



Planning Commission

Tuesday, May 27, 2025

6:30 PM

Elk River City Hall

Regular Meeting Agenda

- Regular meeting in Council Chambers

1. CALL MEETING TO ORDER

2. CONSIDER AGENDA

3. CONSIDER MINUTES

3.1 Draft Minutes - April 22, 2025

4. PUBLIC HEARINGS

An opportunity for the public to express their opinions and raise questions pertaining to the agenda item. All comments become part of the official public record. For this reason, all comments must be made at the podium so they can be heard and recorded. Comments may also be provided in writing. There will not be deliberations, discussions, or answers to questions until the hearing is closed. It is important to be courteous and allow each presenter to comment before adding additional testimony.

4.1 Interim Use Permit: Residential Occupation, MJ and Carl Johnson, 19289 Upland Street NW

4.2 Conditional Use Permit: Commercial Recreational Facility, Pickle Paradise, 550 Freeport Ave. NW

4.3 Preliminary Plat and Ordinance Amendment (PUD): The Villas at Fillmore Third Addition, PID 75-0093 I-0030, Riverside Development

4.4 Ordinance Amendment: PUD Amendment for Tall Pines P.U.D. Second Addition

4.5 Modification of Legal Nonconforming Advertising Signs (Billboards)

5. GENERAL BUSINESS

Items in which the information is presented by city staff or consultants, then deliberation and action occur. General Business items are not opportunities to receive or provide public input. However, the presiding officer may, at its sole discretion, solicit public feedback.

5.1 Downtown Master Plan

6. COUNCIL LIAISON UPDATES

7. MOTION TO ADJOURN REGULAR MEETING



**Meeting of the Planning Commission
Held at the Elk River City Hall
Tuesday, April 22, 2025**

Members Present:

Chair Perry Beise, Commissioner Eric Johnson, Commissioner Robert Rydberg, Commissioner Dennis Booth, Commissioner James Zahler, Commissioner Dornan Bland

Members Absent:

Commissioner Anthony Kaba

Staff Present:

Community Development Director Zack Carlton, Senior Planner Chris Leeseberg, and Sr. Administrative Assistant/Recording Secretary Jennifer Green

1. CALL MEETING TO ORDER

Pursuant to due call and notice thereof, the meeting was called to order at 6:50 PM.

2. CONSIDER AGENDA

Moved by Commissioner Booth and seconded by Commissioner Rydberg to approve the agenda. Motion carried 6-0.

3. CONSIDER MINUTES

Moved by Commissioner Johnson and seconded by Commissioner Booth to approve the following consent items as outlined in their respective staff reports. Motion carried 6-0.

3.1 Draft Minutes - March 25, 2025

4. PUBLIC HEARINGS

4.1 Interim Use Permit: Residential Occupation, Benjamin Gomez, 14270 201st Ave. NW

Mr. Leeseberg presented the staff report.

Chair Beise opened the public hearing. There being no one present to speak, Chair Beise closed the public hearing.

Moved by Commissioner Johnson and seconded by Commissioner Booth to recommend approval of the Interim Use Permit with the following conditions to satisfy the standards set forth in Section 30-654:

- 1. All openings to the accessory structure shall be closed when the business is in operation.**
- 2. There shall be no outdoor storage of any business products or equipment.**
- 3. The building must meet all commercial building requirements.**
- 4. There shall be no customers/retail sales on site.**

Motion carried 6-0.

4.2 Conditional Use Permit: AEGIR Brewing Company LLC, 19050 Industrial Blvd. NW

Mr. Leeseberg presented the staff report.

Chair Beise asked for clarification about what is considered parking areas. Mr. Leeseberg outlined the business's parking areas with an aerial photograph.

Chair Beise opened the public hearing.

Tim Jones, 13284 Island View Drive NW, Elk River, stated he was one of the owners of AEGIR Brewing Company, and explained when the parking was approved by the City Council in 2024, he thought parking was allowed for day-to-day operations, but the parking was only allowed for special event permits. His request is to reset the parking conditions back to the original CUP. He stated he is open to restricting the number of days they can hold events, and currently only have 4-5 events per year, such as car shows and kids events. He stated they are also open to closing the back half of the taproom if necessary to account for more parking.

Chair Beise asked about proof of parking and if the brewery had plans to add additional parking spaces.

Mr. Jones indicated that if the industry starts doing well again, they would add additional parking spaces. He stated they have a quote for seal coating the parking lot and hopes to be able to afford to complete that this year.

Chair Beise asked if additional parking could be placed closer to the road.

Mr. Leeseberg stated they could expand parking while maintaining a 10' setback from the roadway.

Chair Beise asked if they could limit the number of events to 4 per year.

Mr. Jones stated that would be fine.

Commissioner Johnson asked if all parking spots will be utilized for events.

Mr. Jones stated yes, especially for car shows, where they would like to drink near the cars on display.

Commissioner Bland asked if their events take place during the business hours of the surrounding businesses.

Mr. Jones stated his events take place on weekends, with an occasional Friday night. He doesn't believe any area businesses operate during those times. He stated he hasn't heard of any parking complaints from area businesses.

Commissioner Bland stated his only concern was the parking affecting the area businesses. He stated he has enjoyed seeing the business full of patrons as it is near his house.

Commissioner Booth stated he was in support of amending the CUP.

There being no one else to speak, Chair Beise closed the public hearing.

Commissioner Johnson stated he was comfortable with 12 events per year since that's the city's standard, but limit events to Fridays, Saturdays, and Sundays.

Commissioner Bland would like to see hours restricted on Fridays, suggesting after 5:00 p.m. on Fridays.

Commissioner Rydberg noted AEGIR's business was located at the south end of the industrial park and the only business that may be impacted by traffic would be the one located across the street.

Commissioner Johnson suggested amending Condition 7 to read: "Parking areas shall be used for events 12 times per year based on receiving a special event permit and limited to Fridays after 5:00 p.m., Saturdays, and Sundays."

The commissioners discussed the limitations with including those days of the week in the condition.

Commissioner Rydberg suggested having the special event permit define the days/times of the events.

Mr. Leeseberg stated the Administration department manages the special event permitting process, forwards the permit to various city departments for review, and suggested having the Administration staff define the specifics of the event's days and times rather than a condition of the CUP.

Commissioner Bland asked if this CUP would restrict AEGIR's activities on holidays.

Mr. Leeseberg stated they could apply for a special event permit for an event to take place on a holiday, allowing them to shut down their parking lot for the event, no matter the day of the week.

Moved by Commissioner Johnson and seconded by Commissioner Booth to recommend approval of the amended Conditional Use Permit with the following conditions to satisfy the standards set forth in Section 30-654:

- 1. The Conditional Use Permit shall not be recorded until the city building official and fire marshal have inspected, and approved, the building for compliance with all state and local building codes and all applicable conditions of approval have been completed.**
- 2. The space labeled "Event Space" shall only be used for corn hole/corn bag toss lanes and any other uses shall require a special event permit.**
- 3. All comments on the Comment Letter dated July 20, 2023, shall be addressed.**
- 4. No tap room operations or expanded brewery operations can occur until:
 - 1. The Conditional Use Permit has been recorded.**
 - 2. The building has received a Certificate of Occupancy.****
- 5. A taproom license shall be obtained from the state and city.**
- 6. No more than 20,000 barrels of its own brands of malt liquor may be brewed annually.**
- 7. Parking areas shall be used for parking only, except for the 12 special events per year.**

-
8. **Any outdoor patio area shall be delineated with a permanent fence and shall not be in required parking areas.**
 9. **No demolition or construction activities can occur within or on the exterior of the building until the Conditional Use Permit is approved and a building permit has been applied for and issued.**
 10. **Conditional Use Permit CU 23-17 shall become void.**

Motion Carried 6-0.

4.3 Ordinance: Permit Off-Premise City-Owned Advertising Signs, City of Elk River

Mr. Carlton presented the staff report.

Commissioner Rydberg asked if the city was restricting this ordinance to publicly-owned government buildings.

Mr. Carlton indicated the ordinance was intended specifically for government-operated signs.

Commissioner Dornan asked if it would be a permanent sign.

Mr. Carlton stated yes, a permanent sign.

Commissioner Zahler asked questions about billboard sizes.

Commissioner Dornan asked if the advertising was specific to the events happening at the facility and not general government usage.

Mr. Carlton stated yes, advertising would be specific for events at the facility, and it was not a sign for general government use or a general community sign.

The commission discussed the advertising and the consensus was to make the language specific to the facility.

Chair Beise opened the public hearing. There being no one to speak, Chair Beise closed the public hearing.

Commissioner Rydberg asked who would monitor the advertising language.

Mr. Carlton stated the communications department with staff from the FT Center will determine advertising language.

Moved by Commissioner Eric Johnson and seconded by Commissioner Robert Rydberg to recommend approval of an ordinance allowing off-premise advertising signs for government facilities with the condition that the sign will only be used for advertising and marketing events at the FT Center. Motion carried 6-0.

5. GENERAL BUSINESS

5.1 Review Downtown Small Area Plan

Mr. Carlton presented the staff report. He introduced **Mike Thompson** and **Andrew Dresener** from Bolton & Menk, who provided a presentation of the highlights and key recommendations of the draft downtown small area plan. The plan will come before the Planning Commission next month, for consideration and action, and then for consideration before the City Council.

After the presentation, the Planning Commission provided questions and comments about the plan.

Commissioner Rydberg asked about changing traffic at Jackson and Main Street to have it be right in, right out only. He stated he felt that adding parallel parking cuts down on the number of current parking spaces.

Mr. Thompson stated closing off Jackson Street was considered and felt having as many accesses open to Main Street was important.

Commissioner Johnson asked how many spots a parking ramp would hold.

Mr. Dresener stated the reduction of on-street parking spots was minimal when changing to parallel from diagonal parking. The ramp would more than make up for the difference in the loss of on-street space (less than 20 spaces) and double the amount of parking on the current lot. He asked the commission to consider the parking be more than just looking at a number, but rather creating a different experience of being downtown, by parking and walking to a store.

Commissioner Rydberg felt it would be tough watching people parallel-park.

Mr. Dresener stated he heard a similar comment in the Open House, but he also felt that watching people back out from diagonal parking onto Main Street was also tough.

Commissioner Johnson asked where the farmers market would be located should a ramp be constructed where the current market takes place.

Mr. Carlton stated they had discussed ideas with Karen Wingard, who manages the Farmers Market, and considered numerous opportunities for a different Farmers Market experience. A few suggestions were having the market linear using the wider sidewalks, locating the vendors at the top of the ramp, on the backside of the Jackson Street or Main Street buildings, or shutting down Main and King Avenue/Jackson.

Commissioner Rydberg noted the potential for employee parking to be displaced.

Mr. Carlton stated the committee spent hours discussing parking and while there is a loss in the number of spaces with some of these changes, there would be street improvements with a wider pedestrian area and benches, all of which contribute a more enjoyable public space experience and provide opportunities for people to spend more time downtown. Other ideas include parking limits, but that can create other needs such as enforcement.

Commissioner Johnson stated he liked the overall concept.

Commissioner Booth asked about the city-owned property at King Avenue and what would happen if someone came in with a hotel proposal.

Mr. Carlton stated if someone were to approach the city with a downtown development plan for the parking lot, they would bring the plan to the city council for consideration and incorporate parts of this plan to support it. He stated there was also some feedback at the open house with comments about the area known as Uptown, and creating a smoother transition between single-family homes to taller buildings, and the importance of keeping the green space in the downtown area.

Commissioner Bland asked if there are cosmetics guiding this plan; what it looks like and how that is addressed. He explained he likes the "vibe" of Elk River's downtown. He went on to describe how he feels about the downtown and it being a personal experience for him and his family, with visits to the book store, coffee shop, and attending Halloween and Christmas events. He didn't want to risk not continuing with any of these plan components if there would be a loss of that vibe, turning into a commercial or a low-dollar attraction, but rather have it remain a local, small town feel where you know the name of the small business owners.

