ and  
<sup>Esti E</sup> ~~Richard D. Hinkle~~, the Clerk-Administrator and Mayor,  
<sup>Gunkel</sup> respectively, of the City of Elk River, on behalf of the City.

[Signature]  
Notary Public

STATE OF MINNESOTA  
SS.  
COUNTY OF HENNEPIN



The foregoing instrument was acknowledged before me this  
15 day of December, 1987, by Roger H. Scherer, the  
general partner of Scherer Limited Partnership, a partner of  
Elk River Shopping Center Company, a Minnesota partnership, on  
behalf of the partnership.



[Signature]  
Notary Public

AMENDMENT NO. 3  
to  
ELK RIVER CROSSING  
PLANNED UNIT DEVELOPMENT AGREEMENT  
CITY OF ELK RIVER, SHERBURNE COUNTY, MINNESOTA

THIS AMENDMENT NO. 3 is made as of the 23<sup>rd</sup> day of November, 2004, by and between the CITY OF ELK RIVER, Sherburne County, Minnesota (the City), and Associated Investors of Elk River, Inc./Fischer Sand & Aggregate, LLP.

WITNESSETH:

WHEREAS, on December 21, 1999, the City of Elk River and Elk River Associated Investors, Inc. hereto entered into the Elk River Crossing Planned Unit Development Agreement; which agreement was amended by Amendment No. 1 effective February 20, 2001;

WHEREAS, on March 19, 2001, the Elk River City Council approved an amendment to allow a free-standing monument sign for Jiffy Lube also located on Lot 1, Block 1, Elk River Crossing 2<sup>nd</sup> Addition; and

WHEREAS, on August 18, 2003, the Elk River City Council approved an amendment to allow two additional wall signs, two additional canopy signs, and an illuminated canopy for the Wash-N-Fill site only; and

WHEREAS, on November 1, 2004, the Elk River City Council approved an amendment to allow an additional five acres into the Planned Unit Development for the construction of a furniture store.

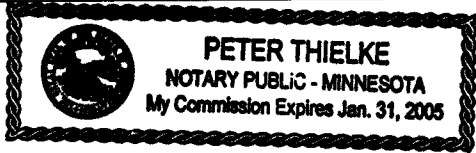
NOW, THEREFORE, it is hereby agreed by and between the parties here as follows


1.) The Elk River Crossing Planned Unit Development Agreement is hereby amended to allow an additional five acres into the Planned Unit Development for the construction of a furniture store.



STATE OF MINNESOTA )  
 ) ss.  
COUNTY OF SHERBURNE )

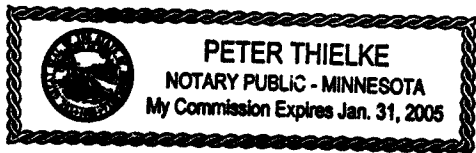
The foregoing instrument was acknowledged before me this 30<sup>th</sup> day of  
NOVEMBER, 2004, by LIZA ROBSON and  
of ASSOCIATED INVESTORS OF ELK RIVER, INC.

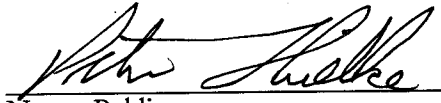


  
Notary Public

STATE OF MINNESOTA )  
 ) ss.  
COUNTY OF SHERBURNE )

The foregoing instrument was acknowledged before me this 30<sup>th</sup> day of  
NOVEMBER, 2004, by LIZA ROBSON and  
of FISHER SAND & AGGREGATE, LLC



  
Notary Public

This instrument was drafted by:


City of Elk River  
13065 Orono Parkway  
Elk River, MN 55330

Office of the County Recorder  
Sherburne County, MN

**Doc. No. 572763**

Certified filed and/or recorded on  
12-16-2004 at 08:00

Michelle Ashe, County Recorder

 Deputy Fee: \$ 19.50

STATE OF MINNESOTA

CITY OF ELK RIVER CITY COUNCIL

COUNTY OF SHERBURNE

CONDITIONAL USE  
PROCEEDINGS

CITY OF ELK RIVER

In the matter of: Associated Investors of Elk River, Inc./Fischer Sand & Aggregate, LLD

Request: Conditional Use Permit to Amend the Planned Unit Development Agreement for  
Elk River Crossing, Case No. CU 04-33

ORDER GRANTING

CONDITIONAL USE

**Associated Investors of Elk River AND Fisher Sand & Aggregate, LLD, Owners**

The above entitled matter came to be heard before the City Council on the 1st day of  
November, 2004, on a petition for a Conditional Use pursuant to the City of Elk River Zoning  
Ordinance, for the following described property:

**Lot 1, Block 3, Elk River Crossing AND  
Lot 1, Block 1, Elk River Business Park,  
Sherburne County, Minnesota**

IT IS ORDERED that a Conditional Use be granted as upon the following conditions or reasons:

1. ALL CONDITIONS OF THE ORIGINAL ELK RIVER CROSSING PUD,  
INCLUDING THE DESIGN STANDARDS.
2. THE PROPOSED MAXSUN FURNITURE STORE WILL NOT COUNT  
TOWARD THE BUILD OUT REQUIREMENT STIPULATED IN THE  
CONTRACT FOR PRIVATE REDEVELOPMENT AND THEREFORE IS  
INELIGIBLE FOR REIMBURSEMENT TO THE APPLICANT.

See Attached Amendment No. 3 to the Elk River Crossing Planned Unit  
Development Agreement.

DATED this 1st day of November, 2004.

STATE OF MINNESOTA )  
                                  )  
COUNTY OF SHERBURNE ) ss.

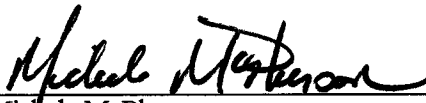
OFFICE OF CITY PLANNING  
AND ZONING

ELK RIVER

CITY OF ELK RIVER

I, Michele McPherson, Director of Planning for the City of Elk River with and in for said City, do hereby certify that I have compared the foregoing copy and Order granting a Conditional Use with the original record thereof preserved in my office, and have found the same to be a true transcript of the whole thereof.

If the proposed work described in a conditional use permit has not been substantially completed within two (2) years after its date of issuance, the permit shall expire and become void except that the Council may, following recommendations of the Planning Commission, extend the permit for an additional period determined by the council on the receipt of a request for a permit extension prior to its expiration. A Conditional Use permit authorizes only the use specified in the permit and shall expire if, for any reason, the authorized use ceases for more than one (1) year.

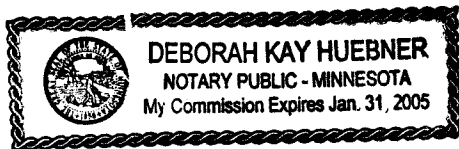
  
\_\_\_\_\_  
Michele McPherson  
Director of Planning

IN TESTIMONY WHEREOF, I have hereunto subscribed my hand at Elk River, Minnesota, in the County of Sherburne, in the City of Elk River, on the 22<sup>nd</sup> day of November, 2004.

DRAFTED BY:

City of Elk River  
Elk River, Minnesota 55330

Signed before me a NOTARY PUBLIC  
this 22<sup>nd</sup> day of November, 2004.



  
\_\_\_\_\_  
Deborah Kay Huebner


SA

Office of the County Recorder  
Sherburne County, MN

**Doc. No. 600878**

Certified filed and/or recorded on  
**09-29-2005 at 09:30**

Michelle Ashe, County Recorder

By  Deputy Fee: \$ 46.00



AMENDMENT NO. 4  
to  
ELK RIVER CROSSING  
PLANNED UNIT DEVELOPMENT AGREEMENT  
CITY OF ELK RIVER, SHERBURNE COUNTY, MINNESOTA

THIS AMENDMENT NO. 3 is made as of the 20th day of June, 2005, by and between the CITY OF ELK RIVER, Sherburne County, Minnesota (the City), and STM III, LLC.

WITNESSETH:

WHEREAS, on December 21, 1999, the City of Elk River and Elk River Associated Investors, Inc. hereto entered into the Elk River Crossing Planned Unit Development Agreement; which agreement was amended by Amendment No. 1 effective February 20, 2001;

WHEREAS, on March 19, 2001, the Elk River City Council approved an amendment to allow a free-standing monument sign for Jiffy Lube also located on Lot 1, Block 1, Elk River Crossing 2<sup>nd</sup> Addition; and

WHEREAS, on August 18, 2003, the Elk River City Council approved an amendment to allow two additional wall signs, two additional canopy signs, and an illuminated canopy for the Wash-N-Fill site only; and

WHEREAS, on November 1, 2004, the Elk River City Council approved an amendment to allow an additional five acres into the Planned Unit Development for the construction of a furniture store.

WHEREAS, on June 20, 2005, the Elk River City Council approved an amendment to allow wall signs on the north and south elevations for the interior tenants, three wall signs for the end cap tenants and no freestanding sign.

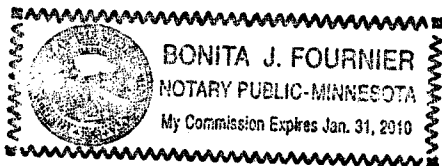
NOW, THEREFORE, it is hereby agreed by and between the parties here as follows

- 1.) The Elk River Crossing Planned Unit Development Agreement is hereby amended to allow wall signs on the north and south elevations for the interior tenants, three wall signs for the end cap tenants and no freestanding sign.



STATE OF MINNESOTA )  
 ) ss.  
COUNTY OF SHERBURNE )

The foregoing instrument was acknowledged before me this 1<sup>st</sup> day of July, 2005, by Jay Pitt and STM LLC of STM LLC.



Bonita J. Fournier  
Notary Public

This instrument was drafted by:

City of Elk River  
13065 Orono Parkway  
Elk River, MN 55330

**AMENDMENT NO. 5  
TO  
ELK RIVER CROSSING  
PLANNED UNIT DEVELOPMENT AGREEMENT  
CITY OF ELK RIVER, SHERBURNE COUNTY, MINNESOTA**

THIS AMENDMENT NO. 5 is made as of the 15<sup>th</sup> day of September, 2008, by and between the CITY OF ELK RIVER, Sherburne County, Minnesota (the City), and STM Development.

**WITNESSETH:**

WHEREAS, on December 21, 1999, the City of Elk River and Elk River Associated Investors, Inc. hereto entered into the Elk River Crossing Planned Unit Development Agreement; which agreement was amended by Amendment No. 1 effective February 20, 2001;

WHEREAS, on March 19, 2001, the Elk River City Council approved an amendment to allow a free-standing monument sign for Jiffy Lube also located on Lot 1, Block 1, Elk River Crossing 2<sup>nd</sup> Addition; and

WHEREAS, on August 18, 2003, the Elk River City Council approved an amendment to allow two additional wall signs, two additional canopy signs, and an illuminated canopy for the Wash-N-Fill site only; and

WHEREAS, on November 1, 2004, the Elk River City Council approved an amendment to allow an additional five acres into the Planned Unit Development for the construction of a furniture store.

WHEREAS, on June 20, 2005, the Elk River City Council approved an amendment to allow wall signs on the north and south elevations for the interior tenants, three wall signs for the end cap tenants and no freestanding sign.

WHEREAS, on September 15, 2008, the Elk River City Council approved an amendment to allow signage on the north side of the retail building (Dollar Tree) that is attached to the Home Depot, as illustrated on the drawings dated July 24, 2008 (attached).

NOW, THEREFORE, it is hereby agreed by and between the parties here as follows

1.) The Elk River Crossing Planned Unit Development Agreement is hereby amended to allow signage on the north side of the retail building (Dollar Tree) that is attached to the Home Depot, as illustrated on the drawings dated July 24, 2008 (attached).

2.) The words, phrases, terms, and conditions used herein shall be interpreted the same as those used in the Elk River Crossing Planned Unit Development Agreement.

3.) All other terms and conditions of the Elk River Crossing Planned Unit Development Agreement, as amended, shall remain in full force and effect and shall be binding upon the parties hereto, except to the extent clearly inconsistent with the terms and conditions of this Amendment No. 5.

IN WITNESS WHEREOF, the parties have caused this Amendment No. 5 to be duly executed in their names and on their behalf on or as of the date first above written.

ATTEST:

Lori Johnson  
Lori Johnson  
Its: City Administrator

CITY OF ELK RIVER

Stephanie Klinzing  
Stephanie Klinzing  
Its: Mayor

DEVELOPER:

STM Development

By:

Its:

OWNER:

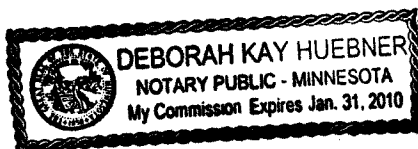
Maurice Frank Children's Trust  
1625 Virginia Avenue South  
Spring Lake Park, MN 55426

612 919 4754

Justin Holman  
Property Management  
STM

STATE OF MINNESOTA )  
 ) ss.  
COUNTY OF SHERBURNE )

The foregoing instrument was acknowledged before me this 13<sup>th</sup> day of October, 2008, by Stephanie Klinzing and Lori Johnson, the Mayor and City Administrator, respectively, for the City of Elk River, Minnesota.



Deborah Kay Huebner  
Notary Public

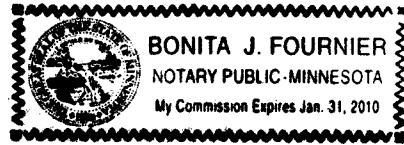
STATE OF MINNESOTA )  
 ) ss.  
COUNTY OF SHERBURNE )

The foregoing instrument was acknowledged before me this 2<sup>nd</sup> day of October, 2008, by Barbara Bounds and ST Property of ST Development & Property management.

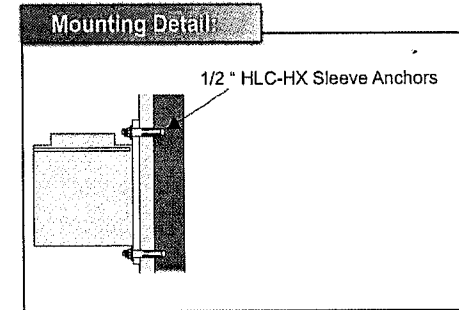
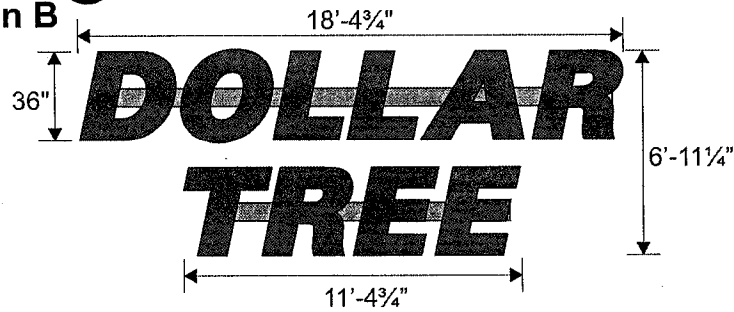
Bonita J. Fournier  
Notary Public

This instrument was drafted by:

City of Elk River  
13065 Orono Parkway  
Elk River, MN 55330



Sign B



**Electrical Specifications:**  
 TETRA MAX GREEN LED  
 (5) PS-12 @ .85 each  
 Total Amps: 4.25

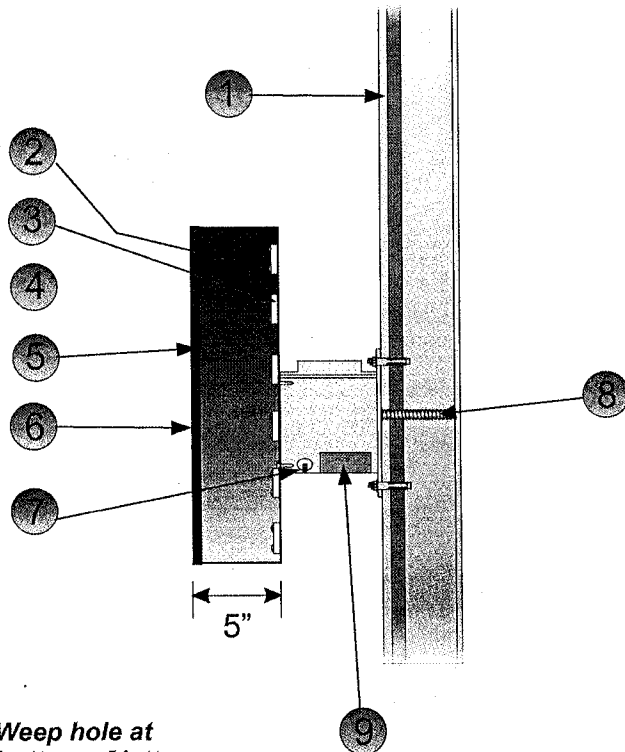
**General Notes**

- A. Customers junction box. In compliance w/ NEC 800-21.
- B. 20 amp general use disconnect switch as per NEC 380-14a per circuit per NEC 600-21 as per NEC 500-21.
- C. UL listed and approved time clock.
- D. Primary wiring in NEC approved conduit.
- E. Junction box mounted behind fascia or nearby w/ 1/4" x 2 1/2" Lexcon concrete screws.
- F. UL external disconnect switch wired to transformer per NEC 600-6

**Notes:**

This sign is intended to be installed in accordance with the requirements of Article 600 of the National Electrical Code and/or other applicable local codes. This includes proper grounding and bonding of the sign.