Chair Beise asked how architectural requirements are put in place to create an aesthetically pleasing town.

Mr. Carlton stated the architectural requirements for downtown building storefronts are outlined in the ordinance and guides any new construction. He stated an action step could include building design guidelines, but that could restrict some developers.

The Planning Commission continued to discuss the downtown 'vibe' and ensure the importance of continuing it with any new development opportunities.

The Planning Commission then discussed the challenges of getting people across the highway, with the railroad and MnDOT restricting those opportunities, both financially and feasibility with grades and elevations.

The Planning Commission will discuss this item at next month's meeting.

6. COUNCIL LIAISON UPDATES

Councilmember Wagner provided a brief update to the Planning Commission.

7. MOTION TO ADJOURN REGULAR MEETING

Moved by Commissioner Johnson and seconded by Commissioner Rydberg to adjourn the meeting. Motion carried 6-0.

The meeting adjourned at 8:02 P.M.

Minutes prepared by Jennifer Green.

Perry Beise, Chair

Tina Allard, City Clerk

DRAFT



Request for Action

To
Planning Commission

Item Number
4.1

Meeting Date
May 27, 2025

Prepared By
Chris Leeseberg, Senior Planner

Item Description
Interim Use Permit: Residential Occupation, MJ and Carl Johnson, 19289 Upland Street NW

Reviewed by
Zack Carlton

Action Requested

Recommend, by motion, approval of the Interim Use Permit with the following conditions to satisfy the standards set forth in Section 30-654:

1. No more than two non-resident employees may be present during business hours.
2. Hours of operation shall be limited to 7:00 a.m. to 7:00 p.m., Monday through Saturday.
3. The applicant must maintain current professional licensure and liability insurance.
4. The interim Use Permit will terminate when the home is sold.

Background/Discussion

The property, approximately 5.5 acres in size, is zoned R3 (Townhouse/Multiple Family Residential) where residential occupations are either permitted administratively or require an Interim Use Permit (IUP). This request is subject to a review by the Planning Commission and City Council because the business will involve the use of equipment not typically found in a house. By contrast, a home-based business such as a tax office, which uses standard office equipment, would be permitted without the need for an IUP.

The applicant is requesting an IUP to operate a residential-based aesthetic and wellness business, Northern Rose MedSpa, from the planned home located at 19289 Upland St. NW. The business will involve cosmetic and wellness services such as injectables, facials, and skincare consultations. The proposed use qualifies as a Residential Occupation under Section 30-801 of the Zoning Ordinance and the IUP is required to allow the use within the house.

The applicant anticipates approximately five (5) clients per day and will be seen one at a time. Client parking will occur in the driveway and not on the street. Prior to the property being rezoned to Residential, it was zoned Commercial, where significantly more traffic would have been generated. The ordinance limits the hours of operation to 7:00 a.m. – 7:00 p.m. Monday through Saturday with no operations on Sunday.

The city recognizes that introducing residential occupations in residential districts could be noticeable and strives to include approval conditions aimed at mitigating possible impacts.

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Interim Use - Applicable Regulations

Before the city council may authorize an interim use, it must make the following findings:

- 1. The period of time for which the interim use permit is to be granted will terminate before any adverse impacts are felt upon adjacent properties.*

The ordinance (Section 30-801) states an approved IUP for a residential occupation shall terminate when the property is sold or when a lease has expired. The applicant acknowledges the temporary nature of the IUP. Although termination does not guarantee adverse effects will not affect nearby properties, the recommended conditions strive to prevent this outcome.

- 2. There is adequate assurance that the property will be left in suitable condition after the use is terminated.*

The proposed residential occupation will occur inside the house. Upon termination, the space will revert to a standard residential use with no remediation needed.

- 3. The use is similar to uses allowed in the zoning district in which the property is located.*

Residential occupations are allowed with an IUP in the R3 zoning district and remain secondary to the residential use of the property.

- 4. The date or event that will terminate the use can be identified with certainty.*

The ordinance states an approved IUP for a residential occupation shall terminate when the property is sold or when a lease has expired, so the termination of the use can be identified with certainty.

- 5. Authorizing the use will not impose additional costs on the public if it is necessary for the public to take the property in the future.*

No public infrastructure or improvements are needed. If the public were to take over the property, no additional costs would be imposed on the public, as the proposed residential occupation will occur in the house and the site will be left in a suitable condition.

- 6. The applicant and user agree to all conditions that the city council deems appropriate for authorization of the interim use.*

This will be determined at the City Council meeting.

- 7. The applicant provides assurances deemed adequate by the city council that the use will terminate as provided in the interim use permit. The city council may require the applicant to deposit a cash amount with the city, or provide some other form of security, to ensure compliance with this criteria.*

Again, the ordinance states an approved IUP for a residential occupation shall terminate when the property is sold or when a lease has expired. Based on the scale of the use, a cash deposit does not appear to be necessary to comply with the criteria.

- 8. Authorization of the interim use will not result in adverse effects on public health, safety, or welfare.*

The proposed use will occur entirely within the house and, due to its low-impact nature, poses no known health or safety risks while preserving neighborhood harmony. Although the current exhibit depicting the house's proposed location is subject to change, the final siting and intended use are not anticipated to create any health or safety concerns.

9. *The proposed interim use is consistent with the City of Elk River Comprehensive Plan and conforms to the city's zoning regulations.*

The use is consistent with the Comprehensive Plan which guides the property as Mixed Residential. Single-family uses, with residential occupations, are allowed in the zoning district.

Financial Impact

None

Mission/Policy/Goal

Ethical, efficient, and responsible.

Attachments

1. Location Map
2. Narrative
3. Site Plan - Draft Project Plan
4. Floor Plan



SITE

CORD I

Project Location Map

MJ & Carl Johnson

Interim Use Permit

Case No: IU 25-04



IU 25-04 Narrative

Project Title: Residential MedSpa Clinic Addition

Project Overview:

This project involves incorporating a professional MedSpa clinic into the new home that is to be under construction soon. The clinic will feature one treatment room and potentially a variety of specialized equipment, creating a luxurious, therapeutic environment for aesthetic and wellness services. Designed to harmonize with the home's aesthetic, the MedSpa will offer both comfort and clinical functionality.

Project Goals:

1. **Functional Integration:** Design a MedSpa that operates as a dedicated clinic while blending with the residential architecture.
2. **Ambiance and Comfort:** Establish a sophisticated, serene atmosphere to enhance client experience.
3. **Compliance and Safety:** Construct the clinic to meet medical standards for sanitation, ventilation, and accessibility.
4. **Efficiency and Flexibility:** Design functional spaces to streamline workflow for staff, with a layout flexible for future service additions.

Key Features and Spaces:

- **Reception Area:** A small, yet welcoming reception with comfortable seating and a calm, modern aesthetic. (This feature has not yet been decided on.)
- **One Treatment Room:** Private, soundproofed treatment room equipped with necessary equipment for a range of aesthetic services.
- **Sterilization and Storage:** Dedicated area for equipment sterilization and storage, maintaining a clean and organized workspace.
- **Client Amenities:** High-end finishes, adjustable lighting, soundproofing, and climate control for an enhanced client experience.

Common MedSpa Equipment:

- **Adjustable Treatment Tables:** Ergonomic, height-adjustable tables for client comfort during treatments.
- **Potential for FUTURE Laser Machines:** For hair removal, skin resurfacing, and various skin treatments.

- Microdermabrasion Devices: For exfoliating and rejuvenating skin.
- LED Light Therapy Machines: Used for skin rejuvenation and acne treatment.
- Ultrasound and Radiofrequency Devices: Non-invasive options for skin tightening and contouring.
- Injectable Coolers and Storage: Temperature-controlled storage for injectables.
- Autoclave or Sterilization Equipment: For safe sterilization of reusable instruments.
- Medical-Grade Lighting: Adjustable lighting for precise treatments.
- Possible FUTURE Oxygen Therapy/ProNox Machines.

Timeline:

- Design Phase: Complete architectural and equipment planning within 2-3 months.
- Construction Phase: Coordinate MedSpa build-out with overall house construction, aiming to complete within 6-8 months.

Applicant will send final design and site plans as they are completed. All attached documents are PRELIMINARY drafts. Please let me know if there are any questions or concerns.

City of Elk River – Home Occupation Compliance Statement

Prepared for: Northern Rose MedSpa

Prepared by: MJ Johnson Aesthetic RN, BSN, PHN

Date: April 29, 2025

I. Business Overview

Business Name: Northern Rose MedSpa

Business Address: 19289 Upland St. NW, Elk River, MN 55330

Zoning District: R3? Please confirm with Planning Department.

Business Type: Aesthetic and wellness services (e.g., cosmetic injectables, facials, skincare consultations)

Owner/Operator: MJ Johnson

Contact Information: 320-292-0866 (mj@northernrosemespa.com)

II. Purpose

This document is provided to demonstrate that Northern Rose MedSpa, operating from a residential property, meets all City of Elk River requirements for a home occupation under **Zoning Ordinance Section 30-1033 (Home Occupations)**.

III. Home Occupation Compliance Checklist

Requirement	Description	Compliance
Business conducted entirely within the dwelling	All business activities (consultations and treatments) are conducted in a dedicated room inside the home. No detached structures are used.	✔ Yes
Residential character preserved	No external alterations have been or will be made that alter the residential appearance of the home.	✔ Yes
No exterior evidence of business	There will not be excessive business signage, product display, or obnoxious lighting on the exterior of the home.	✔ Yes
Limited traffic	Clients are seen by appointment only, with sufficient time between clients to avoid traffic or parking issues.	✔ Yes
Parking accommodated on-site	The property has a driveway with adequate parking for client vehicles. No street parking will be used.	✔ Yes

Requirement	Description	Compliance
No more than two non-resident employee	Only the resident operator and the potential for 1-2 licensed &/or unlicensed assistants may be present in the future during business hours.	✔ Yes
No sale of goods on premises	Retail sales (e.g., skincare products) are conducted mainly only. Only products relevant to services rendered will be available for purchase on site.	✔ Yes
No noise, odor, vibration, glare, or electrical interference	All equipment used (e.g., medical-grade facial equipment) operates quietly and does not emit odors or disruptive sounds.	✔ Yes
Licensing and certification	The business owner holds all required professional certifications (e.g., RN or NP license), and a Minnesota Tax ID is maintained.	✔ Yes
Building safety and accessibility	The treatment area meets local building and safety codes. Clients can access the treatment space without navigating hazardous or steep steps.	✔ Yes

IV. Supporting Documents (Available Upon Request)

- Floor plan showing business area (within residence)
- Proof of professional licensure
- Liability insurance certificate
- Property ownership or lease agreement
- Proposed hours of operation
- Client intake and scheduling policies
- Product MSDS sheets (if applicable)

V. Conclusion

Northern Rose MedSpa fully complies with the City of Elk River's ordinances governing home occupations. All operations are designed to maintain the quiet, residential character of the neighborhood, prioritize client safety, and adhere to all local and state regulations.

This statement is submitted in support of any necessary approvals or applications related to a Home Occupation Permit or Conditional/Interim Use Permit.

Prepared by:

MJ Johnson Aesthetic RN, BSN, PHN
 Owner, Northern Rose MedSpa
 320-292-0866 | northernrose.aesthetics@gmail.com

APPX PROPERTY LINE

WALKING PATH

APPX SETBACK LINE

Potential
Future
Outbuilding

DRAFT
House Placement
Subject to Change

Residential
Occupation

UPLAND ST NW

APPX PROPERTY LINE

ELK LAKE RD NW







Request for Action

To
Planning Commission

Item Number
4.2

Meeting Date
May 27, 2025

Prepared By
Chris Leeseberg, Senior Planner

Item Description
Conditional Use Permit: Commercial Recreational Facility, Pickle Paradise, 550 Freeport Ave. NW

Reviewed by
Zack Carlton

Action Requested

Recommend, by motion, approval of the Conditional Use Permit as the standards set forth in Section 30-654 have been met.

Background/Discussion

The 5.69 acre parcel is zoned Mixed Use – Elk River Plaza (MU-ERP) where commercial recreational facilities require a Conditional Use Permit (CUP) to operate. The applicant has requested approval of a CUP to allow construction of an indoor pickleball club within 21,750 sq.ft. of the existing 40,000 sq.ft. structure at 570 Freeport Ave.

Pickle Paradise will feature seven indoor pickleball courts, a small pro shop, a snack bar, lounge, elevated mezzanine viewing area, and a flexible-use event space.

The applicant anticipates approximately 700-900 visitors per week, with hours of operation from 6 a.m. to 10 p.m., seven days a week, with limited closures on major holidays. The facility will typically be staffed by 2-3 employees during regular hours and up to 6 during peak periods and events.

Applicable Regulations

The issuance of a Conditional Use Permit can be ordered only if the use at the proposed location:

1. *Will not endanger, injure or detrimentally affect the use and enjoyment of other property in the immediate vicinity or the public health, safety, morals, comfort, convenience or general welfare of the neighborhood or the city.*

Commercial Recreational Facilities are allowed with a CUP in the subject property zoning district and because the use is entirely contained within a building and there is ample parking, the use will not endanger, injure or detrimentally affect the use and enjoyment of other property in the immediate vicinity or the public health, safety, morals, comfort, convenience or general welfare of the neighborhood or the city.

Staff do not see a need to impose specific conditions to satisfy this standard.

2. *Will be consistent with the comprehensive plan.*

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The property is guided for Highway Business uses. The highway business category primarily consists of a mix of auto-oriented retail and service businesses, restaurants, and community and regional-scale shopping centers. Highway business uses are located along Highway 169 and Highway 10 and have high visibility from these corridors. The use is consistent with the Comprehensive Plan.

Staff do not see a need to impose specific conditions to satisfy this standard.

- 3. Will not impede the normal and orderly development and improvement of surrounding vacant property.*

The area is fully developed and, as there are no vacant lots, the proposed use will not affect the development of the surrounding area.

Staff do not see a need to impose specific conditions to satisfy this standard.

- 4. Will be served adequately by and will not adversely affect essential public facilities and services including streets, police and fire protection, drainage, refuse disposal, water and sewer systems, parks and schools; and will not, in particular, create traffic congestion or interference with traffic on adjacent and neighboring public thoroughfares.*

The use will be served adequately by and will not adversely affect essential public facilities and services, including streets, police and fire protection, drainage, refuse disposal, water and sewer systems, parks and schools; and will not, in particular, create traffic congestion or interference with traffic on adjacent and neighboring public thoroughfares.

The subject parcel and the adjacent parcel previously operated as a movie theater complex, which generated significantly more traffic and patron activity than the proposed use is expected to. With seven courts and four players per court, approximately 28 vehicles will be on-site during play, with an additional 28 vehicles likely waiting for the next time slot. Therefore, an estimated 56 vehicles per hour may be present on the site.

Staff do not see a need to impose specific conditions to satisfy this standard.

- 5. Will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons or property because of excessive traffic, noise, smoke, fumes, glare, odors, dust or vibrations.*

The activities and equipment associated with a commercial recreational facility should not detrimentally affect nearby properties. The use will not involve uses, activities, processes, materials, equipment and conditions of operation that will be detrimental to any persons or property because of excessive traffic, noise, smoke, fumes, glare, odors, dust or vibrations.

Staff do not see a need to impose specific conditions to satisfy this standard.

- 6. Will not result in the destruction, loss or damage of a natural, scenic or historic feature of major importance.*

There are no natural, scenic or historic features of major importance on the subject parcel.

Staff do not see a need to impose specific conditions to satisfy this standard.

- 7. Will fully comply with all other requirements of this Code, including any applicable requirements and Standards for the issuance of a license or permit to establish and operate the proposed use in the city.*

The use will fully comply with all other requirements of this Code, including any applicable requirements and

standards for the issuance of a license or permit to establish and operate the proposed use in the city.

All applicable commercial building, plumbing, mechanical, and electrical permits shall be obtained from the city and state.

Staff do not see a need to impose specific conditions to satisfy this standard.

If denial of such a permit should occur, it shall accompany recommendations or determinations by findings or a report stating how the proposed use does not comply with the standards set forth in Section 30-654.

In the review of the standards for CUP as outlined, it appears that the request is consistent with all of these standards.

Financial Impact

None

Mission/Policy/Goal

Meet changing needs - agile.

Attachments

1. Location Map
2. Applicant's Narrative
3. Site Survey
4. Floor Plan



Project Location Map

Pickle Paradise - Julie Russo

Conditional Use Permit

Case No: CU 25-04

CU 25-04 Narrative

This application is for a Conditional Use Permit (CUP) for “Pickle Paradise,” an indoor pickleball club in Elk River, MN. We are planning to lease 21,750 square feet of space at 570 Freeport Ave NW, Elk River, MN 55330, the former Marcus Cinema building, from Price Homes (owner: Adam Price). The property is zoned “mixed-use” and is well-suited for adaptive reuse as an indoor commercial recreational facility.

Pickle Paradise will feature seven indoor pickleball courts, a small pro shop, a snack bar, lounge, elevated mezzanine viewing area (public or private event space), and a flexible-use event space. Our goal is to provide a welcoming, active space for all ages and skill levels, while contributing positively to the character and economic vitality of the surrounding community.

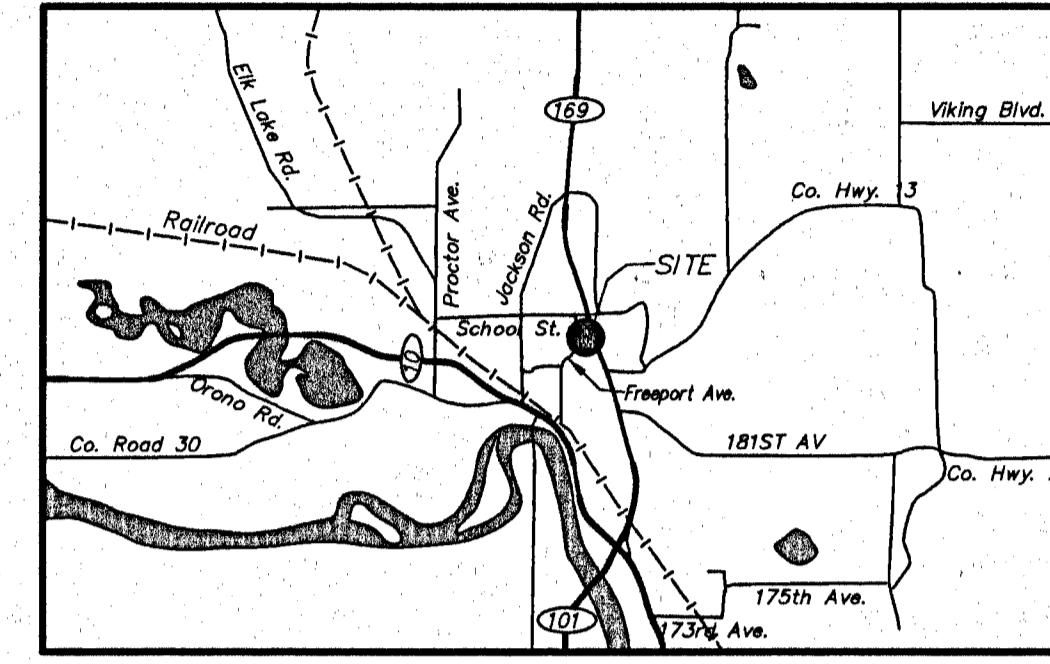
With the growing popularity of pickleball and Minnesota’s long winters, this facility addresses a year-round need for indoor recreational opportunities. We anticipate approximately 700-900 visitors per week, with those visitors spread out across the hours of operation. Pickle Paradise will operate from 6 AM to 10 PM, seven days a week, with limited closures on major holidays. The facility will typically be staffed by 2–3 employees during regular hours and up to 6 during peak periods and events.

The site offers ample parking and existing infrastructure that can comfortably support our projected traffic. We expect staggered traffic flow with minimal congestion due to the nature of the sport (typically only 4 players per court, per hour), court reservation and management systems, and event scheduling. All activities are indoors, minimizing any external noise or disruption to neighboring businesses or residential areas.

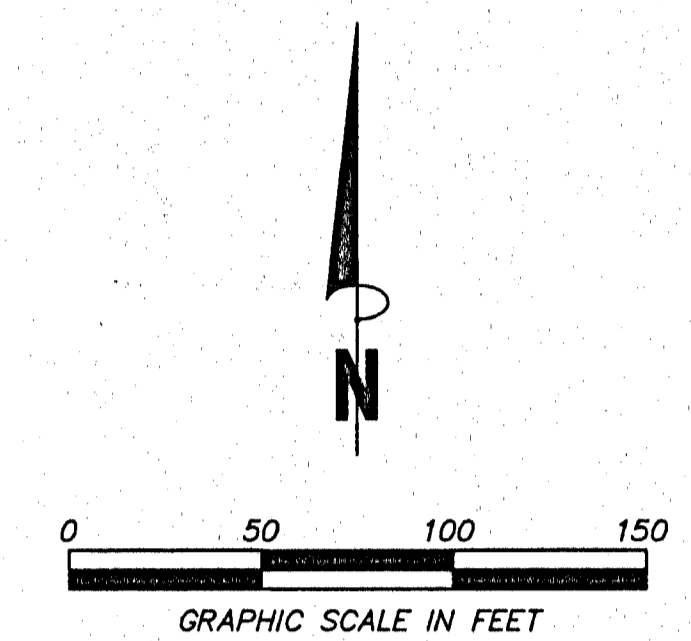
The building will be ADA compliant and outfitted with appropriate life-safety measures including emergency exits, security cameras, and clear signage, in coordination with the building owner, Price Homes.. Any exterior lighting or signage will meet Elk River’s ordinances and maintain neighborhood compatibility. Pickle Paradise will bring health, wellness, and benefits to the community, offering programs for leagues, youth, seniors, and private groups.. We look forward to creating a positive and active destination that contributes to the commerce and vitality of Elk River.

ALTA/ACSM LAND TITLE SURVEY FOR SCHERER BROTHERS LUMBER COMPANY

9110 83RD AVENUE NORTH
BROOKLYN PARK, MN 55445
PH. (612) 379-9633



VICINITY MAP



GRAPHIC SCALE IN FEET

- DENOTES IRON MONUMENT SET #7439
- ⊗ DENOTES DRILL HOLE SET IN CONCRETE
- ▲ DENOTES NAIL SET IN BITUMINOUS
- DENOTES IRON MONUMENT FOUND #7439

SURVEYOR'S NOTE

A Title Commitment was not provided for Lot 2, Block 1, Elk River Plaza Second Addition.

PROPERTY

Lot 1, Block 1, Elk River Plaza Second Addition, according to the plat thereof on file and of record in the office of the County Recorder in and for Sherburne County, Minnesota. Together with an easement for utilities, ingress, egress, drainage and parking over that part of Lot 2, Block 1, Elk River Plaza Second Addition lying northerly of the South line of said Lot 1, Block 1, Elk River Plaza Second Addition, as extended to the west and west of the westerly line of said Lot 1, Block 1, Elk River Plaza Second Addition.

Lot 2, Block 1, Elk River Plaza Second Addition, according to the plat thereof on file and of record in the office of the County Recorder in and for Sherburne County, Minnesota.