**All signs are U.L. listed**



Weep hole at bottom of letter can

**Sign Specifications:**

1. Existing Facade: Concrete Block
2. .040 Aluminum bronze letter returns
3. TETRA MAX GREEN LED
4. .065 Aluminum backs (interior of sign can painted white for maximum illumination)
5. .125" x 1" dark bronze trim cap
6. 3/16" white acrylic faces with first surface applied vinyl to be Dollar Tree green
7. Waterproof disconnect switch
8. Primary electrical feed
9. Transformers

Attachment Detail



Dollar Tree # 3852  
 18011 Zane Street Northwest  
 Elk River, MN 55330

07/24/08



Drawing by: Darin Houston

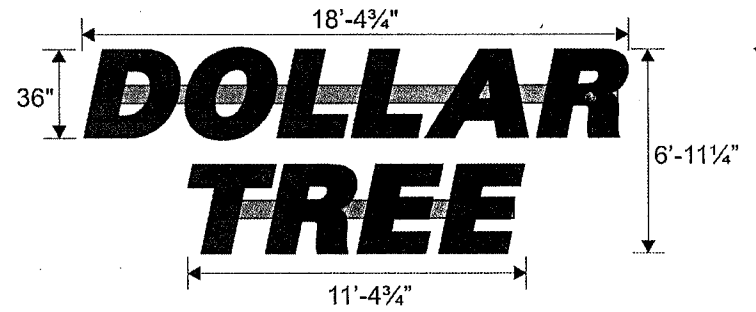
1.800.213.3331

**Sign B**

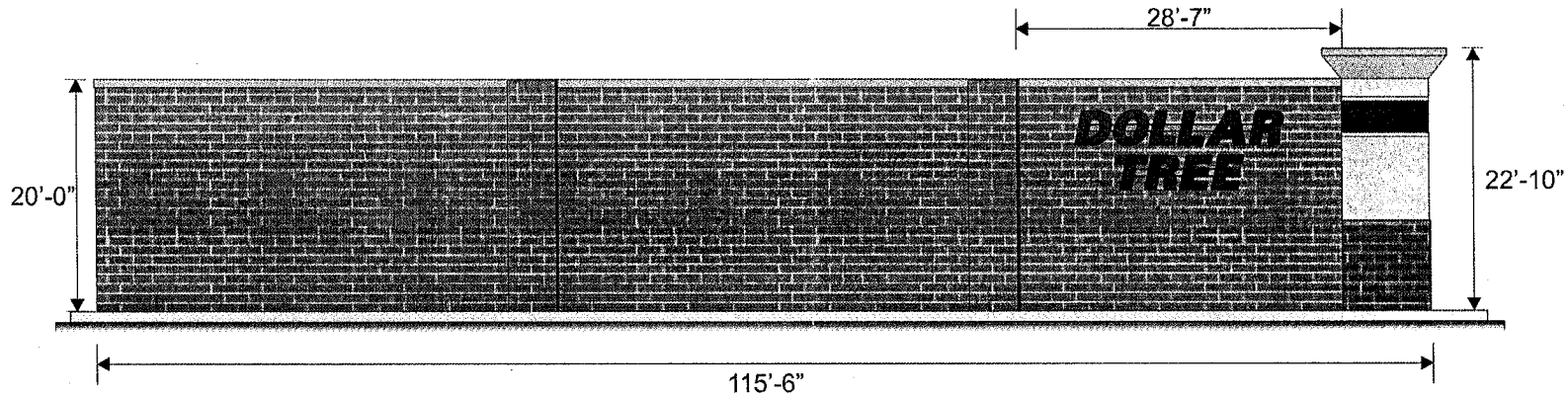
(1) set of 36" internally illuminated channel letters mounted on a raceway painted to match the facade = 128 square feet

Total signage this elevation = 128 square feet

All signs are UL listed



top of sign to grade= 17'-6 1/4"  
bottom of sign to grade= 10'-7"



**DATE RECEIVED**

7-24-08

Side (North) Elevation

Scale 3/32" = 1'-0"



Dollar Tree # 3852

18011 Zane Street Northwest  
Elk River, MN 55330

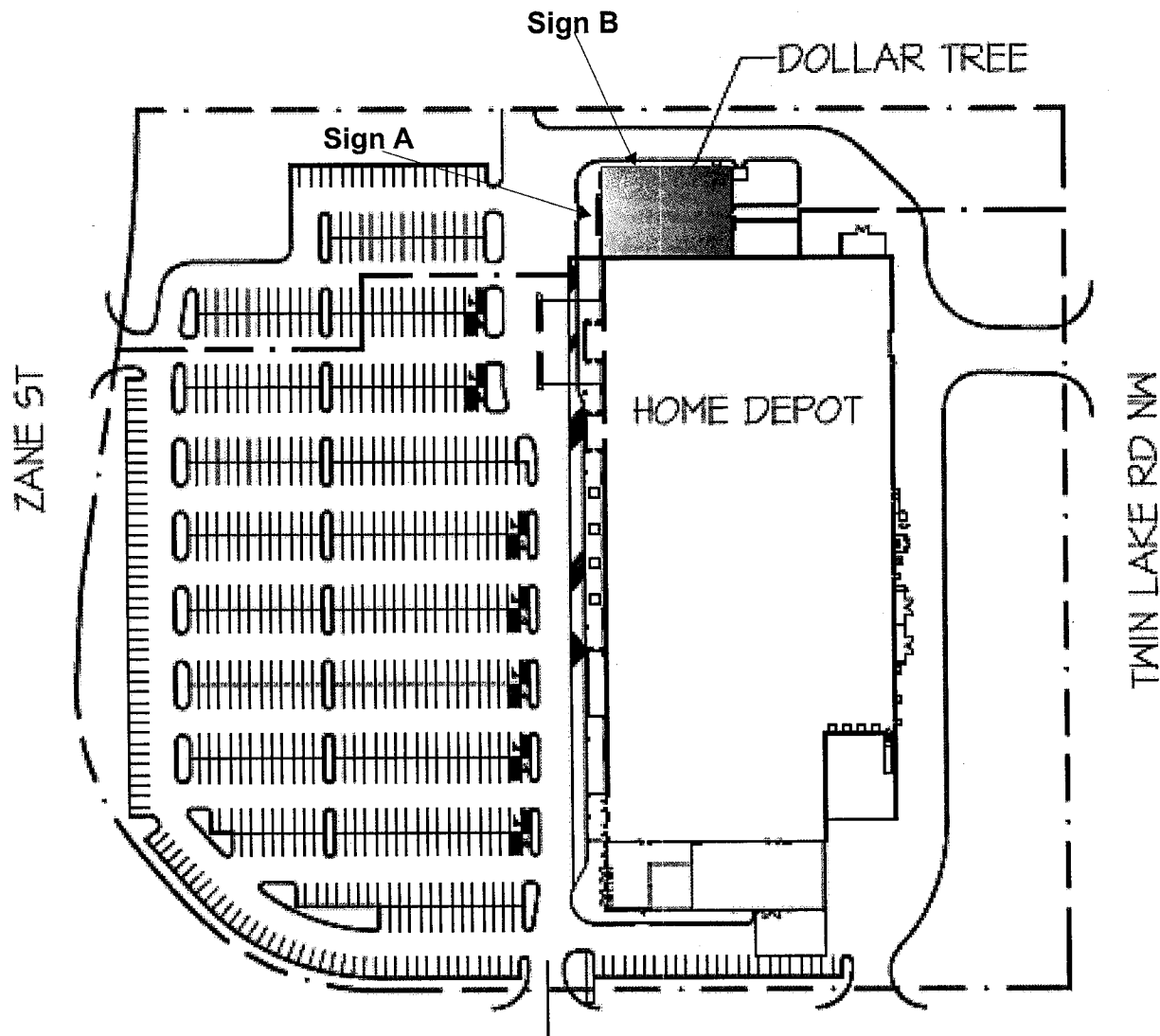
07/23/08

Drawing by: Darin Houston



1.800.213.3331

Sign A is existing.



Conceptual Site Plan

DATE RECEIVED  
7-24-08



Dollar Tree # 3852

18011 Zane Street Northwest  
Elk River, MN 55330

01/31/08

Drawing by: Darin Houston

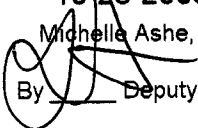


**Doc. No. 681983**

Certified filed and/or recorded on

10-23-2008 at 02:25

Michelle Ashe, County Recorder

By  Deputy Fee: \$ 46.00



681983

STATE OF MINNESOTA

CITY OF ELK RIVER CITY COUNCIL

COUNTY OF SHERBURNE

CONDITIONAL USE  
PROCEEDINGS

CITY OF ELK RIVER

In the matter of: Anchor Sign, Inc, on behalf of Dollar Tree

Request: Conditional Use Permit to Amend the Planned Unit Development Agreement for  
Elk River Crossing, Case No. CU 08-18

ORDER GRANTING

CONDITIONAL USE

**Maurice Frank Children's Trust, Owners**

*Sept. 2008*

*15<sup>th</sup>*

The above entitled matter came to be heard before the City Council on the 1st day of ~~November, 2004~~, on a petition for a Conditional Use pursuant to the City of Elk River Zoning Ordinance, for the following described property:

**Lot 1, Block 1, Elk River Crossing Seventh Addition,  
Sherburne County, Minnesota**

IT IS ORDERED that a Conditional Use be granted as upon the following conditions or reasons:

**See Attached Amendment No. 5 to the Elk River Crossing Planned Unit Development Agreement.**

DATED this 15<sup>th</sup> day of September, 2008.





# Request for Action

**To**  
Planning Commission

**Item Number**  
5.1

**Meeting Date**  
February 25, 2025

**Prepared By**  
Zack Carlton, Community Development Director

**Item Description**  
Land Use Amendment, Zone Change, and Ordinance Amendment: Specht Family Farm Residential Development (PUD), 16330 US Highway 10, Capstone Homes

**Reviewed by**  
Chris Leeseberg

## Action Requested

Recommend, by motion, approval of a Land Use Amendment updating the city's comprehensive plan to align with the proposed residential and commercial areas, subject to the condition that the associated Zone Change (ZC 24-06) and Ordinance Amendment (OA 24-05) are approved by the City Council.

Recommend, by motion, approval of a zone change amending the city's zoning map to designate parcels for Highway Commercial and Planned Unit Development (PUD) uses consistent with the development proposal, and subject to the condition that the associated Land Use Amendment (LU 24-05) and Ordinance Amendment (OA 24-05) are approved by the City Council.

Recommend, by motion, approval of an ordinance amendment to establish a residential Planned Unit Development, subject to the following conditions:

1. The City Council must approve the associated Land Use Amendment (LU 24-05) and Zone Change (ZC 24-06).
2. An Environmental Assessment Worksheet (EAW) must be reviewed in accordance with all state and local policies and receive a negative finding for an Environmental Impact Statement (EIS).
3. MnDOT and the city shall approve an access location and configuration that supports the traffic needs of the proposed development.
4. The MnDNR must provide written approval of the work proposed within the Wild and Scenic River District.
5. Provide additional vegetative screening along the non-wooded boundary line with the Oliver Kelley Farm. The design should incorporate feedback from the Kelley Farm.
6. The cost of sewer and water trunk extensions to the development shall be financed by the developer. If oversizing of water pipes is required, Elk River Municipal Utilities (ERMU) will reimburse the developer for the cost to oversize the infrastructure.
7. All proposed streets, trails, and sidewalks shall follow the city's engineering design standards.
8. The lots currently identified as 50-foot wide must be updated to include both 55 and 50-foot lots at a reasonable distribution.
8. The developer shall satisfy the park dedication requirements with a 50/50 combination of land and cash dedication. The land dedication shall be split between the lower, non-developable, floodway and an upland

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*A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity*



park of approximately 6 to 8 acres.

9. The cash dedication shall be a pro-rated amount for each lot and paid at the time of final platting.

### **Background/Discussion**

The developer, Capstone Homes, has submitted a series of applications to support their proposed development on the property commonly referred to as the Specht Family Farm - now known as Oakwater Ridge. The site is approximately 250 acres in size and includes a significant area of frontage along the Mississippi River and Highway 10. The area currently includes three different zoning districts (FAST, R-1d, and Business Park) and is guided by the Comprehensive Plan for residential and commercial uses.

The proposal includes 533 new single-family parcels, and 5.17 acres slated for future commercial uses. Approval of the project requires amendments to the Comprehensive Plan (Land Use), zoning amendments to create a Planned Unit Development (PUD) for the residential areas and rezone the commercial areas to C-3 (Highway Commercial), and an ordinance amendment establishing the standards for the PUD.

The developer is seeking approval of the PUD to establish the framework for the project. Future applications and reviews will include a preliminary plat and final plats. These will provide more details on the grading, stormwater, utility work, and engineering for the development. Discussions should seek to achieve a cohesive project with a series of standards that the developer will follow as the project progresses toward final design.

### **Planning Commission**

The Commission held a public hearing to discuss the project on January 28, 2025. Two family members spoke during the meeting with one outlining the beauty and uniqueness of the place and expressing hope that the development is respectful of the memories the family has growing up there. The second family member noted that he was in support of the development. Additionally, representatives from the Kelley Farm discussed the development's proximity to the historical site and noted that they are working with the developer to create a landscape plan to buffer the site respectfully.

The Planning Commission, along with another representative from the public, expressed concerns about the narrower (29-foot wide) streets proposed as part of the project. Their concerns were with maintenance and emergency vehicles driving through the street, specifically turning around in the cul-de-sacs. The developer stated that the narrow streets contribute to a safer project by slowing traffic through the neighborhoods. The streets also significantly reduce the amount of impervious surfaces throughout the project. The Planning Commission asked for additional information from the public works director regarding his recommendation to remain with the current street width standard of 34 feet. His response is noted below:

*Public works has long supported the Council standards for street widths on public roadways in Elk River. Our current standard provides for a level of service in residential areas that supports parking on both sides while keeping sufficient space in the center to allow movement for delivery vehicles, garbage trucks, and public service/safety vehicles. We understand several communities in the greater metropolitan area have migrated to narrower streets generally at the request of developers as an economic savings to the cost of their projects. We maintain our position that entertaining the request for narrower streets comes at the cost of long-term maintenance inefficiencies where plow trucks cannot safely navigate between vehicles parked on both sides of the roadway. Our current standard provides a balance between these through movements and parking on both sides to ensure residents have the parking space they desire for themselves and their guests.*

The Commission was generally supportive of the layout of the development and the narrower lots - down to 50 feet wide. However, the Commission was not prepared to make a recommendation until they had received more information from the public works director. The developer also encouraged the commission members to drive through a few developments that were noted during the discussion.

The Planning Commission closed the public hearing, but did not make a recommendation and continued their discussion to February 25, 2025.

## **City Council**

The City Council conducted a review of the proposed project on February 18, 2025. The council discussed the Parks and Recreation Commission's recommendation to split the park dedication approximately 50-50 between cash and land. This includes an 8-acre credit in the floodway, outside the 10-year OHW, and another 8 acres above the bluff line. While the commission prefers the park area above the bluff to be centrally located on the property and adjacent to the bluff, the council indicated they are open to considering alternative locations that do not impact the applicant's most valuable parcels.

Regarding the proposed narrower streets, the council stated they would not support deviations from the city standard and that all public streets must maintain the required 34-foot width.

The proposed smaller lot sizes (50-foot) generated mixed reactions, leading to a back-and-forth discussion between the council and the applicant. There was discussion regarding both 55- and 50-foot lots. There seemed to be a consensus that having all the smaller lots at 50-feet wide was too small, but a mix of may be more acceptable.

The council did not act on any of the applications and recommended that the applicant return for a work session to continue discussions.

The applicant would like the Planning Commission to review their current application in light of all the feedback from the board and council discussions thus far. Staff have included conditions of approval that are in-line with these discussions, but they can be modified as part of the recommendation to the City Council.

## **Financial Impact**

None

## **Mission/Policy/Goal**

Opportunity to live, work, and play.

Responsibly grow.

## **Attachments**

1. City Council Staff report dated 2-18-2025



# Request for Action

**To**  
City Council

**Item Number**  
7.5

**Meeting Date**  
February 18, 2025

**Prepared By**  
Zack Carlton, Community Development Director

**Item Description**  
Land Use Amendment, Zone Change, and Ordinance Amendment: Specht Family Farm Residential Development (PUD), 16330 US Highway 10, Capstone Homes

**Reviewed by**  
Chris Leeseberg  
Cal Portner  
Tina Allard

**Action Requested**  
Open the public hearing, receive public comment, and continue the hearing to March 17, 2025.

## Background/Discussion

The developer, Capstone Homes, has submitted a series of applications to support their proposed development on the property commonly referred to as the Specht Family Farm. The site is approximately 250 acres in size and includes a significant area of frontage along the Mississippi River and Highway 10. The area currently includes three different zoning districts (FAST, R-1d, and Business Park) and is guided by the Comprehensive Plan for residential and commercial uses.

The proposal includes 533 new single-family parcels, and 5.17 acres slated for future commercial uses. Approval of the project requires amendments to the Comprehensive Plan (Land Use), zoning amendments to create a Planned Unit Development (PUD) for the residential areas and rezone the commercial areas to C-3 (Highway Commercial), and an ordinance amendment establishing the standards for the PUD.

The developer is seeking approval of the PUD to establish the framework for the project. Future applications and reviews include a preliminary plat and a final plat. These will provide more details on the grading, stormwater, utility work, and engineering for the development. Discussions and conditions from the City Council should seek to achieve a cohesive project with a series of standards that the developer will follow as the project progresses toward final design.

## Planning Commission

The Commission held a public hearing to discuss the project on January 28, 2025. Two family members spoke during the meeting with one outlining the beauty and uniqueness of the place and expressing hope that the development is respectful of the memories the family has growing up there. The second family member noted that he was in support of the development. Additionally, representatives from the Kelley Farm discussed the development's proximity to the historical site and noted that they are working with the developer to create a landscape plan to buffer the site respectfully.

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*A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity*



The Planning Commission, along with another representative from the public, expressed concerns about the narrower (29-foot wide) streets proposed as part of the project. Their concerns were with maintenance and emergency vehicles driving through the street, specifically turning around in the cul-de-sacs. The developer stated that the narrow streets contribute to a safer project by slowing traffic through the neighborhoods. The streets also significantly reduce the amount of impervious surfaces throughout the project. The Planning Commission asked for additional information from the public works director regarding his recommendation to remain with the current street width standard of 34 feet. His response is noted below:

*Public works has long supported the Council standards for street widths on public roadways in Elk River. Our current standard provides for a level of service in residential areas that supports parking on both sides while keeping sufficient space in the center to allow movement for delivery vehicles, garbage trucks, and public service/safety vehicles. We understand several communities in the greater metropolitan area have migrated to narrower streets generally at the request of developers as an economic savings to the cost of their projects. We maintain our position that entertaining the request for narrower streets comes at the cost of long-term maintenance inefficiencies where plow trucks cannot safely navigate between vehicles parked on both sides of the roadway. Our current standard provides a balance between these through movements and parking on both sides to ensure residents have the parking space they desire for themselves and their guests.*

The Commission was generally supportive of the layout of the development and the narrower lots - down to 50 feet wide. However, the Commission was not prepared to make a recommendation until they had received more information from the public works director. The developer also encouraged the commission members to drive through a few developments that were noted during the discussion.

The Planning Commission did not make a recommendation and continued their discussion to February 25, 2025.

### **Park and Recreation**

The Park and Recreation Commission reviewed this project for park dedication on January 8, 2025, and February 12, 2025. During their February meeting, they reviewed the proposed park areas and felt there needs to be a larger focus on a park above the bluff area and not in the floodway. They also believed it was important to not allocate all park dedication to land as a cash dedication would help fund the improvements to the park in this area. They recommend that the required dedication be split roughly 50-50 between land and cash with a significant increase in the land dedication above the bluff line. The park area above the bluff should be located near the center of the property and adjacent to the bluff.

The developer will respond to these recommendations in advance of the next City Council meeting. Recommended conditions to achieve the Commission's goals will be included for Council consideration.

### **Financial Impact**

None

### **Mission/Policy/Goal**

Opportunity to live, work, and play  
Responsibly grow  
Support the growth and development of the community

### **Attachments**

1. Planning Commission Memo dated 1-28-2025
2. Site Rendering dated 2-10-2025



# Request for Action

**To**  
Planning Commission

**Item Number**  
5.5

**Meeting Date**  
January 28, 2025

**Prepared By**  
Zack Carlton, Community Development Director

**Item Description**  
Land Use Amendment, Zone Change, and Ordinance Amendment: Specht Family Farm Residential Development (PUD), 16330 Us Highway 10, Capstone Homes

**Reviewed by**  
Chris Leeseberg

## Action Requested

Recommend, by motion, approval of a Land Use Amendment updating the city's comprehensive plan to align with the proposed residential and commercial areas, subject to the condition that the associated Zone Change (ZC 24-06) and Ordinance Amendment (OA 24-05) are approved by the City Council.

Recommend, by motion, approval of a zone change amending the city's zoning map to designate parcels for Highway Commercial and Planned Unit Development (PUD) uses consistent with the development proposal, and subject to the condition that the associated Land Use Amendment (LU 24-05) and Ordinance Amendment (OA 24-05) are approved by the City Council.

Recommend, by motion, approval of an ordinance amendment to establish a residential Planned Unit Development, subject to the following conditions:

1. The City Council must approve the associated Land Use Amendment (LU 24-05) and Zone Change (ZC 24-06).
2. An Environmental Assessment Worksheet (EAW) must be reviewed in accordance with all state and local policies and receive a negative finding for an Environmental Impact Statement (EIS).
3. MnDOT and the city shall approve an access location and configuration that supports the traffic needs of the proposed development.
4. The MnDNR must provide written approval of the work proposed within the Wild and Scenic River District.
5. Provide additional vegetative screening along the non-wooded boundary line with the Oliver Kelley Farm. The vegetative design should incorporate feedback from the Kelly Farm staff.
6. The cost of sewer and water trunk extensions to the development shall be financed by the developer. If oversizing of water pipes is required, Elk River Municipal Utilities (ERMU) will reimburse the developer for the cost to oversize the infrastructure.
7. All proposed streets, trails, and sidewalks shall follow the city's engineering design standards.
8. Recommendations from the Park and Recreation Commission shall be incorporated into the proposed plans.

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*A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity*



## **Background/Discussion**

The developer, Capstone Homes, has submitted a series of applications to support their proposed development on the property commonly referred to as the Specht Farm. The site is approximately 250 acres in size and includes a significant area of frontage along the Mississippi River. The area currently includes three different zoning districts (FAST, R-1d, and Business Park) and is guided by the Comprehensive Plan for residential and commercial uses.

The development proposal includes 533 new single-family parcels, and 5.17 acres slated for future commercial uses. Approval of the project requires amendments to the Comprehensive Plan (Land Use), zoning amendments to create a PUD for the residential areas and rezone the commercial areas to C-3 (Highway Commercial, and an ordinance amendment establishing the standards for the PUD areas.)

Currently, the developer is seeking approval of the PUD to establish the framework for the project. Future applications and reviews by the Planning Commission include a preliminary plat, which will provide more details on the grading, stormwater, utility work, and engineering for the development. Discussions and conditions from the commission should seek to achieve a cohesive project with a series of standards that the developer will follow as the project progresses towards final design.

## **Comprehensive Plan**

The Comprehensive Plan currently guides the site for Mixed Residential and Highway Business uses. The proposal maintains the same land use classifications but re-aligns the areas to match the property lines proposed with the project.

The proposed commercial areas and future uses align with the land use guidance, and realigning the land use plan to follow the proposed uses is supported by staff.

The mixed residential category supports a range of housing types, with the goal of a gentle increase in density when compared to a typical single-family neighborhood. While the proposal doesn't include townhomes or multifamily uses, the smallest single-family parcels are 50-feet wide, which increases the overall density of the area.

## **Access and Streets**

The development is located between Highway 10 and the river and abuts the Oliver Kelley Farm historic site along the southern boundary. The main access to the site will be from an improved access point at Highway 10 and 165<sup>th</sup> Ave. This is the current location of the driveway serving the property. City staff and the developer have been working with MnDOT to design this access, which is anticipated to require signalization and a possible j-hook. Development of this project is contingent upon MnDOT's approval of this access.

Additional access to the development includes an extension of the existing Yale Street dead-end on the north end of the project and a frontage road along Highway 10.

The project reserves land for a future interchange, but that is not part of this project. It is anticipated that the improvements at 165<sup>th</sup> Ave will provide access for the foreseeable future and the interchange will be constructed when funding and traffic demands allow.

Internal circulation is provided by public streets, there are no private drives included in the project. A sidewalk is included on one side of each residential street, including the frontage road. The developer has requested a narrower street section for many of the interior streets. A typical city street is 34-feet wide, and

the developer requests approval of a narrower public street at 29-feet. The request suggests that narrower roads are naturally traffic calming and will create a safer neighborhood. City engineering staff desire all public streets to comply with city standards.

### **City Utilities**

All parcels will be served by city sewer and water. This will require the extension of existing trunk services, which are the responsibility of the developer. Additional details regarding city services will be included in a future preliminary plat application.

### **Housing Types**

The proposal includes four different lot sizes, ranging in size from 50-foot-wide lots with a 35-foot-wide building pad, up to 100-foot lots overlooking the Mississippi River. The majority of the parcels are between 65 and 75-feet wide and are consistent with much of the development that has been approved in recent years.

The largest lots are located along the river and have a minimum lot size of 20,000 square feet. These standards are a requirement of the Wild and Scenic River District for any parcel that abuts the river. Parcels within the district, but not along the river, are required to be at least 75-feet wide with a minimum size of 10,000 square feet. Proposed setbacks for these lots are 30-feet for the front yard and require a minimum of 15-feet between buildings – generally consistent with the typical R-1c zoning district.

The majority of the lots, 284 in total, are 65-feet-wide. As noted earlier, this has been the most common parcel size constructed in the city for a number of years. The proposed setbacks of 25-feet for the front yard and 15 feet between buildings is also typical of these product types.

The smallest lot sizes are clustered closest to Highway 10 and are 50-feet-wide with a front yard setback of 25-feet and maintaining 15-feet between buildings. The lot size does create some concern for on-street parking and placement of utilities. Parking has typically been a concern for smaller lots, and the developer is proposing restricting driveway width to 16-feet to maximize curb space for parking. This leaves approximately 34-feet between driveways in the best-case scenario, which is typically not enough for two vehicles. Curves in the roadway, fire hydrants, and mailboxes will further restrict the available on street parking. The applicant has included a parking study evaluating the impacts of the smaller lots on available street parking.

City staff have concerns with the narrow lots and the impacts this will have on the project. The Planning Commission should discuss the proposal and the impacts of the smallest lot sizes to the community. One alternative may be to strategically mix the smaller lots throughout the area, so the density is not concentrated, and street parking concerns are minimized.

### **Environmental**

The site is located within the Wild and Scenic River District, which includes additional performance standards and open space requirements. Generally, the standards require larger lots closer to the river, and a net open space requirement for all areas within the district. The developer has been working closely with the DNR to ensure the proposal satisfies the standards.

The attachments include additional information regarding compliance with the Wild and Scenic standards. According to the exhibits, and conversations with the DNR, the project complies with the ordinance, and the PUD will include additional performance standards to ensure long-term compliance.

Additionally, due to the development's size and location, an Environmental Assessment Worksheet (EAW) is required. An EAW is a state mandated analysis of the impacts a proposed development project may generate. It includes a wide variety of topics, including traffic, historical elements, natural features, to name a few. The applicant has submitted the EAW for initial review, and the city will follow the state mandated review process. Ultimate approval of the EAW requires a review from the City Council.

### **Park and Receptions**

A project of this size includes a variety of park amenities. Additionally, the location along the Mississippi River provides additional opportunities for river access. Plans identify approximately 42 acres of park and open space, including a trail running from north to south, with a large segment along the river. The Park and Recreation Commission is reviewing the suggested park information and will make a recommendation to the City Council.

### **Comprehensive Plan Amendment**

The requested amendment to the approved plan is generally consistent with the established guidance. The changes will better align the land use guidance with property lines. This provides clear guidance for the current and future landowners.

### **Zone Change**

Amendments to the approved zoning map must be consistent with the Comprehensive Plan. The current zoning is generally consistent with the approved land use map, but the requested changes will improve consistency. The proposed commercial areas align with land currently guided for similar uses, and along with the requested changes, improves overall compliance. Second, the proposed residential PUD is consistent with the mixed residential guidance of the plan. The zone change will also align with planned parcel lines, improving overall consistency.

### **PUD Ordinance**

The proposed PUD ordinance outlines the development standards for the subdivision. Any standards not specifically noted within the ordinance will follow the general development standards outlined in Chapter 30.

### **Financial Impact**

None

### **Mission/Policy/Goal**

Appropriately govern in an ever-changing environment.

Meet changing needs - agile.

Opportunity to live, work, and play.

Responsibly grow.

Support the growth and development of the community.

### **Attachments**

1. Location Map
2. Comprehensive Plan Exhibit
3. Zoning Comparison
4. Elk River - Specht Farm Master PUD Exhibits - FINAL
5. Specht Farm Proposed PUD Standards



**SITE**

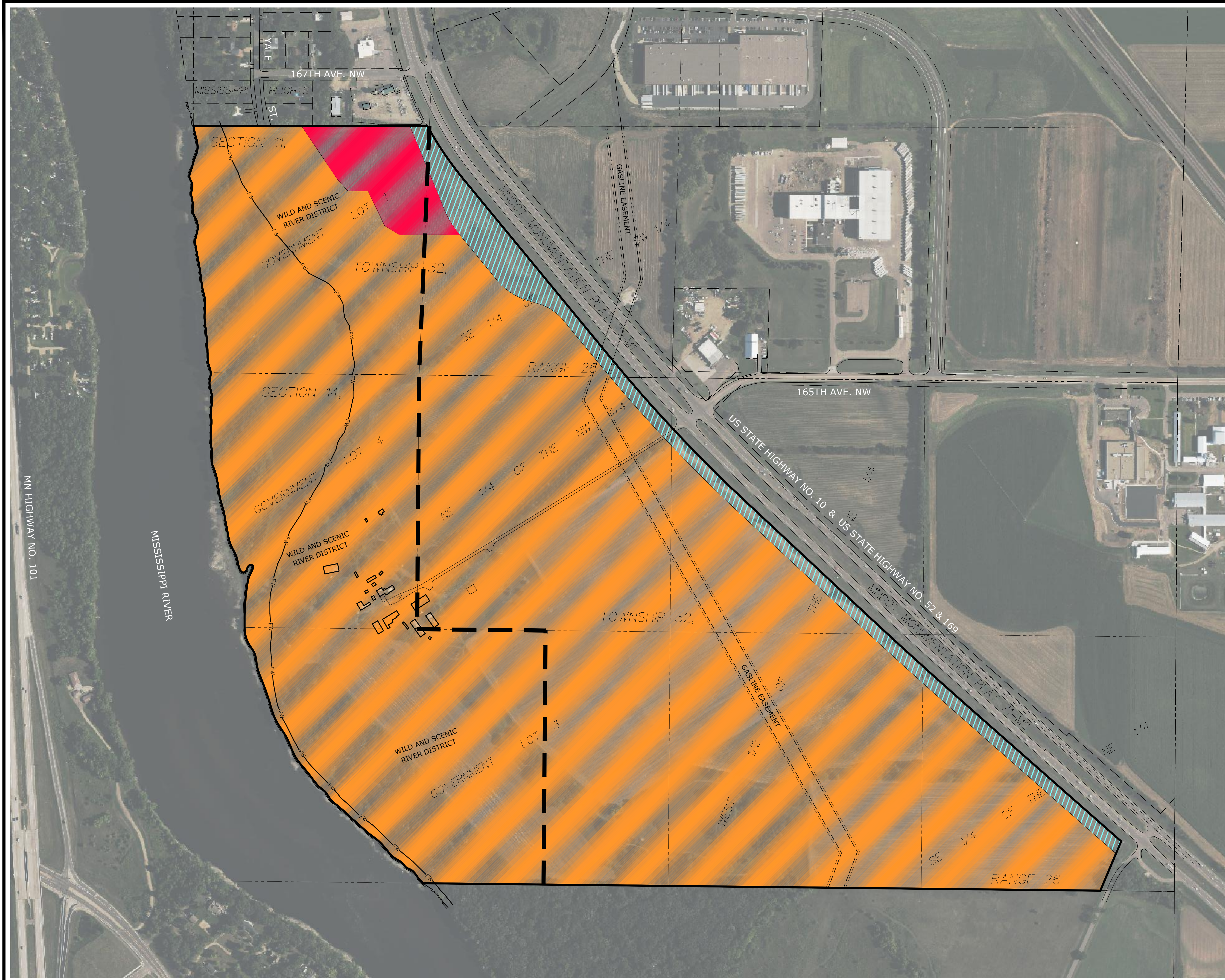
# Project Location Map

Capstone Homes, Inc.