SURVEYOR'S CERTIFICATE

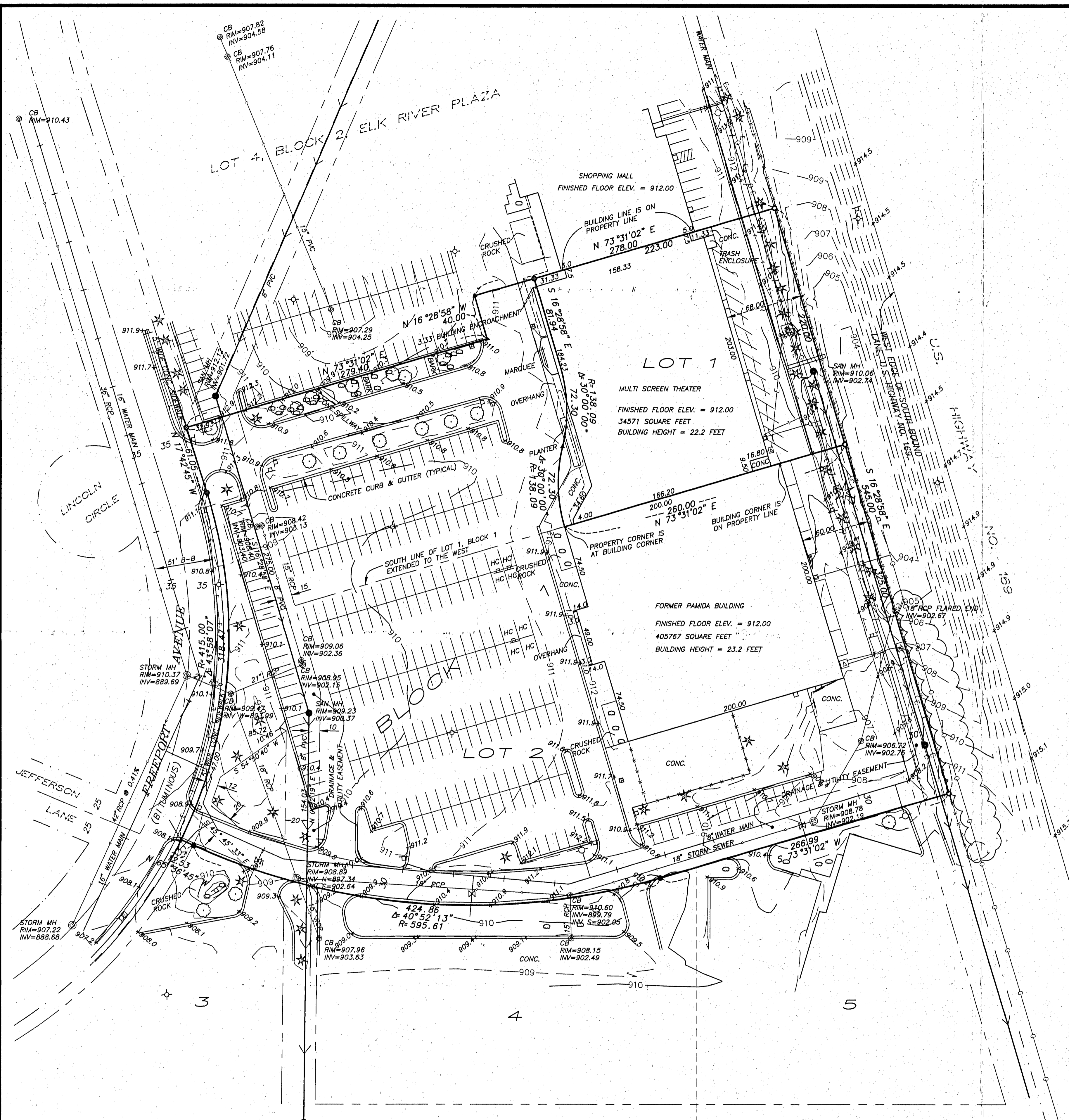
This survey is made for the benefit of B & G Realty, Inc., Old Republic National Title Insurance Company and all lenders that lend money secured by the Property (as defined below) within one (1) year of the date hereof, and their respective successors and assigns.

The undersigned hereby certifies to the aforesaid parties, as of the date set forth below, that:

- A. This survey is an accurate representation of the premises described in Old Republic National Title Insurance Company Commitment No. 5-TLS 18021-E, dated March 13, 1992, (the "Property"), that was made on the ground as per the field notes shown hereon in accordance with the "Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys" jointly established as revised by A.C.S.M. and A.L.T.A. in 1992, and includes items 1 through 11 and 13 of Table A thereof, was prepared in accordance with the Accuracy Standards of an Urban Survey, and correctly shows the boundary lines and dimensions and area of the Property and each individual parcel thereof, correctly shows the distance to the nearest intersection street and the point of reference from which the Property and each individual parcel thereof is measured in the record description, correctly shows the size, type and location of all buildings, structures and other improvements on the Property, and correctly shows the location and dimensions of all alleys, streets, roads, rights-of-way, setback lines and easements and other matters of record affecting the Property of which the undersigned has been advised, whether or not visible, according to the legal description of such rights-of-way, setback lines, easements and other matters (with instrument, volume and page numbers as indicated).
- B. Except as shown, there are no visible easements, roads or rights-of-way across the Property or any other easements or rights-of-way of which the undersigned has been advised, no property walls, no encroachments on adjoining premises, easements, streets or alleys by and of said buildings, structures or other improvements, and no encroachments on the Property by buildings, structures or other improvements situated on adjoining premises.
- C. The right-of-way line(s) for road(s) is/are completely contiguous to the perimeter lines of the premises along the entire right-of-way, and said right-of-way(s) is/are dedicated and accepted by the (City/County).
- D. The Property is located in Federal Flood Zone X, Community Panel Number 270 436 00200 dated 2-19-1992.

Robert J. Rohlin
Licensed Land Surveyor No. 7439
in the State of Minnesota
Date: 3-19-98

- LEGEND**
- FIRE HYDRANT
 - ⊗ GATE VALVE
 - SHRUB
 - DECIDUOUS TREE
 - ★ CONIFEROUS TREE
 - FENCE
 - ⊕ LIGHT POLE
 - UTILITY POLE
 - SIGN
 - ⊗ CATCH BASIN
 - SANITARY SEWER MANHOLE
 - TELEPHONE RISER
 - UNDERGROUND GAS MAIN
 - UNDERGROUND ELECTRIC
 - UNDERGROUND TELEPHONE
 - TREE LINE
 - SANITARY SEWER
 - WATER MAIN
 - ⊗ GAS METER
 - POWER BOX
 - ⊗ IRRIGATION VALVE
 - ⊗ ELECTRIC MANHOLE