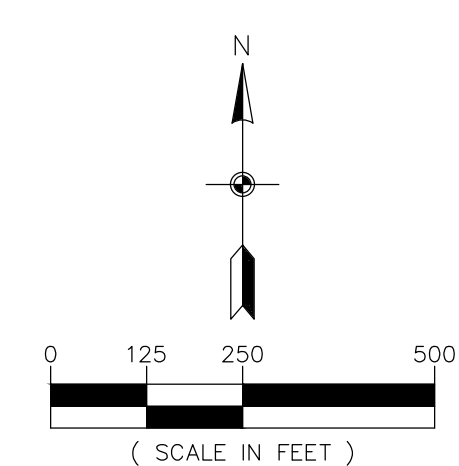
Land Use Amendment  
Zone Change  
Ordinance Amendment

Case No: LU 24-05, ZC 24-06, OA 24-05





- LEGEND**
- HIGHWAY BUSINESS
  - MIXED RESIDENTIAL
  - PUBLIC ROW (FOR FUTURE INTERCHANGE AND FRONTAGE ROAD)
  - FLOODWAY
  - WILD AND SCENIC RIVER DISTRICT

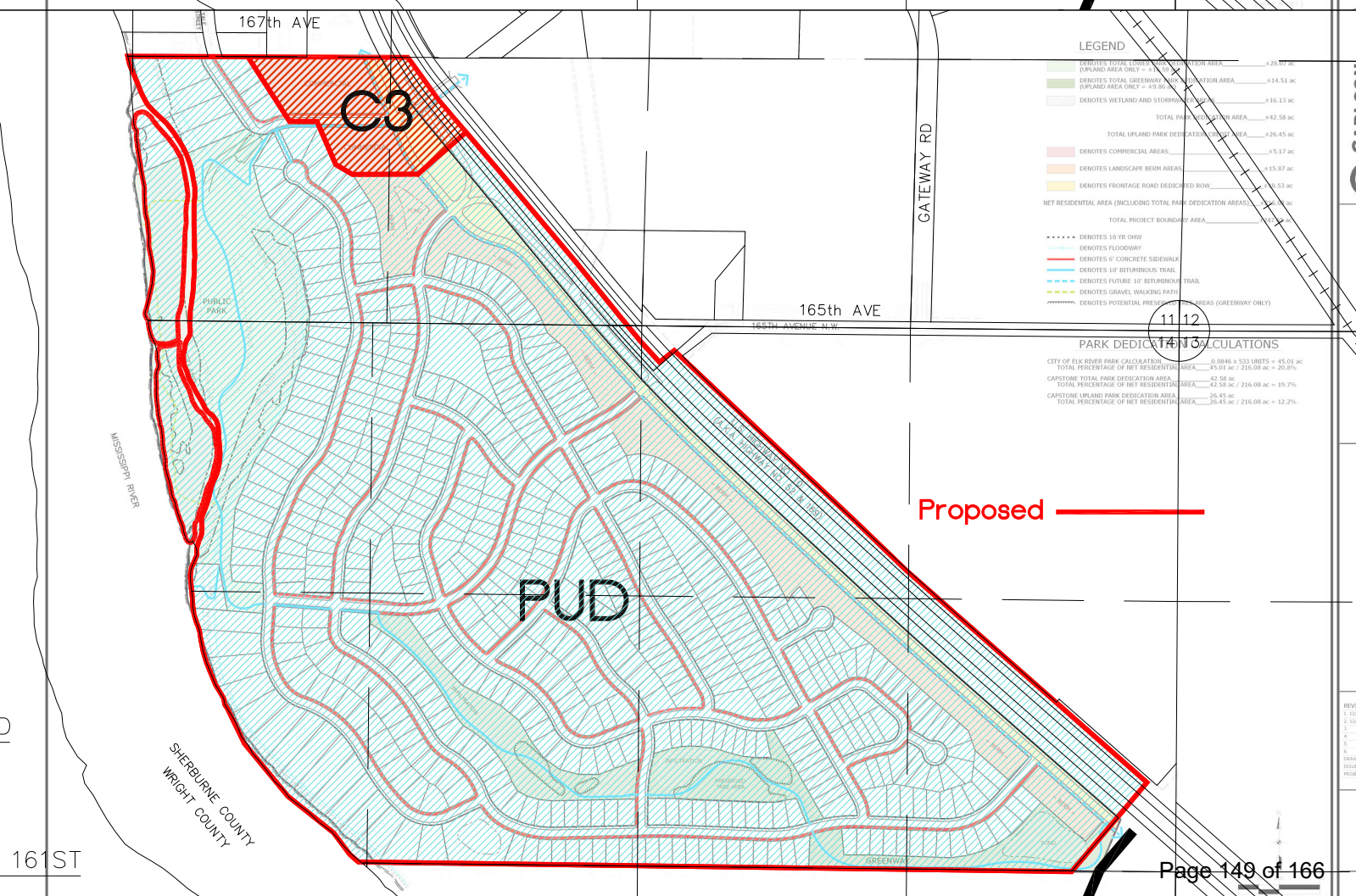
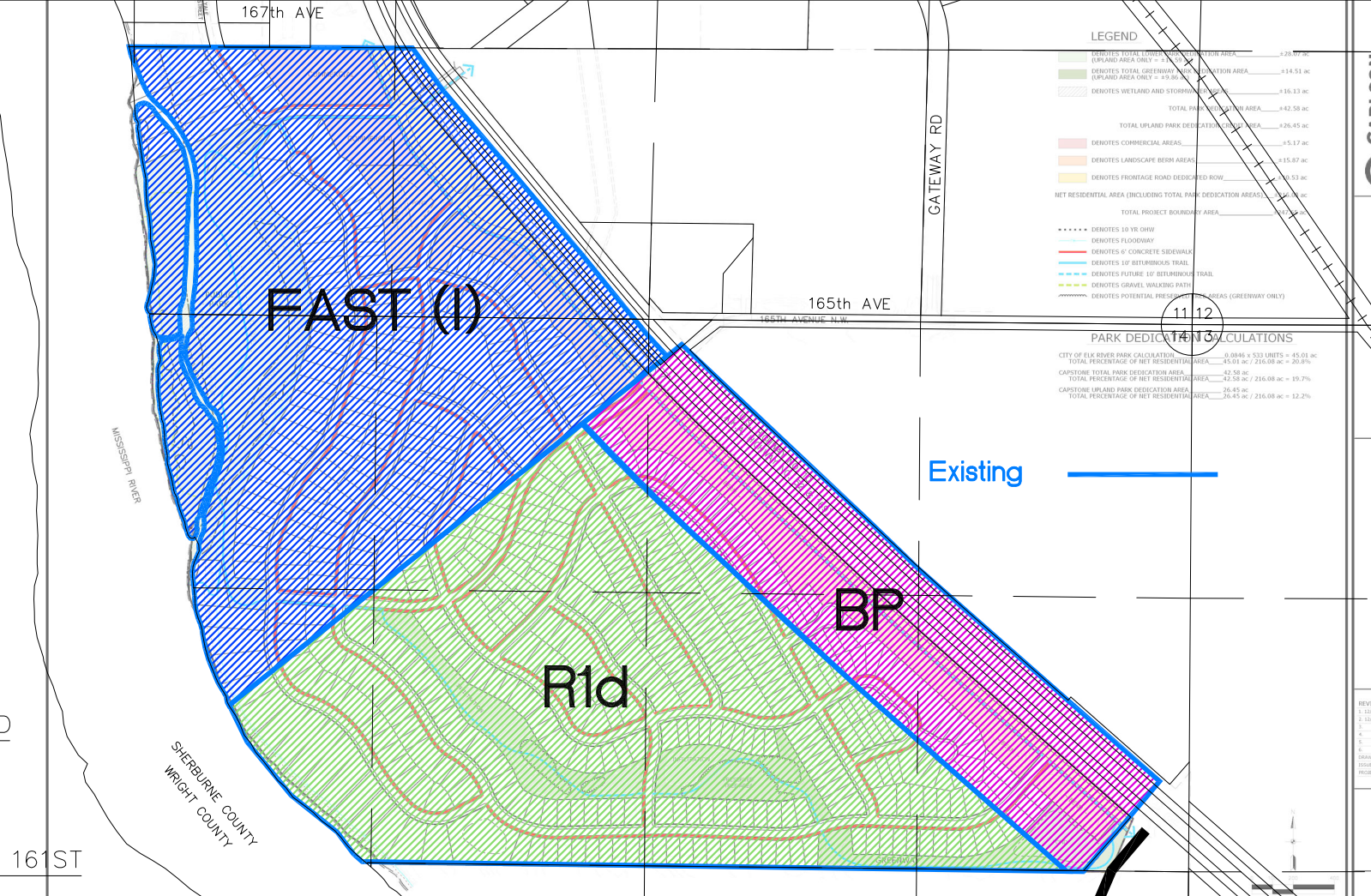


**COMP PLAN  
AMENDMENT EXHIBIT**

**SPECHT FARM**  
Elk River, Minnesota

**CAPSTONE HOMES**  
14015 Sunfish Lake Blvd. - Suite 400  
Ramsey, MN 55303

REVISIONS	
1.	
2.	
3.	
4.	
5.	
6.	
DRAWN BY:	C#
ISSUE DATE:	12/18/2024
FILE NO:	XXX



# SPECHT FAMILY FARM

Elk River, MN

# CAPSTONE

— H O M E S —



**PLANNED UNIT DEVELOPMENT**

December 30<sup>th</sup>, 2024

# Project Narrative

Capstone Homes is submitting for consideration a Master Plan Ordinance Amendment for the development of 250 acres of Mississippi Riverfront, into an exciting new mixed residential community. Capstone Homes is a locally owned hometown builder in the Twin Cities Metro area whose mission is to build quality homes at an exceptional value, while serving and positively impacting the community. Capstone offers modern, open concept floor plans with a huge selection of available structural and cosmetic options to fit any family's needs. We are the largest privately owned builder in the Twin Cities Area and because we are family owned and operated, we treat each home and homeowner with the utmost care, just like we would our own families.

The Specht Family Farm is located at 16330 US Highway 10, running adjacent to the Mississippi River. Nearly 1 mile of this roughly 250 acre neighborhood are home sites that are on and overlooking the Mississippi River. We have worked with city staff and the Department of Natural Resources to establish a neighborhood that protects the Wild and Scenic nature of the Mississippi river throughout the acreage site. Through multiple workshops, meetings and site walks we feel confident that the Master PUD plan submitted meets the design requirements defined in both the City of Elk River Ordinance and the State Wild and Scenic River District.

Details associated with this Neighborhood development plan are outlined in this submission for review by City Staff, Planning Commissioners, and City Council members. We look forward to meeting with the Council, Commissioners and respective boards as we share our vision for meeting the growing residential needs of Elk River.

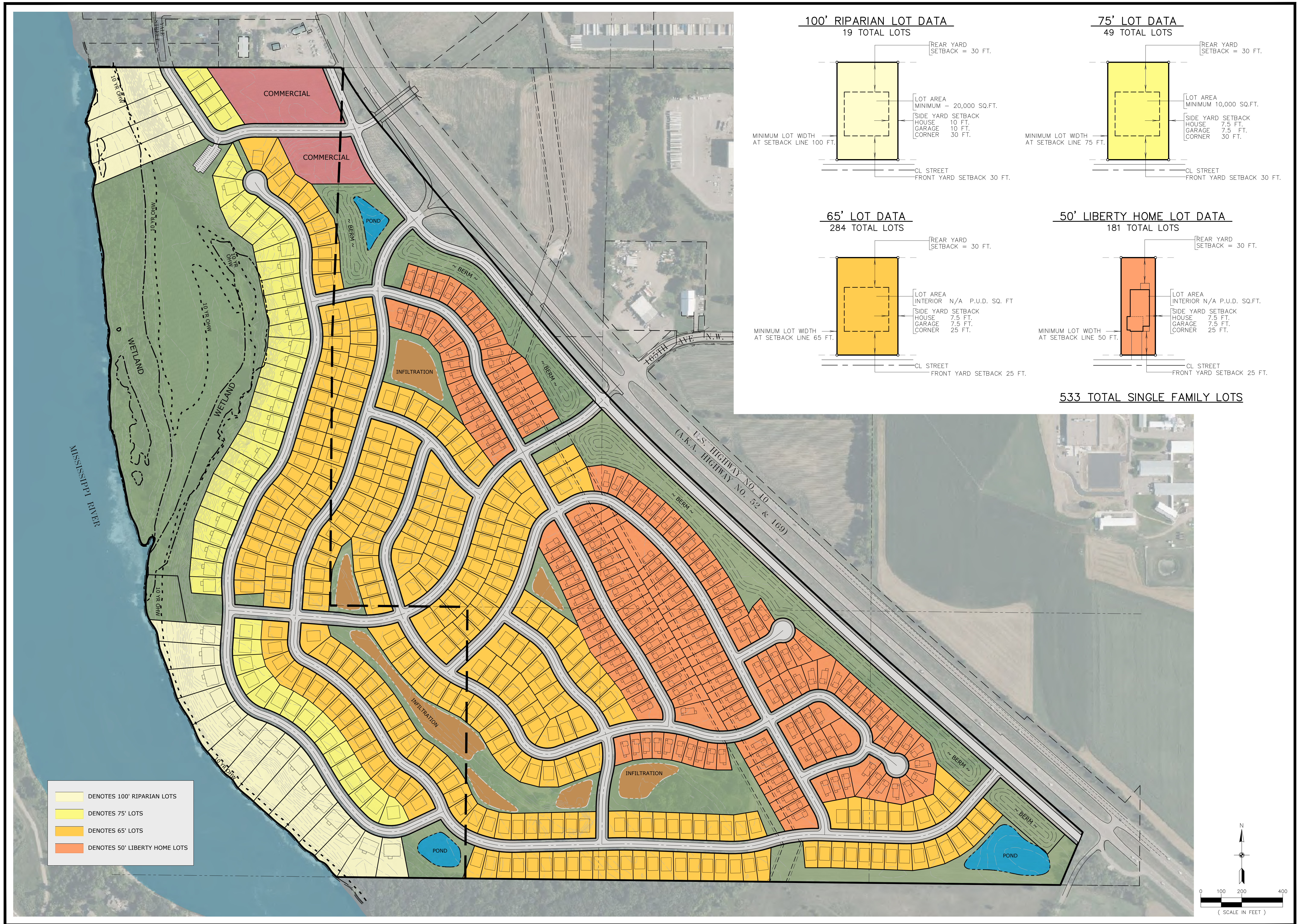
# Existing Conditions



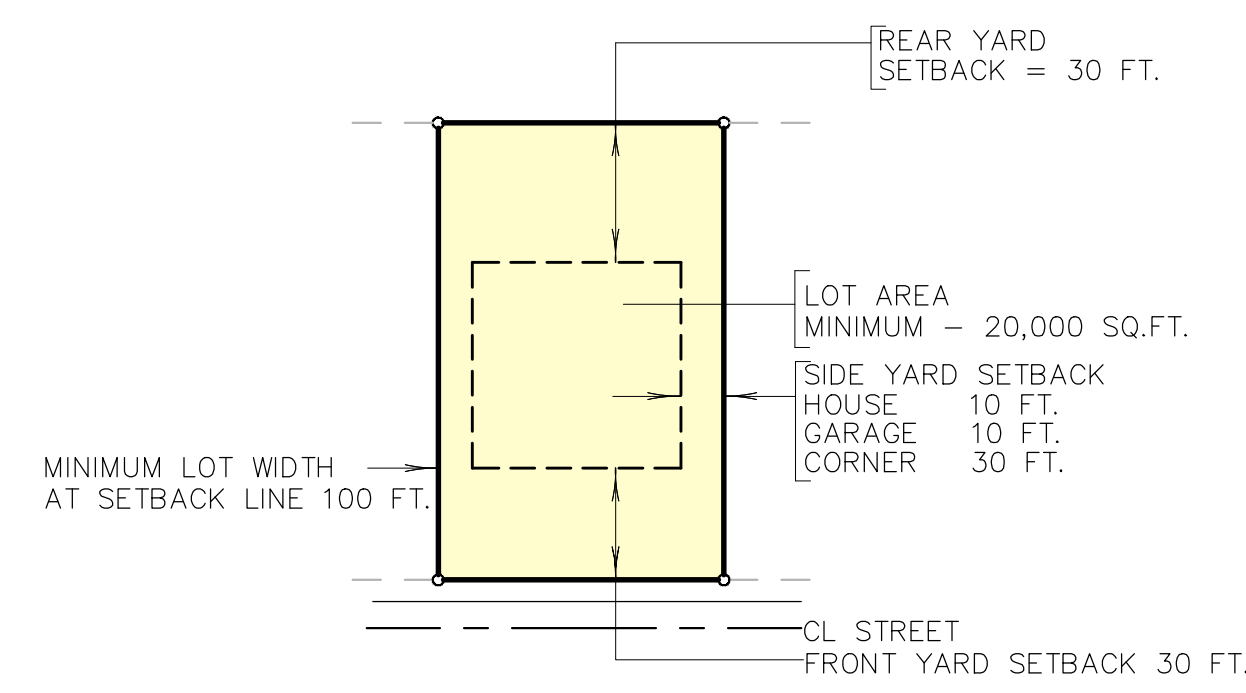
14015 Sunfish Lake Blvd NW, Ste 400  
Ramsey, MN 55303

3890 Pheasant Ridge Dr. NE, Ste 100  
Blaine, MN 55449

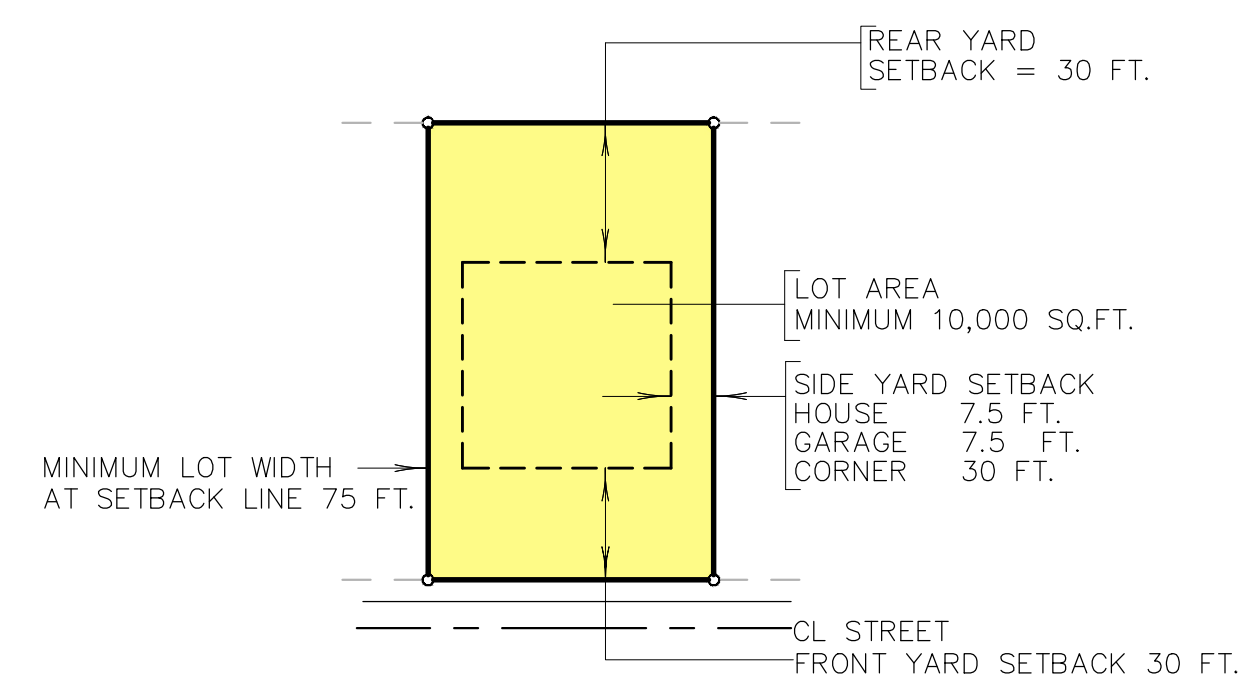
2500 Shadywood Road, Ste 130  
Orono, MN 55331