CITY OF ELK RIVER
13065 ORONO PARKWAY
PO BOX 460
ELK RIVER, MN 55330
612 441-7420

ELK RIVER UTILITIES
322 KING AVE. NW
ELK RIVER, MN 55330
612 441-2020

MINDOT
395 IRELAND BLVD
ST. PAUL, MN
612 298-3221

US WEST COMMUNICATIONS
PO BOX 1301
MINNEAPOLIS, MN 55483
800 603-0000

MINNEGASCO
700 WEST LINDEN AVE.
PO BOX 1165
MINNEAPOLIS, MN 55440
612 321-5426

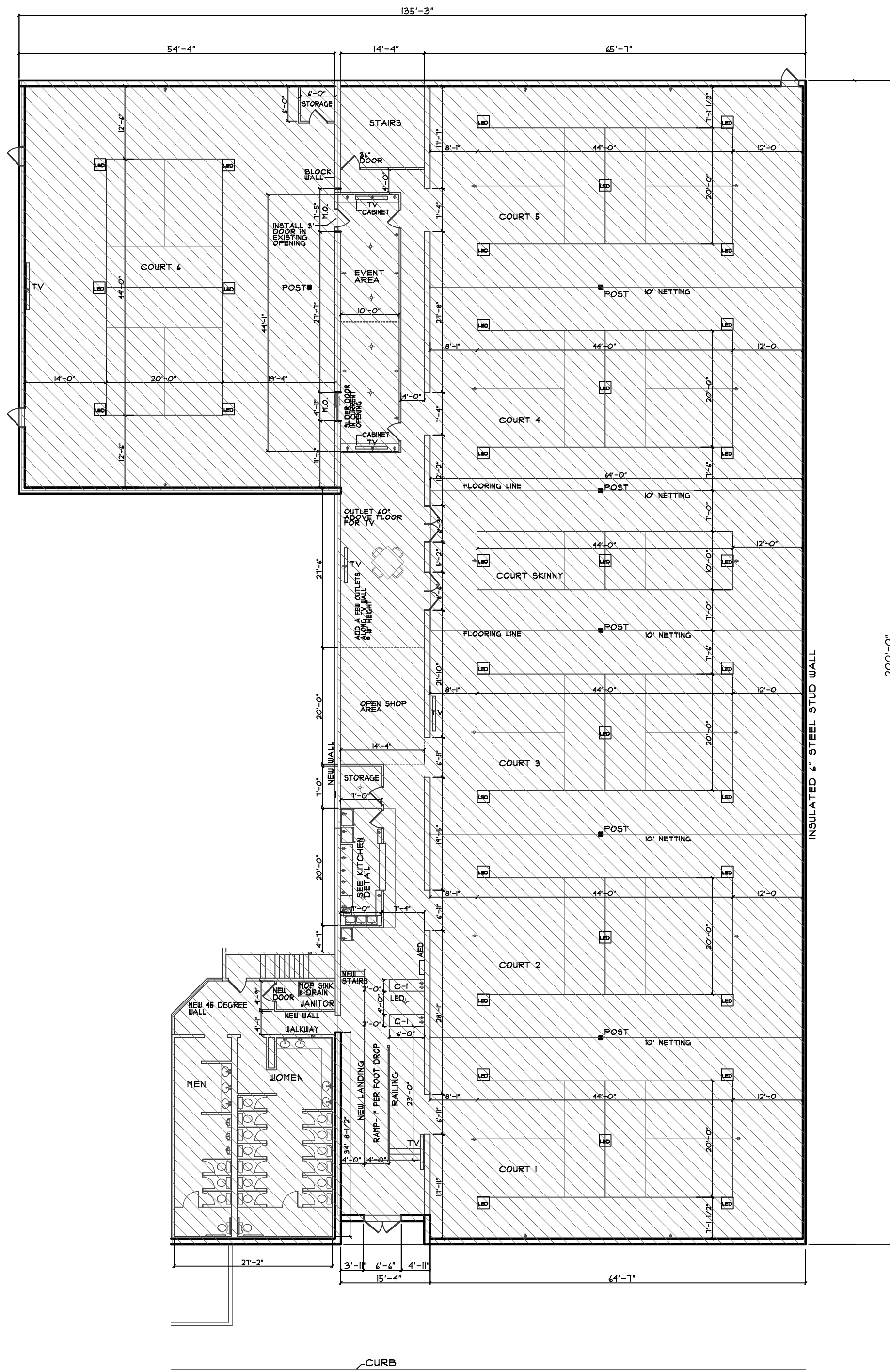
NUMBER OF PARKING SPACES = 301

ZONING - P.U.D., SETBACKS NEGOTIATED BETWEEN DEVELOPER AND CITY

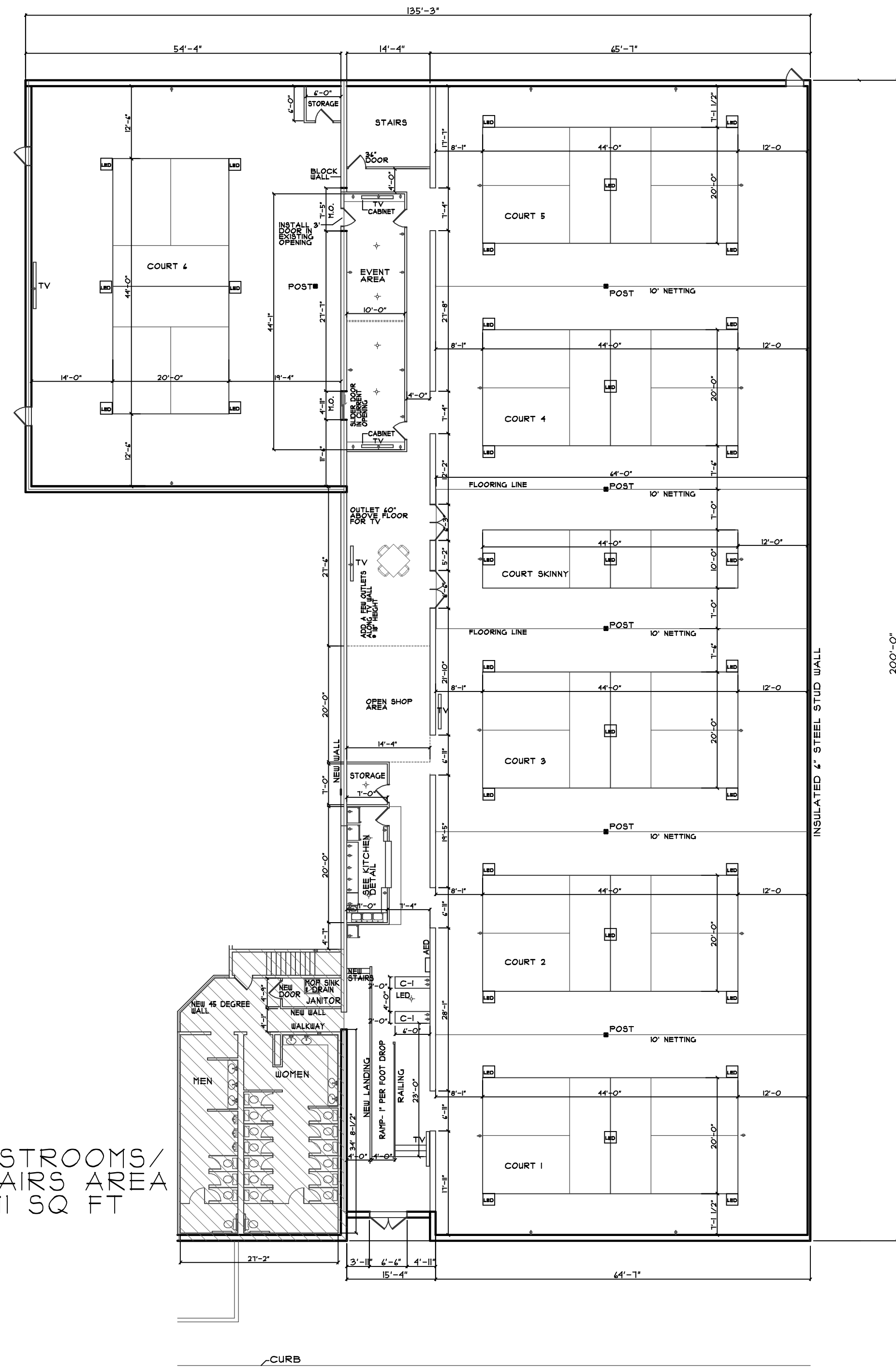
LOT AREAS - LOT 1, BLOCK 1, = 51,615 SQUARE FEET
LOT 2, BLOCK 1, = 247,708 SQUARE FEET

BENCHMARK FINISHED FLOOR OF FRONT ENTRANCE OF THE FORMER PAMIDA BUILDING. ELEVATION = 912.00 NGVD 1929

MEYER-ROHLIN, INC ENGINEERS—LAND SURVEYORS 1111 Hwy. 25N., Buffalo, MN 55313 612 882-1781		REVISIONS	5-4-98 3-31-98
		DATE	3-19-98
DRAWN BY RMB	BOOK 301	PAGE 3	SHEET 1 OF 1 SHEETS
		FILE NO. 98126	



TOTAL SQUARE FOOTAGE
SHADED AREA=21,175 SQ FT



RESTROOMS/
STAIRS AREA
1,371 SQ FT

PICKLEBALL CENTER
SCALE: 1/16" = 1'-0"



Request for Action

To
Planning Commission

Item Number
4.3

Meeting Date
May 27, 2025

Prepared By
Zack Carlton, Community Development Director

Item Description
Preliminary Plat and Ordinance Amendment (PUD):
The Villas at Fillmore Third Addition, PID 75-00931-0030, Riverside Development

Reviewed by
Chris Leeseberg

Action Requested

Recommend, by motion, approval of an ordinance amendment codifying and updating the PUD for The Villas at Fillmore.

Recommend, by motion, approval of the preliminary plat for The Villas at Fillmore Third Addition, with the following conditions:

1. Council approval of the ordinance amendment approving the PUD changes.
2. A Water Availability Charge for each lot must be paid before connection to the municipal water system.
3. Compliance with the City of Elk River Engineering Design Standards is mandatory for all planning and construction activities.
4. The development agreement must include a prorated cost share for the water main extension that was installed along Fillmore Street in 2022 and was paid for by Elk River Municipal Utilities (ERMU) in the amount of \$44,590.
5. The sanitary sewer will be privately owned/maintained.
6. The storm pipe will be privately owned/maintained as it captures only storm water from the private street.
7. Electrical service for the lift station is considered commercial. The developer will need to provide a secondary electric service.

Background/Discussion

The applicant is proposing to update the previous approval for the third addition and complete the final phase of The Villas at Fillmore. The proposal includes six single-family lots located on a private drive. The site has limited development potential due to existing wetlands, storm water features, sanitary sewer limitations, and large overhead powerlines. These factors contributed to the developers' decision to construct the project on a private street.

Site Plan

The Elk River Vision

A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity



The project includes a private drive that provides access to all six parcels, which are generally 60-foot wide. Additional parking, beyond the driveway areas, is provided on one side of the private drive. The homes will be a minimum of 10-feet apart and maintenance will be addressed with a Homeowners Association (HOA).

Public Utilities

All six homes will be connected to city sewer and water. However, the sewer service requires a private lift station to convey the effluent to the city's system. The city will not maintain this lift station, and the development will connect to the public system near the intersection of 177th Ave and Fillmore Street.

The water main is located beneath the trail on the west side of Fillmore Street and the developer is proposing a new service line for each home, which connects directly to the main.

The water main providing service to this phase was installed by the developer with the first phase of this subdivision, but ERMU reimbursed the developer as the extension was for a looped service at the time. ERMU has requested reimbursement for this cost as the extension is now providing service for this phase. These types of utility services are generally installed at the developer's expense.

Ordinance Amendment

The proposed ordinance amendment will take the place of a PUD agreement. The standards of the previous PUD agreement will be codified into city ordinances, providing more clarity and transparency for the public. The only changes proposed are for the inclusion of the third phase and memorializing the private street and utility items.

Preliminary Plat

City ordinance section 30-375 outlines the required findings for approval of a Preliminary Plat.

1. The proposed subdivision is consistent with the zoning regulations (article VI of this chapter) and conforms in all respects with all requirements of this Code, including the zoning regulations and this article.

The property was rezoned to Planned Unit Development with the first phases of this subdivision. With the PUD designation, the development will follow its own development standards, as approved in the ordinance amendment for the PUD.

2. The proposed subdivision is consistent with all applicable general and specialized city, county, and regional plans, including, but not limited to, the city's comprehensive development plan.

As the property was rezoned to PUD and is guided for residential uses by the Comprehensive Plan, the subdivision is consistent with all applicable plans.

3. The physical characteristics of the site, including but not limited to topography, soils, vegetation, susceptibility to erosion and siltation, susceptibility to flooding, and drainage, are suitable for the type and density of development and uses contemplated.

The physical characteristics of the site are suitable for the type and density of development and uses contemplated.

4. The proposed subdivision makes adequate provision for water supply, storm drainage, sewage transportation, erosion control and all other services, facilities and improvements otherwise required in this article.

The subdivision will include infrastructure to meet city requirements. The sanitary service and required lift station will be privately owned and maintained, but will become public after making the connection to existing services. The water service also requires reimbursement for previous work paid for by ERMU, but that now serves the current development phase. Finally, additional permits required to comply with erosion control standards are required and will be secured prior to building permit issuance. These concerns have been included as conditions of approval.

5. The proposed subdivision will not cause substantial environmental damage.

Although there will be site grading, the subdivision will not cause substantial environmental damage.

6. The proposed subdivision will not conflict with easements of record or with easements established by judgment of a court.

The proposed subdivision will not conflict with easements of record or with easements established by judgment of a court.

7. The proposed subdivision will not have an undue and adverse impact on the reasonable development of neighboring land.

The only undeveloped property is one similarly sized and shaped directly to the west of the subject parcel. The proposed subdivision will not impact reasonable development of neighboring land.

8. The proposed subdivision is not premature. A subdivision is premature if any of the following exists:

- a. Lack of adequate stormwater drainage.*
- b. Lack of adequate roads.*
- c. Lack of adequate sanitary sewer systems.*
- d. Lack of adequate off-site public improvements or support systems.*

The subdivision is not premature as the above conditions have been provided for.

Financial Impact

None

Mission/Policy/Goal

Support the growth and development of the community.

Responsibly grow.

Attachments

1. Location Map
2. Preliminary Plat
3. Civil Plans dated April 23, 2025



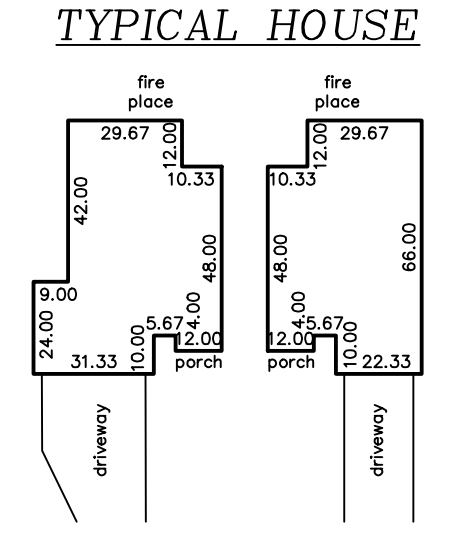
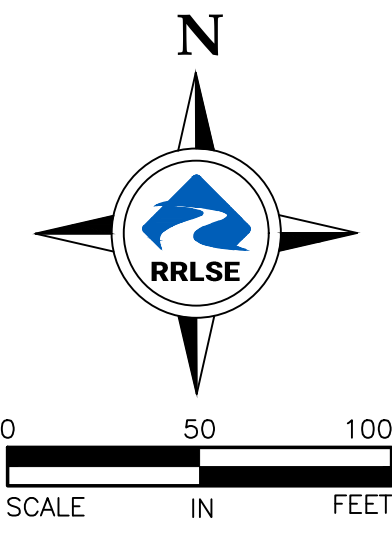
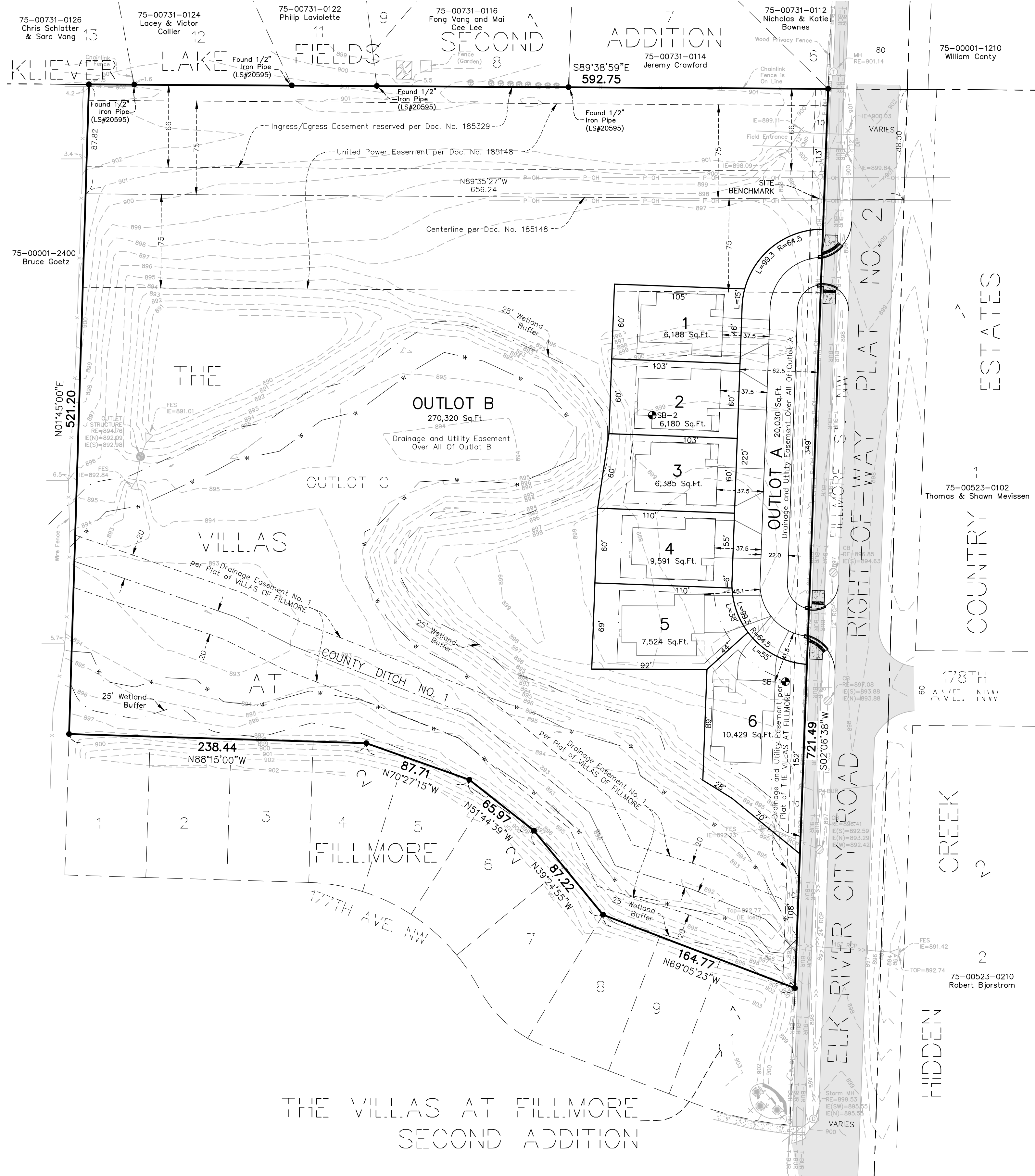
Project Location Map

Riverside Development

Preliminary Plat (P 25-10)

Final Plat (P 25-11)



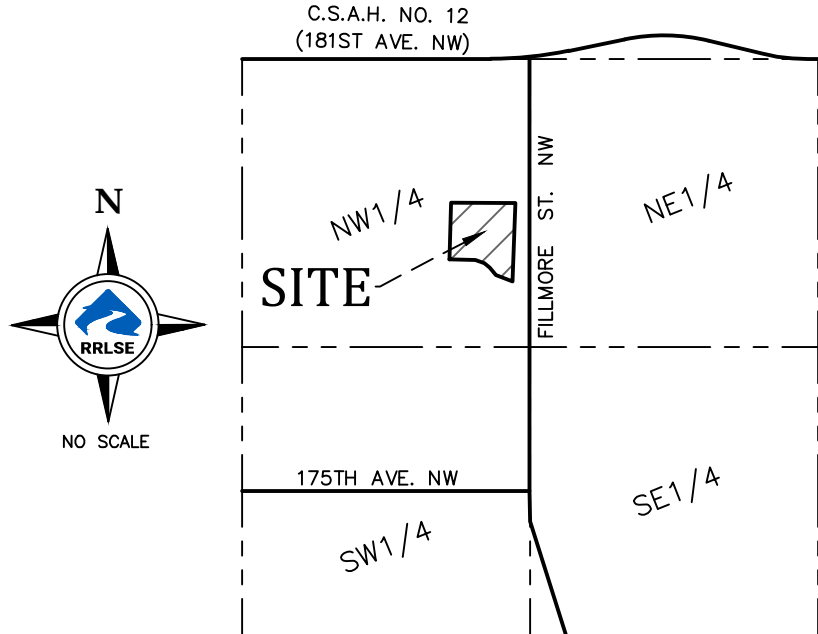


- LEGEND**
- = DENOTES FOUND 1/2" X 14" IRON PIPE WITH PLASTIC CAP MARKED "RLS 49138" UNLESS OTHERWISE NOTED
 - 980 = DENOTES EXISTING CONTOUR (1 FOOT INTERVAL)
 - = DENOTES BUILDING SETBACK LINE
 - - - = DENOTES 25' WETLAND BUFFER
 - W- = DENOTES EDGE OF DELINEATED WETLAND PER PLAT OF THE VILLAS AT FILLMORE
 - SB-1 = DENOTES SOIL BORING BY OTHERS
 - S- = DENOTES SANITARY SEWER LINE
 - CB- = DENOTES CATCHBASIN
 - FES- = DENOTES FES
 - SS- = DENOTES STORM SEWER LINE
 - UP- = DENOTES UTILITY POLE
 - OH- = DENOTES OVERHEAD UTILITY LINES
 - BUR- = DENOTES BURIED TELECOMMUNICATIONS
 - TM- = DENOTES TELECOMMUNICATIONS MANHOLE
 - S- = DENOTES SIGN
 - = DENOTES DECIDUOUS TREE (DIAMETER)
 - = DENOTES CONIFEROUS TREE (DIAMETER)
 - = SHRUB
 - = BITUMINOUS SURFACE
 - = CONCRETE SURFACE

DESCRIPTION OF PROPERTY TO BE PLATTED:
 (PID NO. 75-00931-0030)
 Outlot C, THE VILLAS AT FILLMORE, Sherburne County, Minnesota.

- NOTES:**
- The professional surveyor has made no investigation or independent search for easements of record, encumbrance, restrictive covenants, ownership title evidence, or any other facts that an accurate and current title search may disclose.
 - The underground utilities shown are depicted per observed evidence. The surveyor makes no guarantee that the underground utilities shown comprise all such utilities in the area, either in service or abandoned. The surveyor further does not warrant that the underground utilities shown are in the exact location indicated although he does certify that they are located as accurately as possible from information available. The surveyor has not physically located the underground utilities. Prior to any excavation, contact Gopher State One Call for an on-site location (651-454-0002). The subsurface utility information on this survey is utility quality level D. This quality level was determined according to the guidelines of CI/ASCE 38-2, entitled "Standard Guidelines for the Collection and Depiction of Existing Subsurface Utility Data."
 - Project Benchmark: MnDOT Monument No. POWER MNDT AZ MK. Elevation = 902.23 feet (NGVD 1929)
 Site Benchmark: Railroad Spike in East Face of Power Pole on the West side of Fillmore Street NW, 400 feet North of the intersection with 178th Ave. NW. Elevation = 899.85 feet (NGVD 1929)

VICINITY MAP
 SEC. 1, T. 32, R. 26



Municipality:	City of Elk River	Proposed Building Setbacks:	Front yard: 37.5 feet (From Road Edge) Side yard: 25 feet (Street) Rear yard: 20 feet Side yard: 5 feet (Interior)
Existing Zoning:	R1c - Single Family Residential	Proposed Zoning:	Planned Unit Development
Proposed Zoning:	Planned Unit Development	Road Mileage:	300 LF or 0.06 Miles (Private)
Road Mileage:	300 LF or 0.06 Miles (Private)	Proposed Utilities:	Sewer: Municipal Water: Municipal
Proposed Utilities:	Sewer: Municipal Water: Municipal	Street Lighting:	As Required
Plot Area:	Total Area: 346,310 Sq.Ft. = 7.950 Acres Proposed ROW: 0 Sq.Ft. = 0.00 Acres Park Area: 0 Sq.Ft. = 0.00 Acres	Owner/Subdivider:	Riverside Development Company of Princeton 19230 Evans St. NW Suite 115 Elk River, MN 55330 Marly Glines - (218) 360-4188
		Surveyor:	Rum River Land Surveyors & Engineers PO Box 1044 - 3601 Thurston Ave. Princeton, MN 55371 - Anoka, MN 55303 (763) 389-4476 (763) 427-5860 brian@raa-inc.com

DATE	REVISION
04/25/2025	New Lot Layout
Apr 25, 2025 - 9:33am K:\cod_surr\Land Desktop 2008\2135.36 VILLAS 3rd\dwg\2135.36 PRE PLAT.dwg	

I hereby certify that this survey, plan or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Land Surveyor under the laws of the State of Minnesota.

Brian Person
 Brian Person, PLS
 Date 04/25/2025 Lic. No. 49138

DESIGNED BY:	SMM
DRAWN BY:	JJ
CHECKED BY:	BP
DATE:	04/25/2025
FILE NO.:	P-2135.36



THE VILLAS AT FILLMORE
 3RD ADDITION

PRELIMINARY PLAT
 FOR
 RIVERSIDE DEVELOPMENT COMPANY OF PRINCETON
 SHERBURNE COUNTY, MINNESOTA

SHEET	1	OF	1
SHEETS			

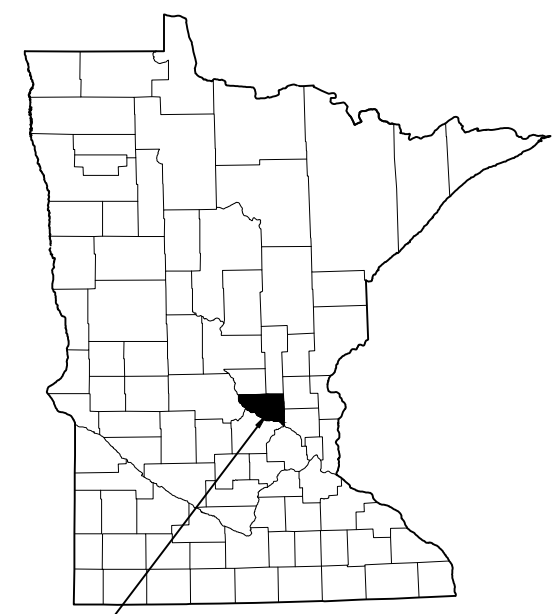
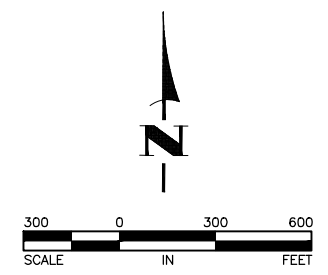
THE VILLAS AT FILLMORE THIRD ADDITION

CITY OF ELK RIVER, MINNESOTA



PROJECT LEGEND

---	900	EXISTING CONTOUR
---	900	PROPOSED CONTOUR
---		PROJECT PROPERTY LINE
---		SURROUNDING PROPERTY LINE
---		RIGHT OF WAY LINE
---		EASEMENT LINE
---	FO-BUR	BURIED FIBER OPTIC CABLE
---	T-BUR	BURIED TELEPHONE CABLE
---	G	GAS MAIN
---	P-BUR	BURIED ELECTRIC CABLE
---	P-OH	OVERHEAD ELECTRIC CABLE
---		UTILITY PEDESTAL
---		POWER POLE
---		GUY WIRE
---		LIGHT POLE
---		SIGNAL POLE
---		STORM SEWER
---		STORM SEWER MANHOLE
---		CATCH BASIN
---		FES
---		WATERMAIN
---		WATERMAIN MANHOLE
---		HYDRANT
---		GATE/BUTTERFLY VALVE
---		WATER SERVICE
---		SANITARY SEWER
---		SANITARY SEWER MANHOLE
---		SANITARY CLEANOUT
---		SEWER SERVICE
---		TRUNCATED DOMES
---		CONCRETE CURB & GUTTER
---		SIGN
---		MAILBOX
---		DETAIL NUMBER
---		SHEET NUMBER
---		SAWCUT BITUMINOUS OR CONCRETE
---		DELINEATED WETLAND
---		EXISTING TREELINE
---		SOIL BORING
---		CONIFEROUS TREE
---		DECIDUOUS TREE
---		CITY STANDARD PLATE



CITY OF ELK RIVER,
SHERBURNE COUNTY,
MINNESOTA

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-2, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA."