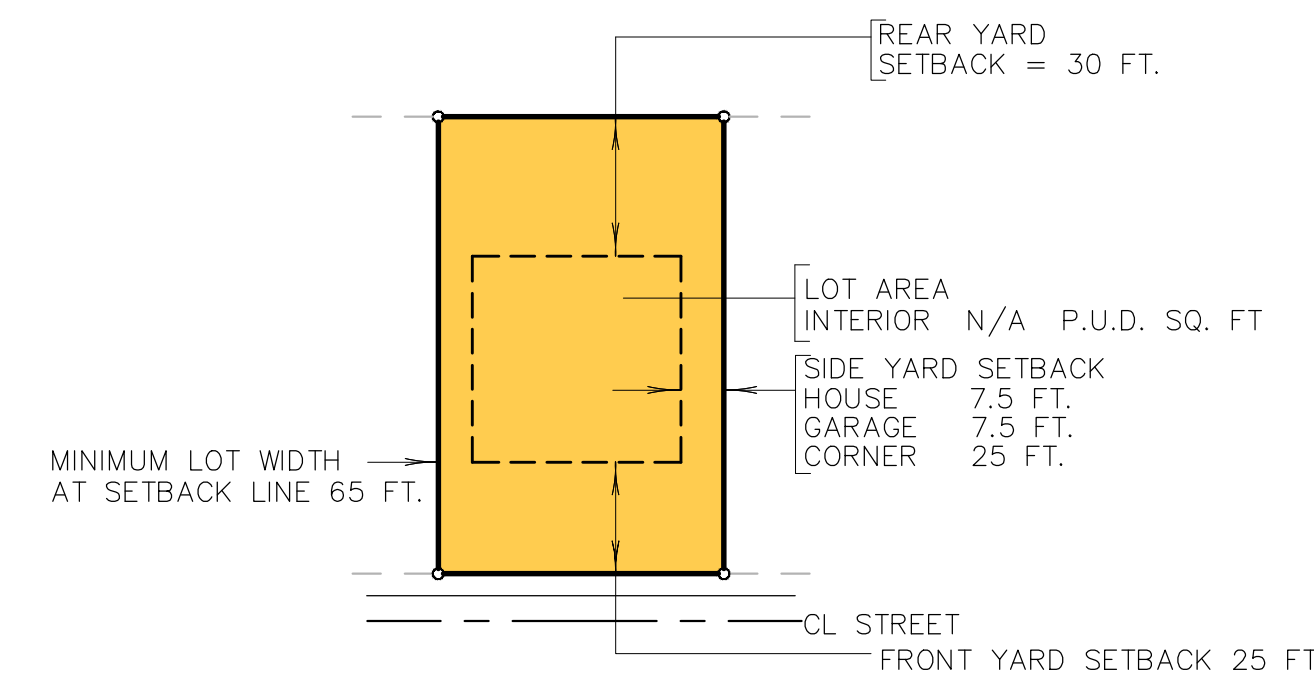
**100' RIPARIAN LOT DATA**  
19 TOTAL LOTS



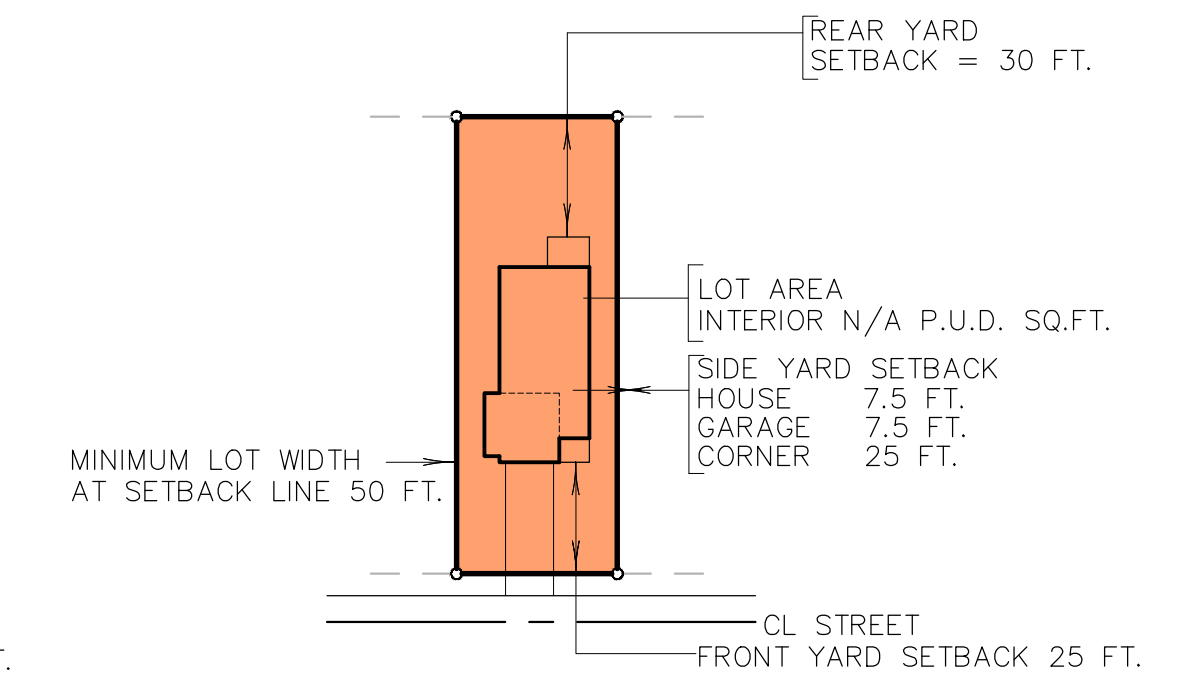
**75' LOT DATA**  
49 TOTAL LOTS



**65' LOT DATA**  
284 TOTAL LOTS



**50' LIBERTY HOME LOT DATA**  
181 TOTAL LOTS



**533 TOTAL SINGLE FAMILY LOTS**

- DENOTES 100' RIPARIAN LOTS
- DENOTES 75' LOTS
- DENOTES 65' LOTS
- DENOTES 50' LIBERTY HOME LOTS

**CARLSON McCAIN**  
 ENGINEERING SURVEYING ENVIRONMENTAL  
 3890 PHEASANT RIDGE DRIVE NE, SUITE 100, BLAINE, MN 55449  
 TEL: 763.489.7900 | FAX: 763.489.7959 | CARLSONMCCAIN.COM

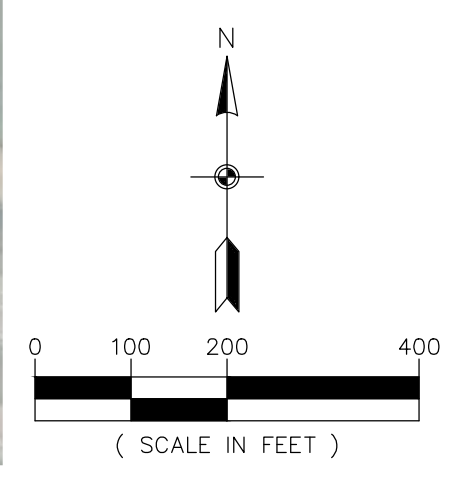
**LOT EXHIBIT**  
**SPECHT FAMILY FARM**  
 Elk River, Minnesota

**CAPSTONE HOMES, INC.**  
 14015 Sunfish Lake Boulevard, Suite 400  
 Ramsey, Minnesota 55303

**REVISIONS**

1.	12/05/24 - Revise Layout.
2.	12/13/24 - Revise Layout.
3.	
4.	
5.	
6.	

DRAWN BY: C# / RJR  
 ISSUE DATE: 11/22/2024  
 PROJECT NO: 9556



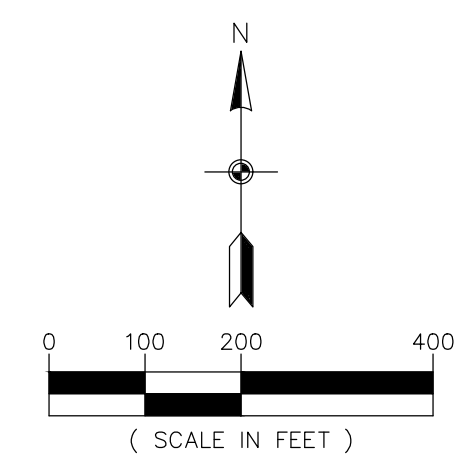


**LEGEND**

- DENOTES TOTAL LOWER PARK DEDICATION AREA ±28.07 ac  
(UPLAND AREA ONLY = ±16.59 ac)
- DENOTES TOTAL GREENWAY PARK DEDICATION AREA ±14.51 ac  
(UPLAND AREA ONLY = ±9.86 ac)
- DENOTES WETLAND AND STORMWATER AREAS ±16.13 ac
- TOTAL PARK DEDICATION AREA ±42.58 ac
- TOTAL UPLAND PARK DEDICATION CREDIT AREA ±26.45 ac
- DENOTES COMMERCIAL AREAS ±5.17 ac
- DENOTES LANDSCAPE BERM AREAS ±15.87 ac
- DENOTES FRONTAGE ROAD DEDICATED ROW ±10.53 ac
- NET RESIDENTIAL AREA (INCLUDING TOTAL PARK DEDICATION AREAS) ±216.08 ac
- TOTAL PROJECT BOUNDARY AREA ±247.65 ac
- DENOTES 10 YR OHW
- DENOTES FLOODWAY
- DENOTES 6' CONCRETE SIDEWALK
- DENOTES 10' BITUMINOUS TRAIL
- DENOTES FUTURE 10' BITUMINOUS TRAIL
- DENOTES GRAVEL WALKING PATH
- DENOTES POTENTIAL PRESERVED TREE AREAS (GREENWAY ONLY)

**PARK DEDICATION CALCULATIONS**

CITY OF ELK RIVER PARK CALCULATION	0.0846 x 533 UNITS = 45.01 ac
TOTAL PERCENTAGE OF NET RESIDENTIAL AREA	45.01 ac / 216.08 ac = 20.8%
CAPSTONE TOTAL PARK DEDICATION AREA	42.58 ac
TOTAL PERCENTAGE OF NET RESIDENTIAL AREA	42.58 ac / 216.08 ac = 19.7%
CAPSTONE UPLAND PARK DEDICATION AREA	26.45 ac
TOTAL PERCENTAGE OF NET RESIDENTIAL AREA	26.45 ac / 216.08 ac = 12.2%



**REVISIONS**

1.	12/09/24 - Revise Layout.	
2.	12/13/24 - Revise Wetlands, Areas	
3.		
4.		
5.		
6.		

DRAWN BY: RJR  
ISSUE DATE: 12/05/2024  
PROJECT NO: 9556



Disc Golf Course



Pavilion



Playground

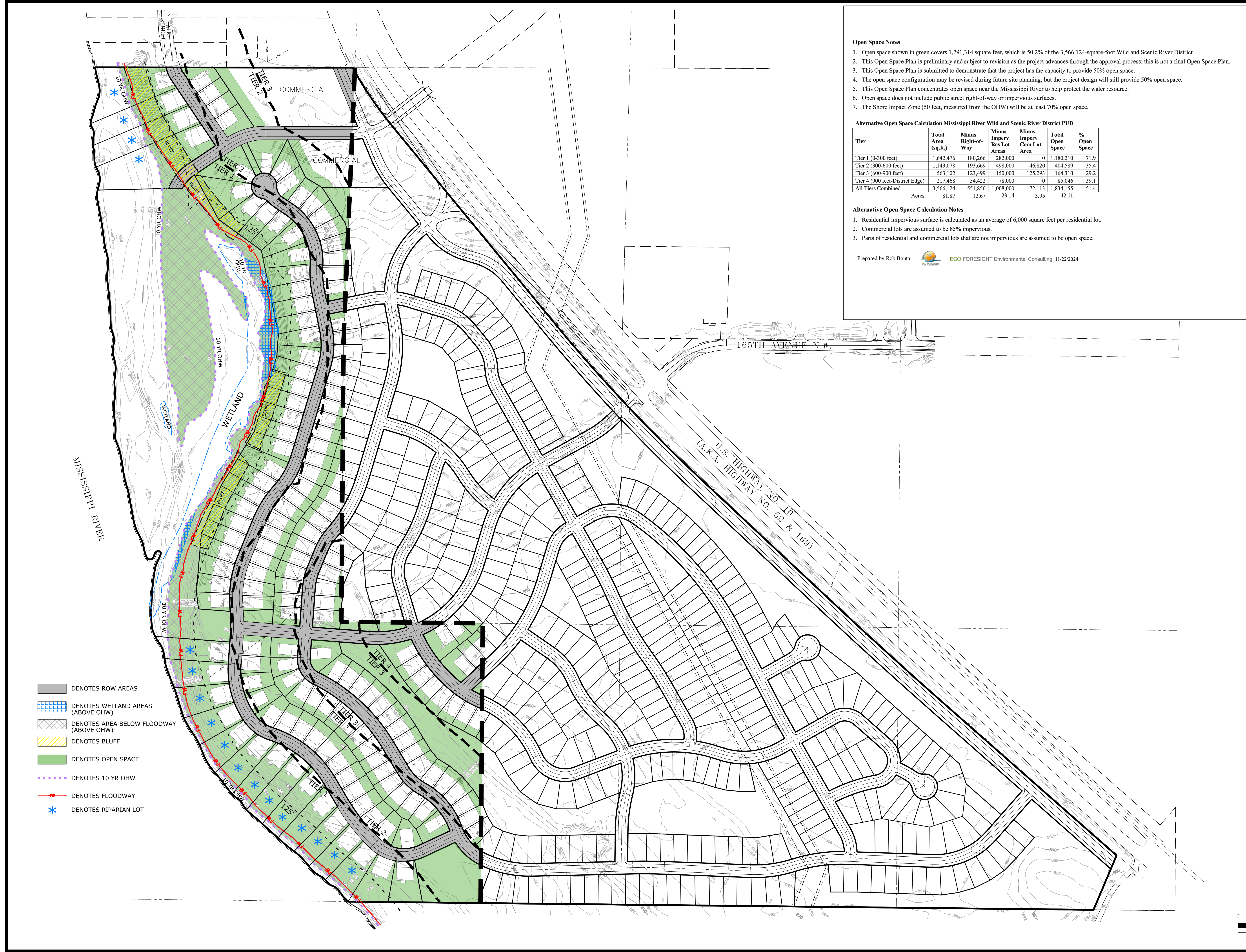


Floodplain Forest / Trails



Floating Dock

These illustrations are park opportunities showing possible amenities. Actual amenities will vary as determined by City depending on current park practices, needs and budget at the time of construction.



- DENOTES ROW AREAS
- DENOTES WETLAND AREAS (ABOVE OHW)
- DENOTES AREA BELOW FLOODWAY (ABOVE OHW)
- DENOTES BLUFF
- DENOTES OPEN SPACE
- DENOTES 10 YR OHW
- DENOTES FLOODWAY
- DENOTES RIPARIAN LOT

**Open Space Notes**

1. Open space shown in green covers 1,791,314 square feet, which is 50.2% of the 3,566,124-square-foot Wild and Scenic River District.
2. This Open Space Plan is preliminary and subject to revision as the project advances through the approval process; this is not a final Open Space Plan.
3. This Open Space Plan is submitted to demonstrate that the project has the capacity to provide 50% open space.
4. The open space configuration may be revised during future site planning, but the project design will still provide 50% open space.
5. This Open Space Plan concentrates open space near the Mississippi River to help protect the water resource.
6. Open space does not include public street right-of-way or impervious surfaces.
7. The Shore Impact Zone (50 feet, measured from the OHW) will be at least 70% open space.

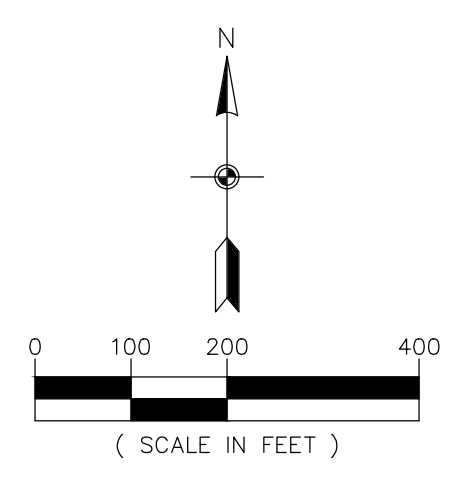
**Alternative Open Space Calculation Mississippi River Wild and Scenic River District PUD**

Tier	Total Area (sq.ft.)	Minus Right-of-Way	Minus Imperv Res Lot Areas	Minus Imperv Com Lot Area	Total Open Space	% Open Space
Tier 1 (0-300 feet)	1,642,476	180,266	282,000	0	1,180,210	71.9
Tier 2 (300-600 feet)	1,143,078	193,669	498,000	46,820	404,589	35.4
Tier 3 (600-900 feet)	563,102	123,499	150,000	125,293	164,310	29.2
Tier 4 (900 feet-District Edge)	217,468	54,422	78,000	0	85,046	39.1
All Tiers Combined	3,566,124	551,856	1,008,000	172,113	1,834,155	51.4
Acres:	81.87	12.67	23.14	3.95	42.11	

**Alternative Open Space Calculation Notes**

1. Residential impervious surface is calculated as an average of 6,000 square feet per residential lot.
2. Commercial lots are assumed to be 85% impervious.
3. Parts of residential and commercial lots that are not impervious are assumed to be open space.