GOVERNING SPECIFICATIONS

THE "CITY OF ELK RIVER ENGINEERING DESIGN STANDARDS" SHALL APPLY. WHEN THE "CITY OF ELK RIVER ENGINEERING DESIGN STANDARDS" CONFLICT WITH OTHER SPECIFICATIONS, THE "CITY OF ELK RIVER ENGINEERING DESIGN STANDARDS" SHALL GOVERN.

THE 2020 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" SHALL APPLY.

THE 2023 EDITION OF THE CITY ENGINEERS ASSOCIATION OF MINNESOTA (CEAM) STANDARD SPECIFICATIONS SHALL APPLY.

ALL FEDERAL, STATE AND LOCAL LAWS, REGULATIONS, AND ORDINANCES SHALL BE COMPLIED WITH IN THE CONSTRUCTION OF THIS PROJECT.

ALL TRAFFIC CONTROL DEVICES AND SIGNING SHALL CONFORM TO THE LATEST EDITION OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD), INCLUDING THE LATEST FIELD MANUAL FOR TEMPORARY TRAFFIC CONTROL ZONE LAYOUTS.

ALL REQUIREMENTS OF THE PROJECT MANUAL FOR THE THE VILLAS AT FILLMORE THIRD ADDITION.

SHEET INDEX

THIS PLAN CONTAINS 21 SHEETS

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	CONSTRUCTION NOTES AND ESTIMATED QUANTITIES
3-6	DETAILS
7	TYPICAL LOT SECTIONS, SOIL NOTES, AND DETAILS
8	EXISTING CONDITIONS, REMOVALS, AND TRAFFIC CONTROL PLAN
9	GRADING AND EROSION CONTROL PLAN
10	WATERMAIN AND SANITARY SEWER PLAN
11	STREET AND STORM SEWER PLAN
12	RESTORATION PLAN
13	PEDESTRIAN RAMP CONSTRUCTION PLAN
14	FORCEMAIN CONSTRUCTION PLAN
15	LIFT STATION LAYOUT AND CONTROL PANEL
16-21	MNDOT PEDESTRIAN CURB RAMP DETAILS

APPROVED _____ DATE _____
JUSTIN FEMRITE, P.E.
CITY ENGINEER

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.
Craig J. Jochum 23461 DATE 4/23/25
CRAIG J. JOCHUM, P.E. LIC. NO.
HAKANSON ANDERSON
DESIGN ENGINEER

DATE	REVISION

SHEET 1 OF 21 SHEETS



Apr 28, 2025 - 8:51am
K:\PRIVATE\2135.36\ENGINEERING\PLAN DWG\2135.36_TITILE.dwg

GENERAL CONSTRUCTION NOTES:

1. SAWCUT AND MILL WHEN PLACING NEW PAVEMENT ADJACENT TO INPLACE PAVEMENT AND AT TERMINI OF CONSTRUCTION TO ENSURE A UNIFORM JOINT.
2. BITUMINOUS AND CONCRETE ITEMS DISTURBED BY CONSTRUCTION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF IN ACCORDANCE WITH MN/DOT SPEC. 2104.
3. USE TACK COAT BETWEEN ALL BITUMINOUS MIXTURES. THE BITUMINOUS TACK COAT MATERIAL SHALL BE APPLIED AT A UNIFORM RATE OF 0.05 GAL/SY TO 0.07 GAL/SY BETWEEN BITUMINOUS LAYERS. THE APPLICATION RATES ARE FOR UNDILUTED EMULSIONS.
4. PERFORMANCE GRADED (PG) ASPHALT BINDER PG 58H-34, SPEC. 3151, SHALL BE USED FOR ALL BITUMINOUS MIXES ON THIS PROJECT. SPECIFIC PG GRADES ARE SHOWN AT THE END OF THE MIX DESIGNATION NUMBER SHOWN ON THE TYPICAL SECTION.
5. THE BITUMINOUS MIXTURES SHALL MEET THE REQUIREMENTS OF SPECIFICATIONS 2360 AND 3139.
6. CONTRACTOR SHALL SCHEDULE CONSTRUCTION IN COOPERATION WITH UTILITY RELOCATIONS IF ANY ARE NECESSARY. INSTALLATION OF NEW CONDUITS WILL BE REQUIRED. THE UTILITY COMPANY WILL FURNISH THE CONDUIT AND CONTRACTOR SHALL INSTALL THEM. EACH CROSSING WILL HAVE MULTIPLE CONDUITS. THE TRENCH WIDTH WILL NEED TO BE WIDE ENOUGH TO SEPARATE EACH UTILITY CONDUIT. A TOTAL OF TWO CROSSINGS ARE ANTICIPATED. THIS WORK SHALL BE PAID AS ITEM 2503 UTILITY CROSSINGS.
7. ALL STORM SEWER SHALL BE REINFORCED CONCRETE PIPE UNLESS OTHERWISE NOTED.
8. HYDRANTS SHALL MEET THE REQUIREMENTS OF CSP 3401.
9. PIPE BEDDING SHALL BE PER CSP 3406.
10. SUBGRADE PREPARATION FOR ALL TRAILS AND WALKS SHALL BE INCIDENTAL.
11. CONCRETE CURB AND GUTTER SHALL BE THE 28" SURMOUNTABLE DESIGN SHOWN IN CSP 2000.
12. THE BITUMINOUS WEARING COURSE SHALL NOT BE PLACED UNTIL THE YEAR 2026.
13. CONCRETE MAT SHALL BE CABLE CONCRETE CC35 MANUFACTURED BY IECS, INC., FLEXAMAT PLUS MANUFACTURED BY MOTZ ENTERPRISES, INC., OR APPROVED EQUAL.
14. CONTRACTOR SHALL CLEAN AND SWEEP THE BITUMINOUS SURFACE PRIOR TO CONSTRUCTING THE WEAR COURSE. THIS WORK SHALL BE INCIDENTAL.
15. SEGMENTS OF THE FORCEMAIN WILL NEED AN OPEN CUT TRENCH. ALL FORCEMAIN CONSTRUCTION WILL BE PAID PER ITEM 2.0" HDPE FORCEMAIN DIRECTIONALLY DRILLED NO MATTER WHAT METHOD IS USED FOR INSTALLATION. ALL REQUIRED FORCEMAIN FITTINGS SHALL BE INCIDENTAL.

GENERAL EROSION CONTROL NOTES:

1. PRIOR TO ANY CONSTRUCTION ACTIVITIES THE CONTRACTOR SHALL ACQUIRE THE NECESSARY MPCA-NPDES STORMWATER PERMIT.
2. THE CONTRACTOR SHALL INSTALL EROSION AND SEDIMENT CONTROL FACILITIES (BMP'S) PRIOR TO GRADING AND REMOVAL ACTIVITIES. BMP'S SHALL BE MAINTAINED FOR THE DURATION OF CONSTRUCTION ACTIVITIES AND POTENTIAL FOR EROSION HAS PASSED.
3. THE CONTRACTOR SHALL SCHEDULE THEIR OPERATIONS TO MINIMIZE THE AMOUNT OF DISTURBED AREA AT ANY GIVEN TIME.
4. BMP'S SHALL BE INSPECTED DAILY BY THE CONTRACTOR AND DOCUMENTED IN THE SWPPP INSPECTION LOG. SWPPP SHALL BE PROVIDED TO THE CITY.
5. ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS APPROVED BY THE ENGINEER AND THE CITY OF ELK RIVER. THIS WORK SHALL BE INCIDENTAL.
6. THE CONTRACTOR SHALL FILE NOTICE OF TERMINATION (NOT) WITH THE MPCA WHEN FINAL STABILIZATION IS APPROVED BY THE ENGINEER.
7. SILT FENCE & BLANKET SHALL BE INSTALLED BEHIND THE CURB DURING CONSTRUCTION BEFORE CLASS 5 AGGREGATE BASE IS CONSTRUCTED.
8. ALL TEMPORARY SOIL STABILIZATION AND SEEDING NECESSARY TO MEET THE REQUIREMENTS OF THE PROJECT MANUAL, MPCA NPDES CONSTRUCTION PERMIT AND MN/DOT STANDARD SPECIFICATION SHALL BE INCIDENTAL.
9. THE CONTRACTOR SHALL POST THE SWPPP ON SITE. THE METHOD OF POSTING SHALL BE APPROVED BY THE ENGINEER.

CITY STANDARD PLATES

PLATE NO.	DESCRIPTION	SHEET NUMBER
2000	SURMOUNTABLE AND B618 CONCRETE CURB AND GUTTER	3
2009	STANDARD SILT FENCE	3
2015	ROCK CONSTRUCTION ENTRANCE BUILDING PERMIT	3
2500	SHALLOW CATCH BASIN	3
2503	CONCRETE PIPE JOINT TIES	4
2504	TRASH GUARD	4
2507	STANDARD INLET CASTING	4
2511	CATCH BASIN INLET PROTECTION	4
2513	CONCRETE EROSION MAT OUTLET INSTALLATION DETAIL	4
3400	TYPICAL WATER LAYOUT	4
3401	PACER HYDRANT DETAIL	4
3402	HYDRANT RESTRAINT DETAIL	4
3403	TYPICAL SERVICE DETAIL	5
3404	WATERMAIN CROSSING	5
3406	CLASS B PIPE BEDDING	5
3408	TRACER WIRE DETAIL	5
3409	TRACER WIRE LAYOUT DETAIL	5
4000	STANDARD MANHOLE	5
4007	TYPICAL HOUSE SERVICE	5