Prepared by Rob Bouts ECO FORESIGHT Environmental Consulting 11/22/2024



**CARLSON MCCAIN**  
ENGINEERING SURVEYING ENVIRONMENTAL

3890 PHEASANT RIDGE DRIVE NE, SUITE 100, BLAINE, MN 55449  
TEL: 763.488.7900 | FAX: 763.488.7950 | CARLSONMCCAIN.COM

**WILD AND SCENIC RIVER DISTRICT  
OPEN SPACE EXHIBIT**

**SPECHT FAMILY FARM**  
Elk River, Minnesota

**CAPSTONE HOMES, INC.**  
14015 Sunfish Lake Boulevard, Suite 400  
Ramsey, Minnesota 55303

**REVISIONS**

1.	
2.	
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4.	
5.	
6.	

DRAWN BY: CA  
ISSUE DATE: 11/22/2024  
PROJECT NO: 9556

1 of 1



Mississippi River Wild and Scenic River District PUD Density Evaluation

Tier	Project Area				Allowed PUD Density						
	Total Area (sq.ft.)	Wetlands (sq.ft.)	Bluffs (sq.ft.)	Net Suitable Area (sq.ft.)	Minimum Sq. Ft. Per Unit	Base Density (Units)	Density Increase Multiplier	Density With Multiplier	Density With Transfer	Density <sup>1</sup> Proposed	Unit Transfer to Next Tier
Tier 1 (0-300 feet) Riparian	606,293	0	53,290	553,003	20,000	27.7	1.5	41.5	41.5	17	24.5
Tier 1 (0-300 feet) Nonriparian	1,036,183	40,937	95,494	899,752	10,000	90.0	1.5	135.0	135.0	30	105.0
Tier 2 (300-600 feet)	1,143,078	0	0	1,143,078	10,000	114.3	2.0	228.6	358.1	83	275.1
Tier 3 (600-900 feet)	563,102	0	0	563,102	10,000	56.3	2.0	112.6	374.7	25	350.7
Tier 4 (900 feet-District Edge)	217,468	0	0	217,468	10,000	21.7	2.0	43.5	394.5	13	381.5
<b>Total</b>	<b>3,566,124</b>	<b>40,937</b>	<b>148,784</b>	<b>3,376,403</b>		<b>310.0</b>		<b>561.2</b>		<b>168</b>	
	Acre: 81.87	0.94	3.42	77.51							

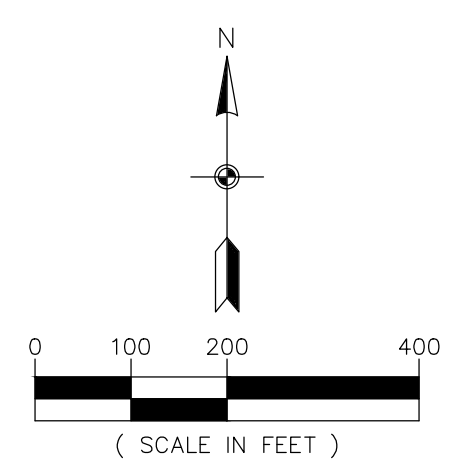
<sup>1</sup>Density proposed includes 1 commercial lot in Tier 2 and 1 commercial lot in Tier 3.

Notes

- The FEMA 10-year flood elevation was fit to topography field surveyed by Carlson McCain and used as the OHW line.
- The FEMA 100-year flood elevation was fit to topography field surveyed by Carlson McCain and used as the Floodway boundary.
- Bluffs were delineated according to the definition contained in the City of Elk River Zoning Ordinance (at least 30% slope rising to at least 25 feet above the OHW).
- River lots have structure setbacks of 125 feet from the OHW and 30 feet from the top of bluff; all riparian lots are at least 100 feet wide at the OHW.
- Impervious surface coverage of riparian lots will not exceed 25% of the lot area.
- For nonriparian lots, the post-development 5-year frequency event peak discharge rate will be no greater than the 5-year historic undeveloped peak discharge rate.

Prepared by Rob Bouta ECO FORESIGHT Environmental Consulting 11/22/2024

- DENOTES LOT AREAS
- DENOTES ROW AREAS
- DENOTES WETLAND AREAS (ABOVE OHW)
- DENOTES AREA BELOW FLOODWAY (ABOVE OHW)
- DENOTES BLUFF
- DENOTES COMMERCIAL LOTS
- DENOTES PUBLIC OPEN SPACE
- DENOTES 10 YR OHW
- DENOTES FLOODWAY
- DENOTES RIPARIAN LOT
- DENOTES 100' LOTS
- DENOTES 75' NON-RIPARIAN LOTS
- DENOTES 65' NON-RIPARIAN LOTS



**CARLSON MCCAIN**  
ENGINEERING SURVEYING ENVIRONMENTAL  
3690 PHEASANT RIDGE DRIVE NE, SUITE 100, BLAINE, MN 55449  
TEL: 763.488.7900 | FAX: 763.488.7959 | CARLSONMCCAIN.COM

**WILD AND SCENIC RIVER DISTRICT PUD DENSITY EXHIBIT**  
**SPECHT FAMILY FARM**  
Elk River, Minnesota

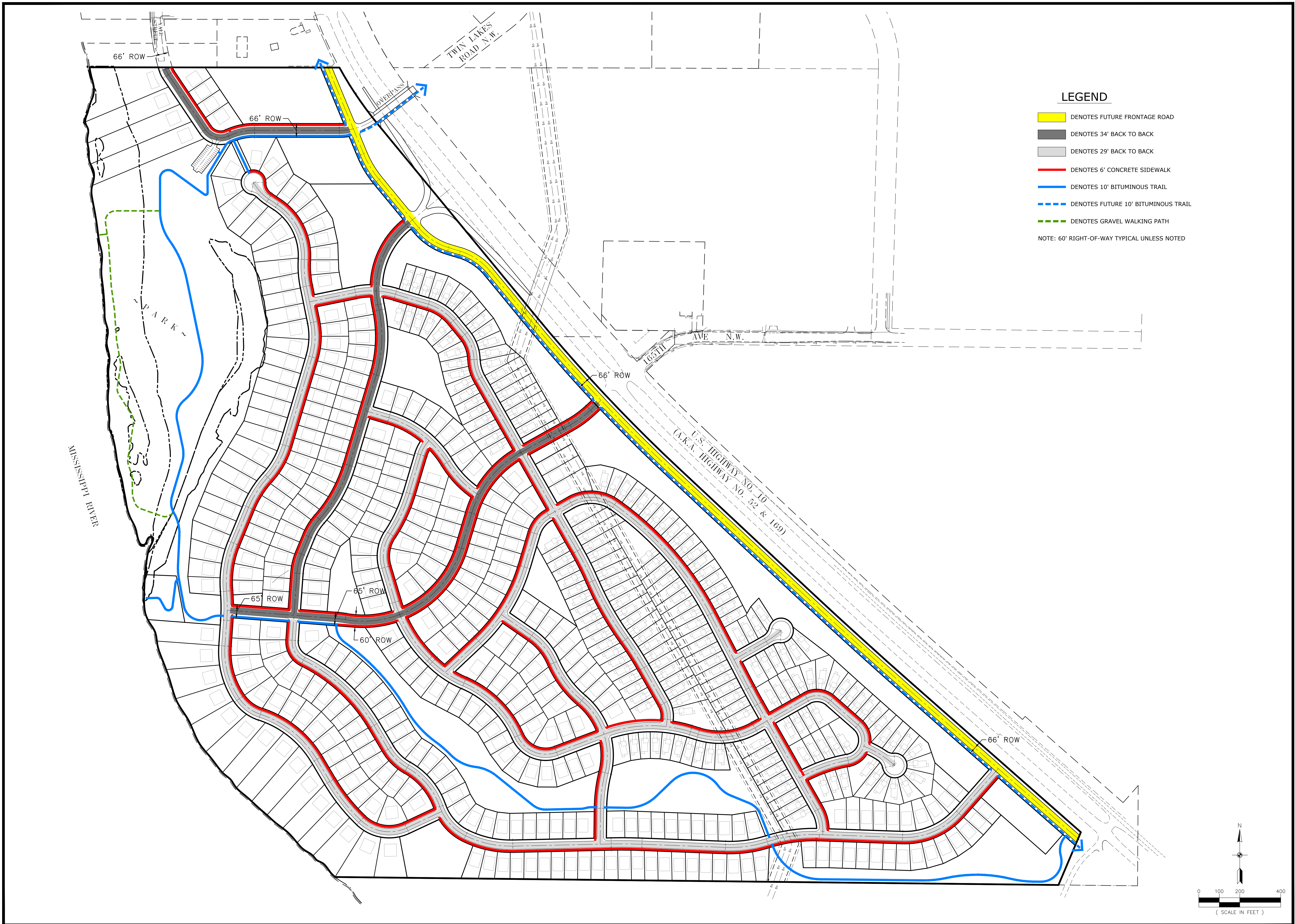
**CAPSTONE HOMES, INC.**  
14015 Sunfish Lake Boulevard, Suite 400  
Ramsey, Minnesota 55303

REVISIONS

1.	11/12/2024	Revise layout.
2.	11/22/2024	Revise layout.
3.		
4.		
5.		
6.		

DRAWN BY: C4  
ISSUE DATE: 10/25/2024  
PROJECT NO: 9556

1 of 1



**LEGEND**

- DENOTES FUTURE FRONTAGE ROAD
- DENOTES 34' BACK TO BACK
- DENOTES 29' BACK TO BACK
- DENOTES 6' CONCRETE SIDEWALK
- DENOTES 10' BITUMINOUS TRAIL
- DENOTES FUTURE 10' BITUMINOUS TRAIL
- DENOTES GRAVEL WALKING PATH

NOTE: 60' RIGHT-OF-WAY TYPICAL UNLESS NOTED

**CARLSON  
McCain**

ENGINEERING  
SURVEYING  
ENVIRONMENTAL

3890 PHEASANT RIDGE DRIVE NE, SUITE 100, BLAINE, MN 55449  
TEL: 763.489.7900 \ FAX: 763.489.7959 \ CARLSONMCCAIN.COM

**STREET WIDTH &  
SIDEWALK/TRAIL EXHIBIT**

**SPECHT FAMILY FARM**  
Elk River, Minnesota

**CAPSTONE HOMES, INC.**  
14015 Sunfish Lake Boulevard, Suite 400  
Ramsey, Minnesota 55303

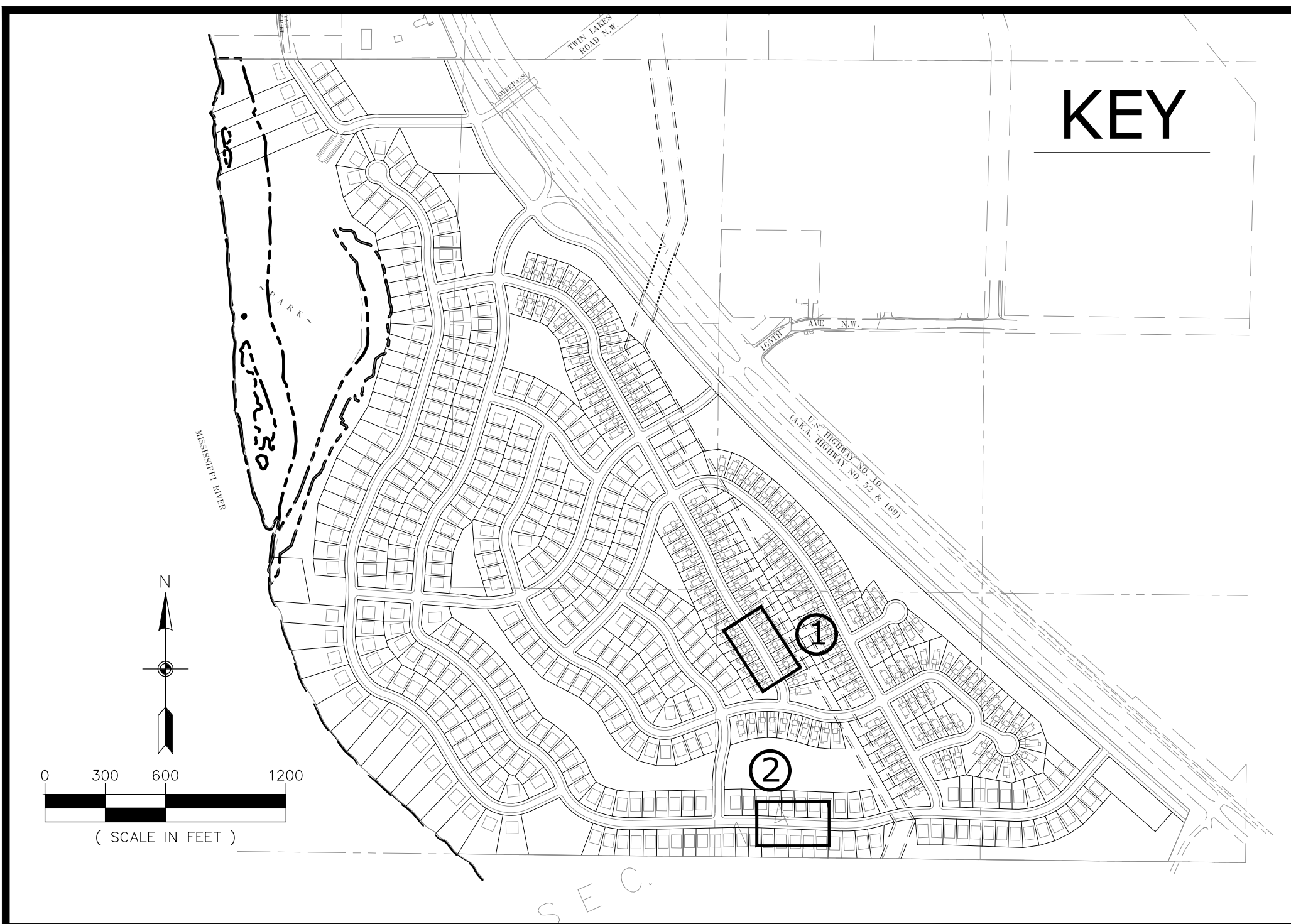
**REVISIONS**

1. 12/04/24 - Park Trails Added.
2. 12/09/24 - Sidewalk Revisions.
3. 12/12/24 - Sidewalk Added, Layout
- 4.
- 5.
- 6.

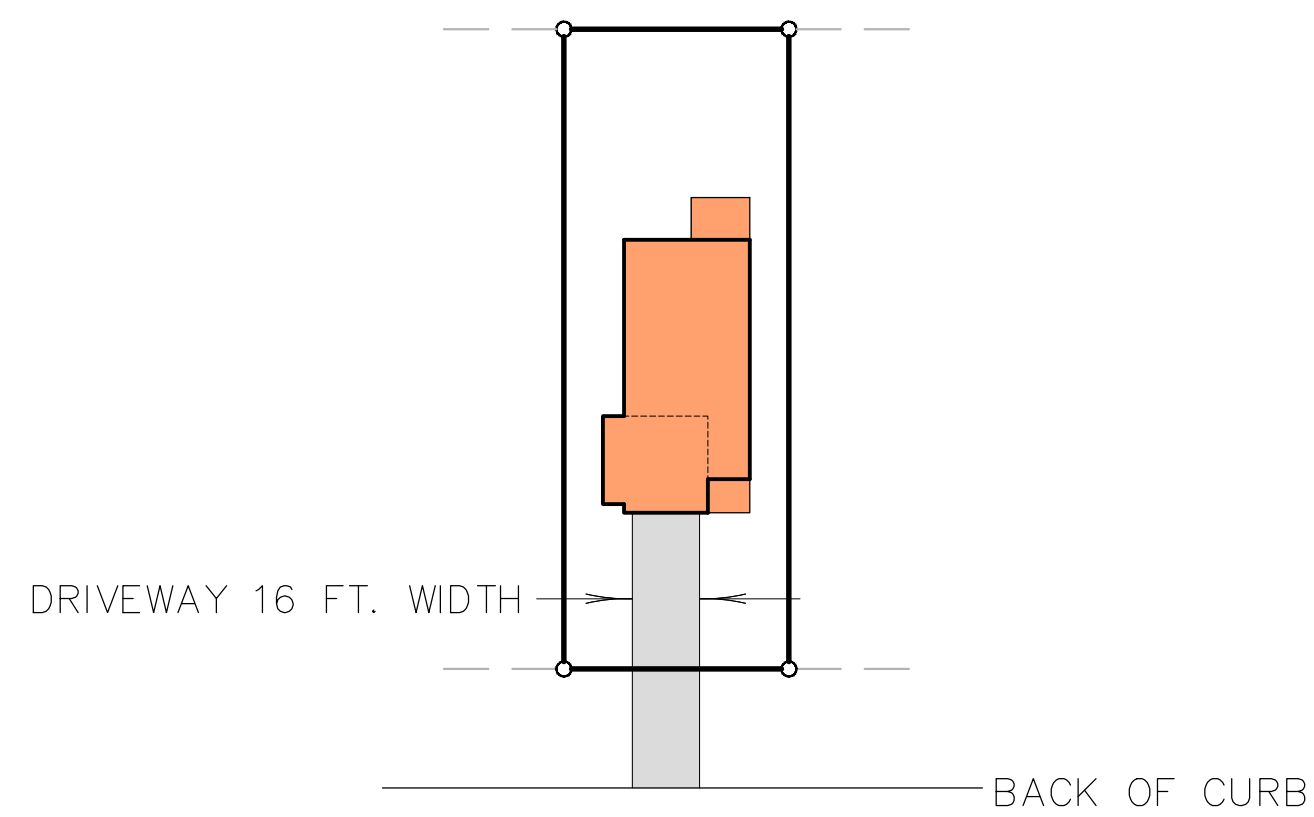
**DRAWN BY:** ADB / RJR  
**ISSUE DATE:** 12/02/2024  
**PROJECT NO.:** 9556

1 of 1

1

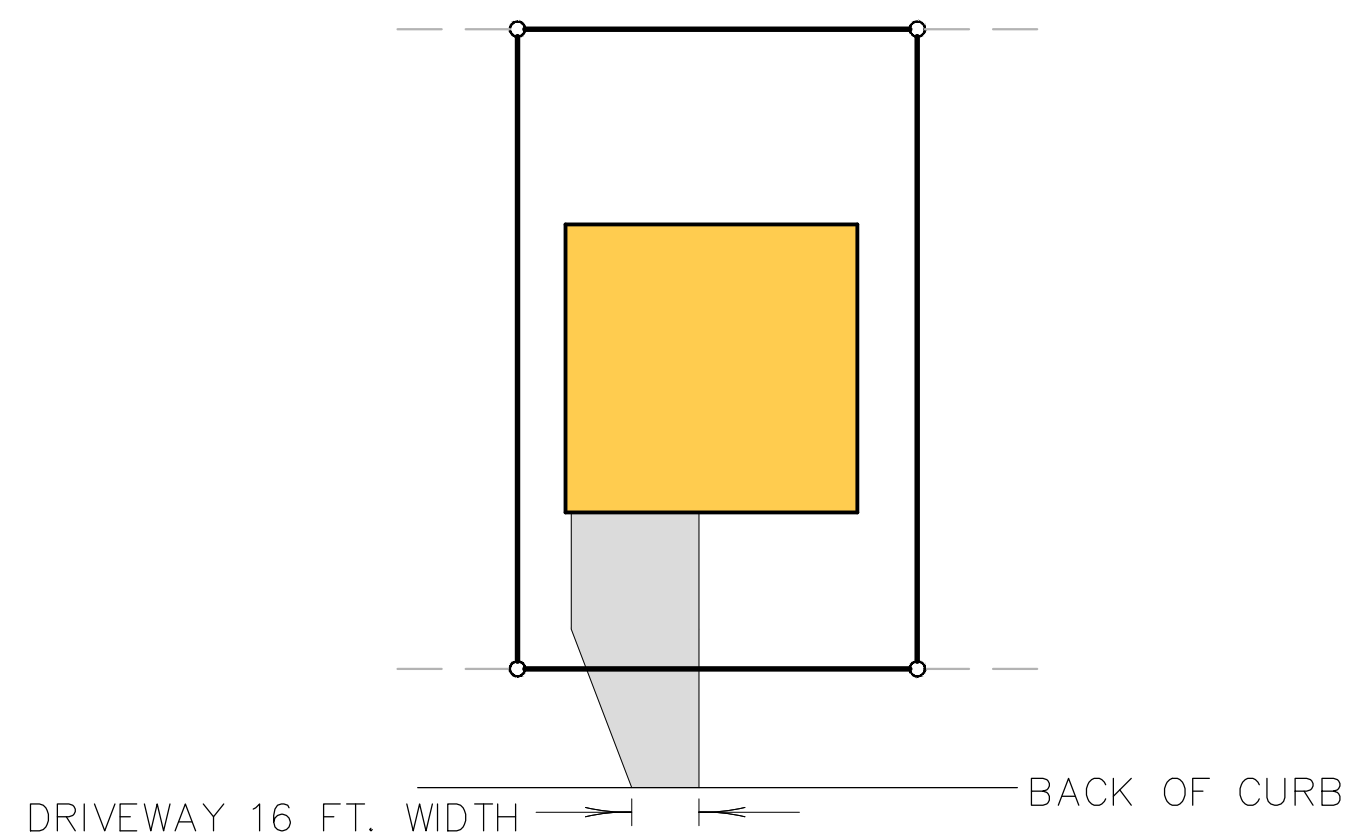


① 50' LIBERTY HOME LOT  
(NOT TO SCALE)

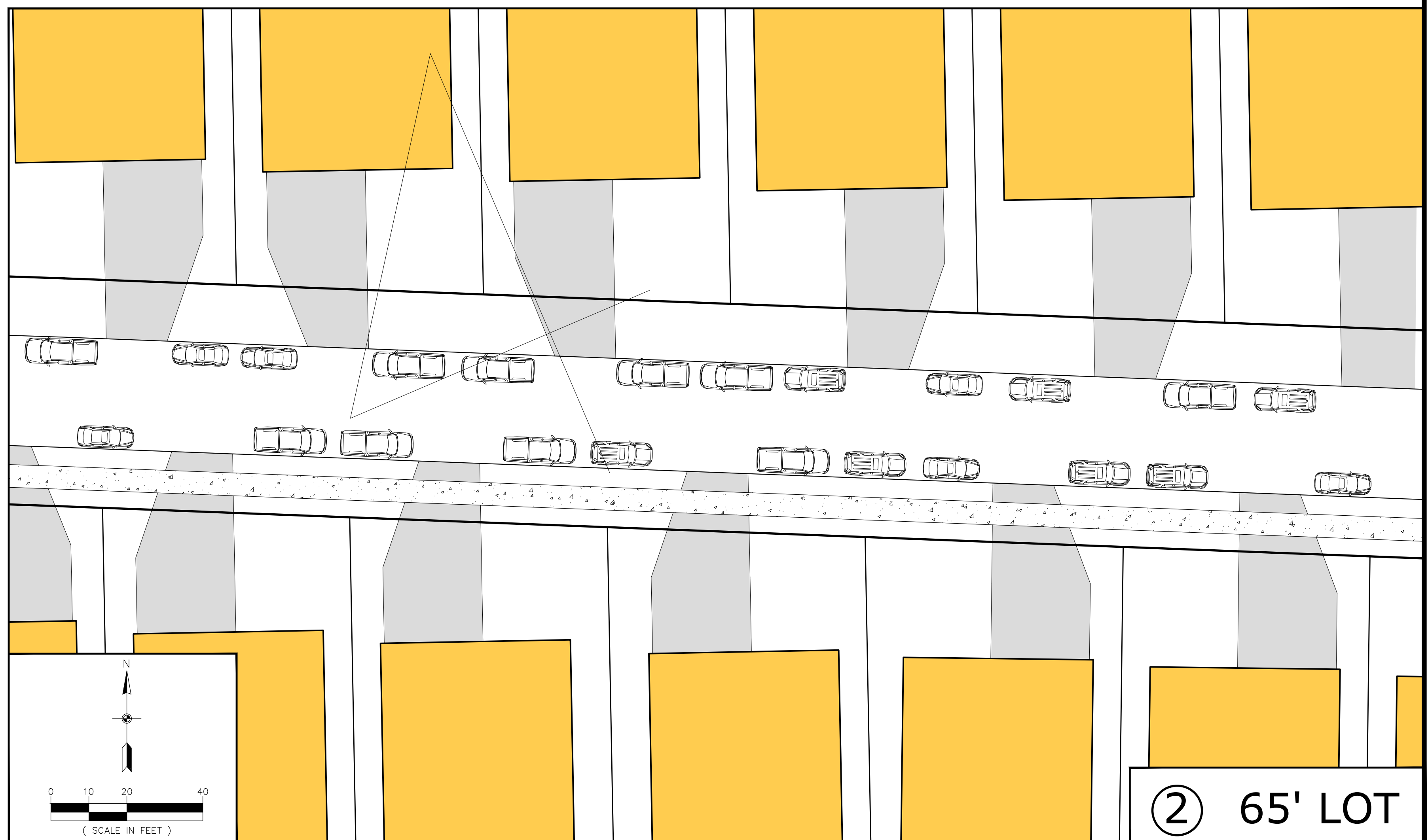


NOTE: MAXIMUM DRIVEWAY WIDTH FOR 50' LOTS IS 16' AT THE BACK OF CURB

② 65' LOT  
(NOT TO SCALE)



NOTE: MAXIMUM DRIVEWAY WIDTH FOR 65' LOTS (AND LARGER) IS 30' AT THE RIGHT OF WAY



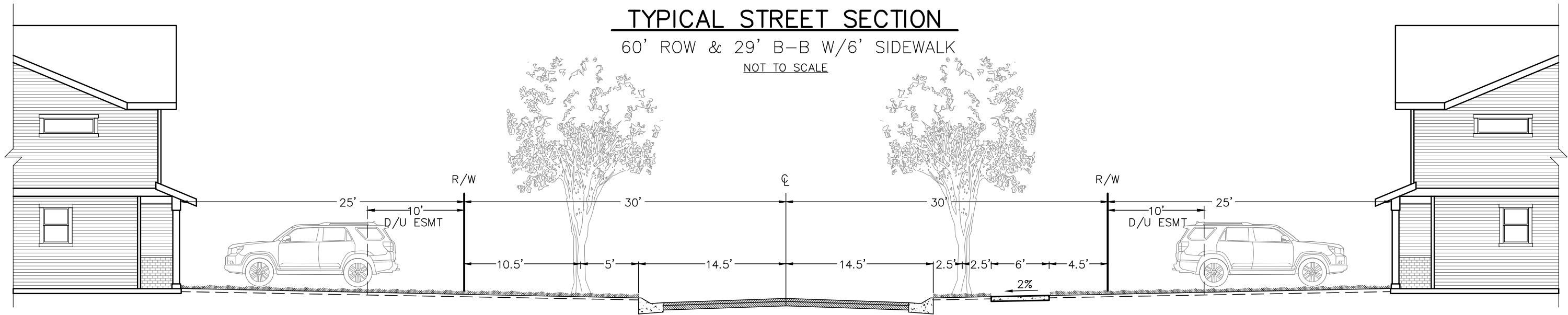
**REVISIONS**

1. 12/13/24 - Add Max Dwy Width Notes
- 2.
- 3.
- 4.
- 5.
- 6.

DRAWN BY: ADB  
ISSUE DATE: 12/5/2024  
PROJECT NO: 9556

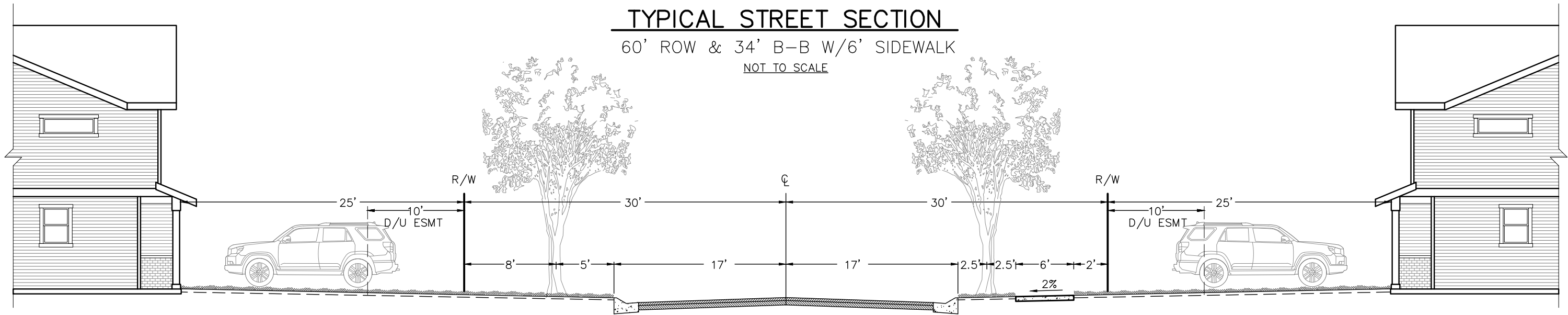
### TYPICAL STREET SECTION

60' ROW & 29' B-B W/6' SIDEWALK  
NOT TO SCALE



### TYPICAL STREET SECTION

60' ROW & 34' B-B W/6' SIDEWALK  
NOT TO SCALE



12/02/24



3300 PHEASANT RIDGE DR NE  
SUITE 100  
BLAINE, MN 55449  
TEL 763.488.7800  
FAX 763.488.7860  
CARLSONMCCAIN.COM

Revisions:  
1.

**CAPSTONE HOMES, INC.**  
14015 Sunfish Lake Blvd. NW, Suite 400  
Ramsey, MN 55303

**SPECHT FAMILY FARM**  
Elk River, Minnesota

**TYPICAL STREET SECTIONS**

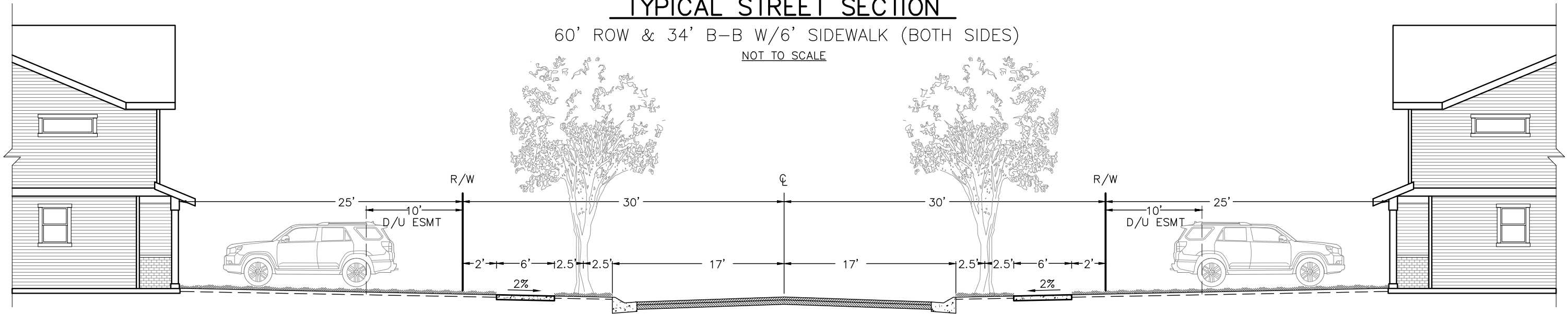
1 of 2

#9556

## TYPICAL STREET SECTION

60' ROW & 34' B-B W/6' SIDEWALK (BOTH SIDES)

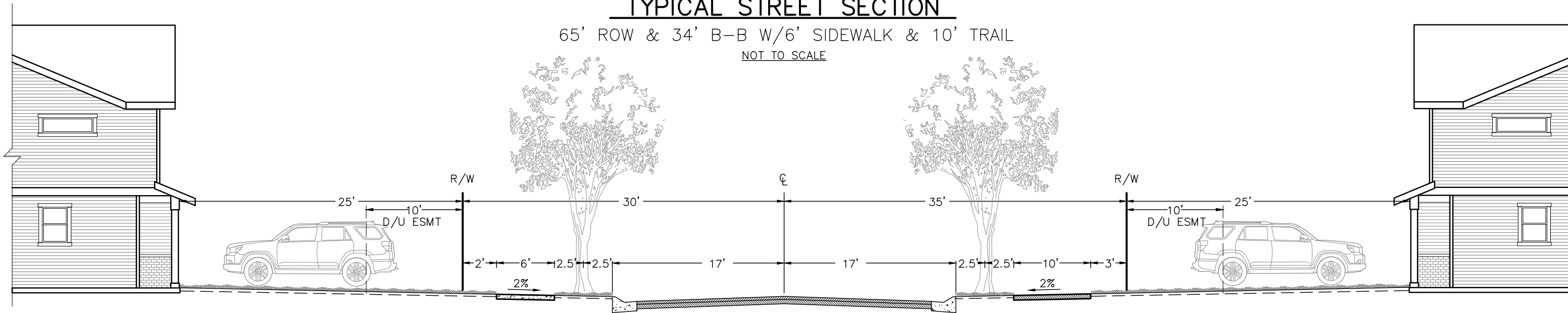
NOT TO SCALE

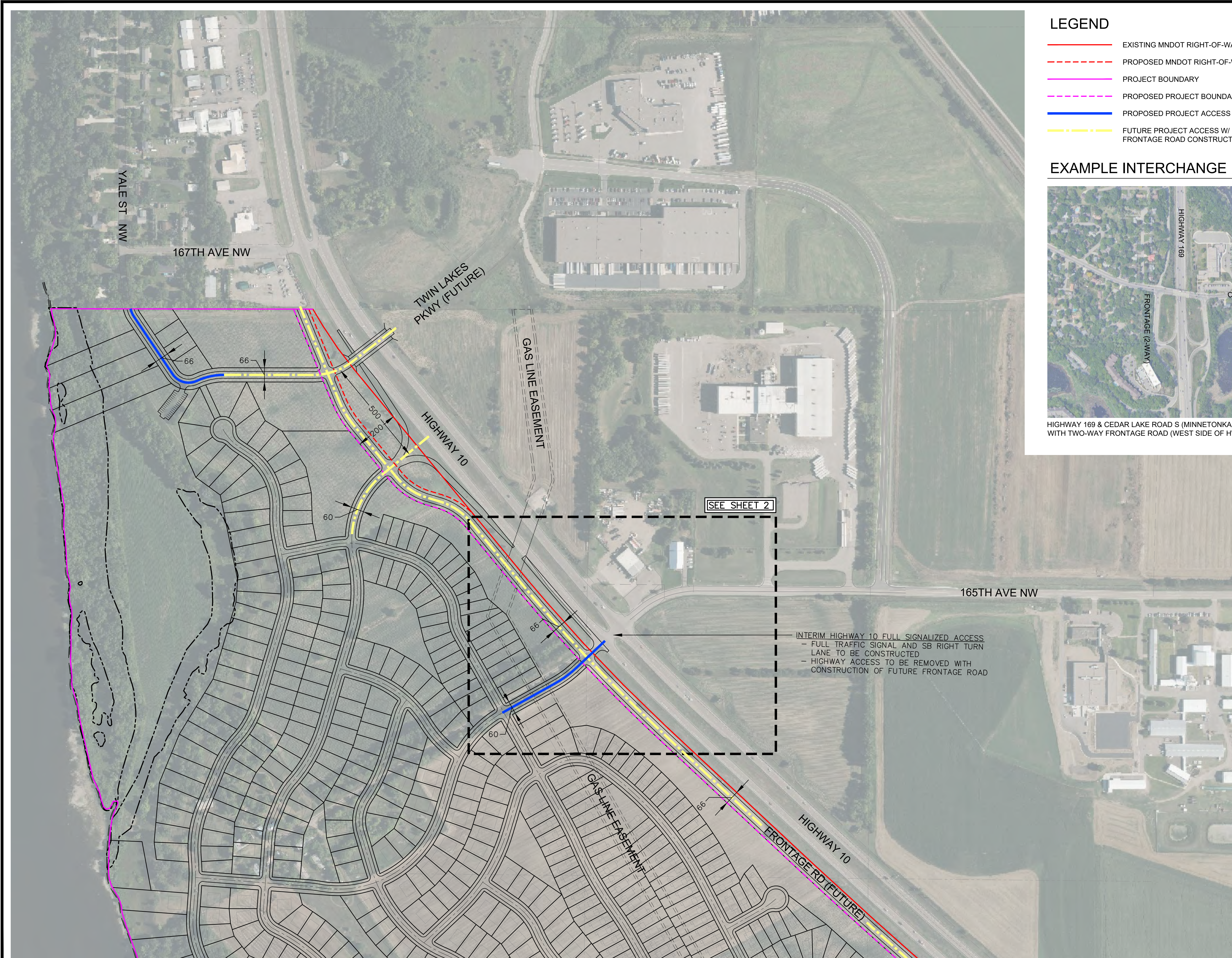


## TYPICAL STREET SECTION

65' ROW & 34' B-B W/6' SIDEWALK & 10' TRAIL

NOT TO SCALE





**LEGEND**

- EXISTING MNDOT RIGHT-OF-WAY (PROJECT BOUNDARY)
- - - PROPOSED MNDOT RIGHT-OF-WAY (FUTURE INTERCHANGE)
- PROJECT BOUNDARY
- - - PROPOSED PROJECT BOUNDARY (FRONTAGE RIGHT-OF-WAY)
- PROPOSED PROJECT ACCESS
- - - FUTURE PROJECT ACCESS W/ HWY 10 INTERCHANGE & FRONTAGE ROAD CONSTRUCTION (BY OTHERS)

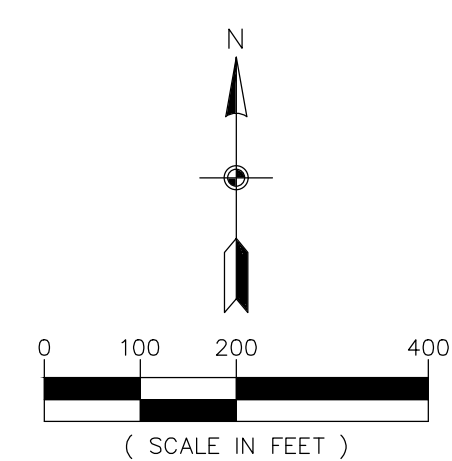
**EXAMPLE INTERCHANGE**



HIGHWAY 169 & CEDAR LAKE ROAD S (MINNETONKA)  
WITH TWO-WAY FRONTAGE ROAD (WEST SIDE OF HWY 169 ONLY)

SEE SHEET 2

INTERIM HIGHWAY 10 FULL SIGNALIZED ACCESS  
 - FULL TRAFFIC SIGNAL AND SB RIGHT TURN LANE TO BE CONSTRUCTED  
 - HIGHWAY ACCESS TO BE REMOVED WITH CONSTRUCTION OF FUTURE FRONTAGE ROAD



**CARLSON McCAIN**  
 ENGINEERING SURVEYING ENVIRONMENTAL  
 3890 PHEASANT RIDGE DRIVE NE, SUITE 100, BLAINE, MN 55449  
 TEL: 763.489.7900 \ FAX: 763.489.7959 \ CARLSONMCCAIN.COM

**HWY 10 ACCESS EXHIBIT**

**SPECHT FAMILY FARM**  
 Elk River, Minnesota

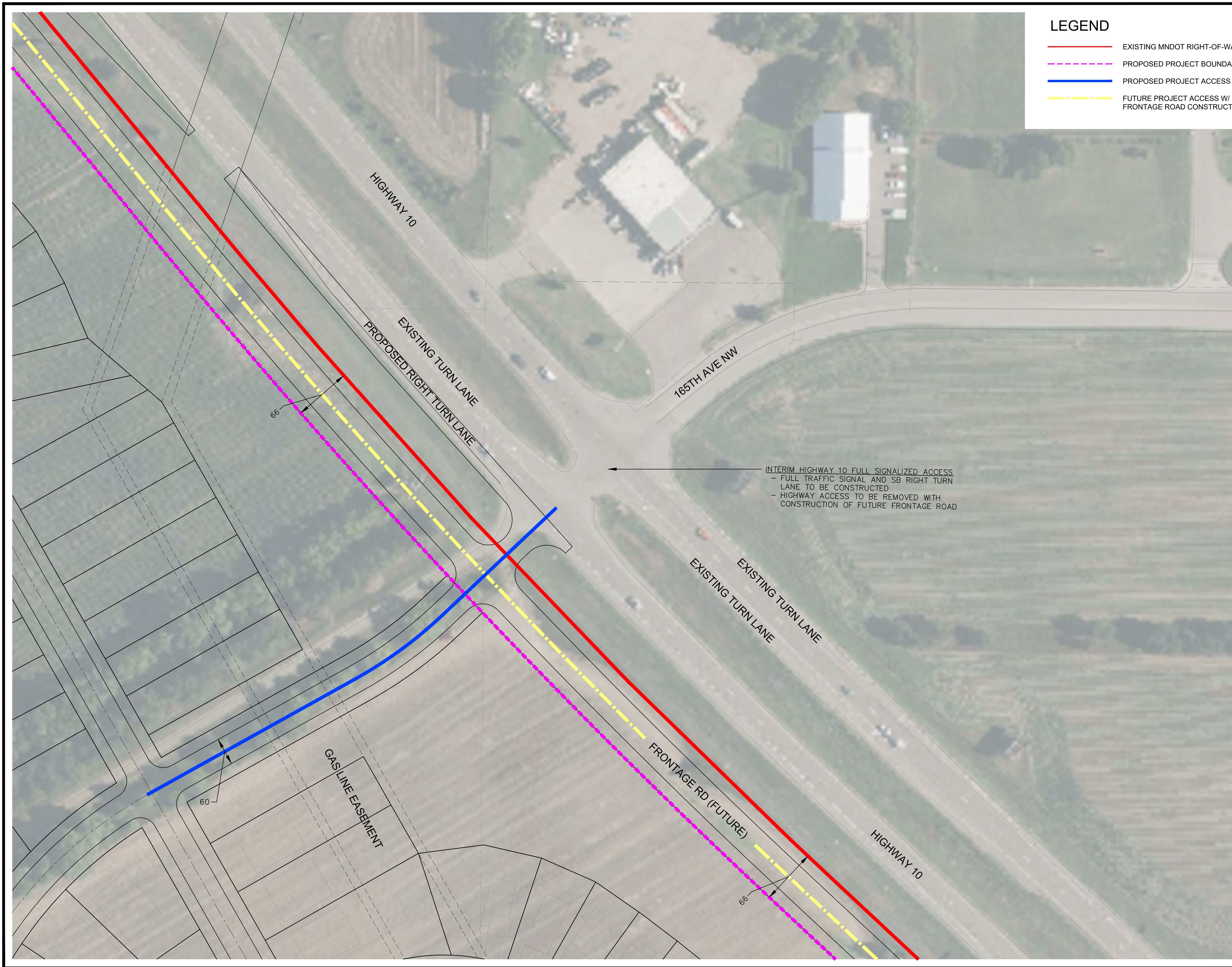
**CAPSTONE HOMES, INC.**  
 14015 Sunfish Lake Boulevard, Suite 400  
 Ramsey, Minnesota 55303

**REVISIONS**

1.	12/13/24 - Revise Layout.
2.	
3.	
4.	
5.	
6.	

DRAWN BY: RJR  
 ISSUE DATE: 09/11/24  
 PROJECT NO: 9556

1 of 2



**LEGEND**

- EXISTING MNDOT RIGHT-OF-WAY (PROJECT BOUNDARY)
- - - PROPOSED PROJECT BOUNDARY (FRONTAGE RIGHT-OF-WAY)
- PROPOSED PROJECT ACCESS (INTERIM)
- . - . - FUTURE PROJECT ACCESS W/ HWY 10 INTERCHANGE & FRONTAGE ROAD CONSTRUCTION (BY OTHERS)

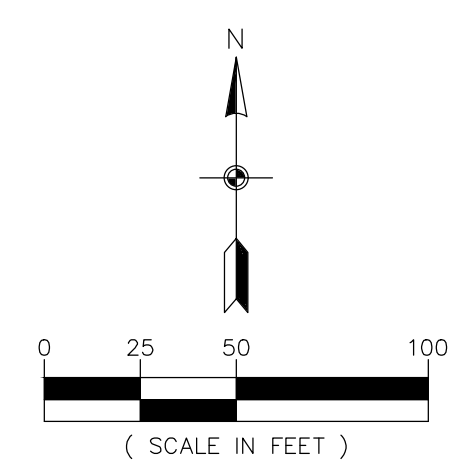
INTERIM HIGHWAY 10 FULL SIGNALIZED ACCESS  
 - FULL TRAFFIC SIGNAL AND SB RIGHT TURN LANE TO BE CONSTRUCTED  
 - HIGHWAY ACCESS TO BE REMOVED WITH CONSTRUCTION OF FUTURE FRONTAGE ROAD

**CAPSTONE HOMES, INC.**  
 14015 Sunfish Lake Boulevard, Suite 400  
 Ramsey, Minnesota 55303

**REVISIONS**

1.	12/13/24 - Revise Layout.
2.	
3.	
4.	
5.	
6.	

DRAWN BY: RJR  
 ISSUE DATE: 09/11/24  
 PROJECT NO: 9556



# CAPSTONE

— H O M E S —

Capstone Homes Inc.  
14015 Sunfish Lake Blvd  
Ramey, MN 55303

RE: Capstone PUD Ordinance Amendment development and building standards.

City of Elk River  
13065 Orono Parkway NW  
Elk River, MN 55330

City of Elk River,

We are pleased to submit our application for consideration of a Master PUD Ordinance Amendment, Rezoning application, and Comprehensive Plan amendment. Outlined below are the proposed design standards and park dedication proposal:

1. Street Design:
  - a. Entrance - Minor Streets have the city standard 60' Right of Way (ROW), with 34' of street section measured at the back of each curb. This street section includes a standard city sidewalk on one or both sides of the street and no parking restrictions.
  - b. Entrance with Trail – Minor Streets with Trail on one side and sidewalk on the other will be 65' ROW, with 34' of street section measure at the back of each curb.
  - c. Neighborhood – Minor side streets throughout the neighborhood have 60' ROW, with 29' street section measured at the back of each curb. This street section includes a sidewalk on one side of the street and no restrictions on parking.
  - d. For street section function and review Capstone welcomes a visit by any staff, commission, board, or council members to our Harvest Run community in Otsego. The model location is 6076 Quin Ave NE, Otsego, MN 55330. Harvest Run streets were built to the City of Otsego code requirements which is 29' back-to-back with sidewalk on one side and no parking restrictions.
2. Lot Width and Setback for the neighborhood will be as follows:
  - a. 50' Liberty Single Family Lot Width:
    - 35' home width
    - 7.5' side setbacks
    - 25' front/corner setback
    - Driveways for Liberty Lots shall be no wider than 16' at the back of curb.
  - b. 65' Single Family Lot Width:
    - 50' home width
    - 7.5' side setbacks
    - 25' front/corner setback
    - Driveways will have a maximum width of 30' measured at the ROW line.

# CAPSTONE

— H O M E S —

- c. 75' Single Family Lot Width:
    - 60' home width
    - 7.5' side setback
    - 30' front/corner setback
    - 30' rear primary structure setback from Bluff where it applies (Wild and Scenic Standards apply)
    - Driveways will have a maximum width of 30' measured at the ROW line.
  - d. 100' Single Family Riparian Lot Width:
    - 80' home width
    - 10' side setback
    - 30' front/corner setback
    - 30' rear primary structure setback from Bluff where it applies (Wild and Scenic Standards apply)
    - 125' rear primary structure setback from 10-year OHW (Wild and Scenic Standards apply)
    - Driveways will have a maximum width of 30' measured at the ROW line.
3. Park Dedication:
- a. 42.58 Acres dedicated of premium river front open space and greenway corridors throughout the neighborhood. This acreage is outlined in the Master PUD Park Exhibit.
  - b. The developer is requesting that the City design and install all park and trail improvements in dedicated acreage areas within a reasonable time proceeding complete dedication of lower park and greenway corridor land.
  - c. Dedication of land will coincide with multiple plats through future phases of development.
  - d. Note: 15.87 of additional Open Space (Highway Berm and Community Entrances) will be maintained by the Homeowners Association.
4. Offsite Utility and Roadway improvements:
- a. Developer will provide utility connections from offsite locations to the boundary of the project and throughout the full neighborhood. A significant cost is involved with these offsite improvements.
  - b. Oversizing of services required by City will be credited to the Developer in the corresponding phase of development.
  - c. Improvements are required at the location of Highway 10 and 165<sup>th</sup> Street. The Developer has planned to contribute up to \$200,000 towards the intersection improvements. The balance of the improvement cost funding will be determined by the City. This intersection is temporary and will be removed upon completion of the MNDOT funded Twin Lakes Overpass.
5. Wild and Scenic River District PUD:
- a. All Riparian (Lots adjacent to the river) Lots are 100' wide measured at the OHW (Ordinary High Water) level.

# CAPSTONE

— H O M E S —

- b. All Non-Riparian Lots on the Bluff that do not encounter the OHW are 75' wide measured at the ROW.
- c. Primary Structure Setback from the OHW will be 125'.
- d. The Shore Impact Zone is 50' from the OHW. 70% of the Shore Impact Zone will remain as open space.
- e. There are no restrictions on Seasonal Docks for all riparian lots.
- f. 65' Lots are permitted in the Wild and Scenic District in areas that are not Riparian or along the bluff.
- g. Open Space must meet or exceed the 50% standard as defined in Table 2 of the Mississippi Wild and Scenic River Open Space Exhibit.
- h. Lots on the bluff will have access through the bluff.
- i. Impervious Surface coverage of riparian lots will not exceed 25% of the lot area.
- j. Non-riparian lots will follow the post development 5-year historic undeveloped peak discharge rate as outlined in the City of Elk River Ordinance.

Capstone Homes has enjoyed working with City Staff, the Park Commission, Planning Commission and City Council during our initial phases of planning. We look forward to many years of continued work together to bring an exemplary neighborhood through The Specht Family Farm property for all the residents of Elk River.

Sincerely,



Matt Barker  
Director of Land Development



SITE DATA		
<span style="display:inline-block; width:20px; height:10px; background-color:#C85130;"></span>	Liberty Homes (50' Lot)	181
<span style="display:inline-block; width:20px; height:10px; background-color:#E6C07C;"></span>	Classic Homes (65' Lot)	284
<span style="display:inline-block; width:20px; height:10px; background-color:#D4E08C;"></span>	Bluff Homes (75' Lot)	49
<span style="display:inline-block; width:20px; height:10px; background-color:#E6E68C;"></span>	River Homes (100' Lot)	19
	<b>Total Residential Lots</b>	<b>533</b>
<span style="display:inline-block; width:20px; height:10px; background-color:#55A868;"></span>	Open Space	58.45 ac.
<span style="display:inline-block; width:20px; height:10px; background-color:#C85130;"></span>	Future Commercial	5.17 ac.
	Future Frontage Road ROW	10.53 ac.
	<b>Net Residential Area</b>	<b>173.50 ac.</b>
	<b>Total Site Area</b>	<b>247.65 ac.</b>