ITEM NO.	Mn/DOT SPEC. NO.	ITEM DESCRIPTION	UNIT	ESTIMATED QUANTITY
1	2021.501	MOBILIZATION	LUMP SUM	1
2	2104.503	SAWING BITUMINOUS PAVEMENT - FULL DEPTH	LIN FT	214
3	2104.504	REMOVE BITUMINOUS PAVEMENT	SQ YD	215
4	2104.601	REMOVE LANDSCAPING	LUMP SUM	1
5	2106.601	DEWATERING	LUMP SUM	1
6	2106.507	EXCAVATION - COMMON	CU YD	1,850
7	2106.607	SALVAGE TOPSOIL (P)	CU YD	1,050
8	2112.519	SUBGRADE PREPARATION	ROAD STATION	3.5
9	2211.509	AGGREGATE BASE CLASS 5	TON	429
10	2232.504	MILL BITUMINOUS SURFACE (1.5")	SQ YD	18
11	2301.504	CONCRETE PAVEMENT 6"	SQ YD	15.1
12	2357.506	BITUMINOUS MATERIAL FOR TACK COAT	GALLONS	47
13	2360.504	TYPE SP 9.5 WEARING COURSE MIXTURE (2,C) 2.5" THICK	SQ YD	40
14	2360.509	TYPE SP 9.5 WEARING COURSE MIXTURE (2,C)	TON	71
15	2360.509	TYPE SP 12.5 NON WEARING COURSE MIXTURE (2,C)	TON	118
16	2501.502	12" RC PIPE APRON	EACH	1
17	2501.502	15" RC PIPE APRON	EACH	2
18	2501.602	TRASH GUARD FOR 12" PIPE APRON	EACH	1
19	2501.602	TRASH GUARD FOR 15" PIPE APRON	EACH	2
20	2503.503	8" PVC PIPE SEWER SDR 35	LIN FT	244
21	2503.503	12" RC PIPE SEWER DESIGN 3006 CLASS V	LIN FT	40
22	2503.503	15" RC PIPE SEWER DESIGN 3006 CLASS V	LIN FT	48
23	2503.601	TRACER WIRE SYSTEM - SEWER FORCEMAIN	LUMP SUM	1
24	2503.602	CONNECT TO EXISTING SANITARY SEWER	EACH	1
25	2503.602	8"x4" PVC WYE SDR 26	EACH	6
26	2503.602	UTILITY CROSSING	EACH	2
27	2503.603	CLEAN AND VIDEO TAPE PIPE SEWER	LIN FT	244
28	2503.603	4" PVC SANITARY SERVICE PIPE SDR 26	LIN FT	250
29	2503.603	2.0" HDPE FORCEMAIN (DIRECTIONALLY DRILLED)	LIN FT	382
30	2504.601	TRACER WIRE SYSTEM - WATER SYSTEM	LUMP SUM	1
31	2504.602	CONNECT TO EXISTING WATERMAIN	EACH	1
32	2504.602	HYDRANT	EACH	1
33	2504.602	1" CORPORATION STOP	EACH	6
34	2504.602	2" CORPORATION STOP	EACH	1
35	2504.602	6" GATE VALVE AND BOX	EACH	1
36	2504.602	1" CURB STOP AND BOX	EACH	6
37	2504.602	2" CURB STOP AND BOX	EACH	1
38	2504.603	1" IPS SIDR 7 HDPE	LIN FT	744
39	2504.603	2" IPS SIDR 7 HDPE	LIN FT	154
40	2504.603	6" WATERMAIN DUCTILE IRON CL 52	LIN FT	52
41	2504.604	4" POLYSTYRENE INSULATION	SQ YD	7.1
42	2506.502	CASTING ASSEMBLY	EACH	3
43	2506.503	CONSTRUCT SANITARY STRUCTURE DESIGN 4007	LIN FT	15.9
44	2506.503	CONSTRUCT DRAINAGE STRUCTURE DESIGN 27 DESIGN H	EACH	1
45	2506.601	INSTALL LIFT STATION	LUMP SUM	1
46	2511.604	CONCRETE MAT	SQ YD	32
47	2521.518	6" CONCRETE WALK	SQ FT	607
48	2531.503	CONCRETE CURB AND GUTTER SURMOUNTABLE	LIN FT	76
49	2531.618	TRUNCATED DOMES	SQ FT	98
50	2563.601	TRAFFIC CONTROL	LUMP SUM	1
51	2564.502	SIGN TYPE C	SQ FT	6.3
52	2564.502	SIGN TYPE D	SQ FT	12
53	2564.602	WETLAND BUFFER SIGN	EACH	3
54	2573.501	STABILIZED CONSTRUCTION EXIT	LUMP SUM	1
55	2573.502	STORM DRAIN INLET PROTECTION	EACH	4
56	2573.503	SILT FENCE, TYPE MS	LIN FT	2,977
57	2574.507	COMMON TOPSOIL BORROW (CV)	CU YD	150
58	2574.508	FERTILIZER TYPE 1	POUND	630
59	2575.504	ROLLED EROSION PREVENTION CATEGORY 25	SQ YD	3,745
60	2575.505	SEEDING	ACRE	2.1
61	2575.508	SEED, SOUTHERN BOULEVARD	POUND	336
62	2575.509	MULCH MATERIAL TYPE 1 - DISK ANCHORED	TON	2.9

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Craig J. Jochem
CRAG J. JOCHUM, P.E.
 Lic. No. 23461

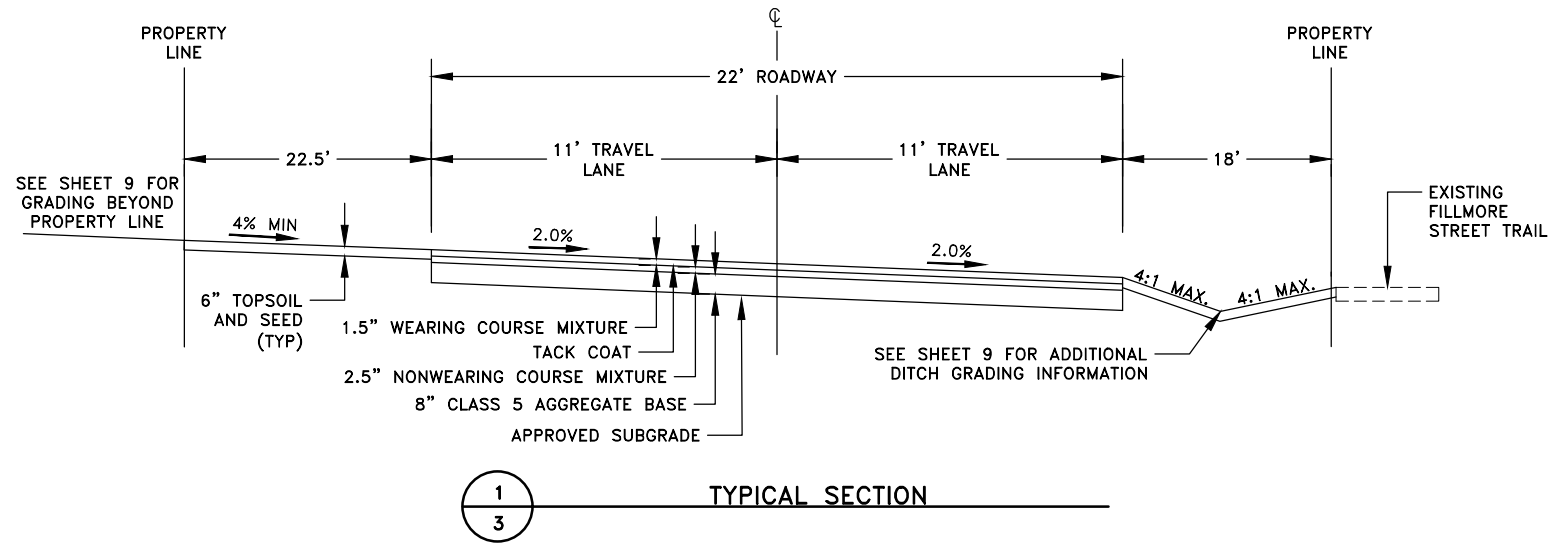
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 DRAWN BY: SGJ
 CHECKED BY: TAE



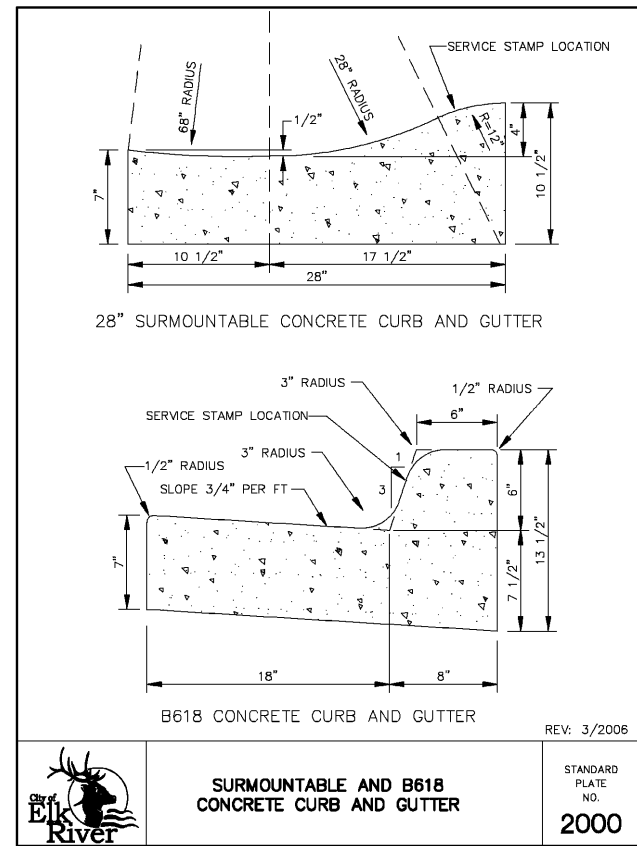
THE VILLAS AT FILLMORE THIRD ADDITION

CONSTRUCTION NOTES AND ESTIMATED QUANTITIES
 CITY OF ELK RIVER, MINNESOTA

SHEET 2 OF 21 SHEETS



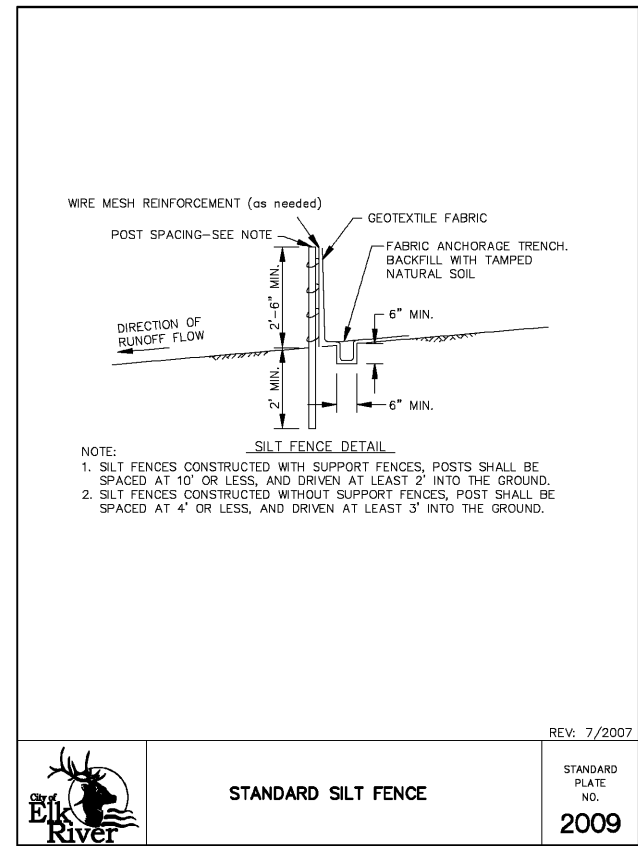
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SURMOUNTABLE AND B618 CONCRETE CURB AND GUTTER

STANDARD PLATE NO. **2000**

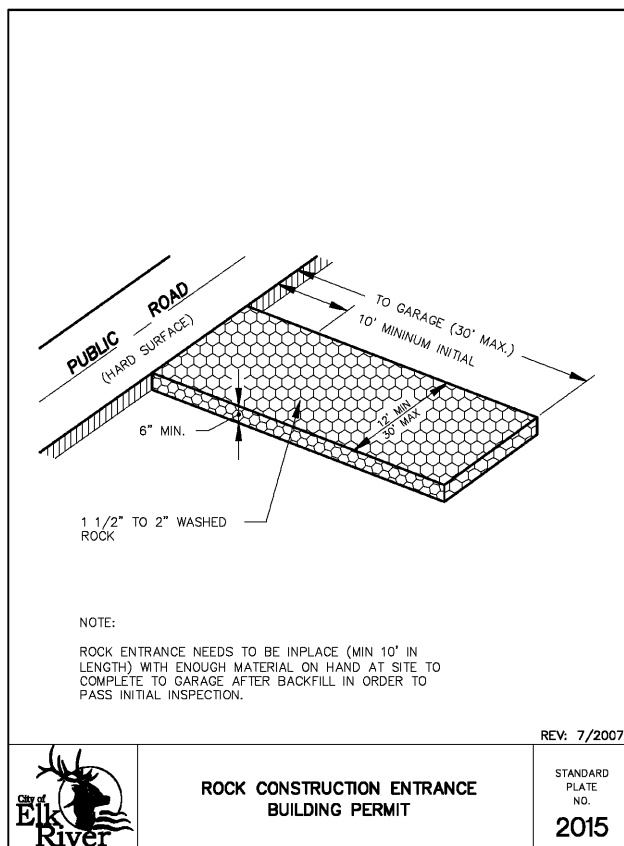
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STANDARD SILT FENCE

STANDARD PLATE NO. **2009**

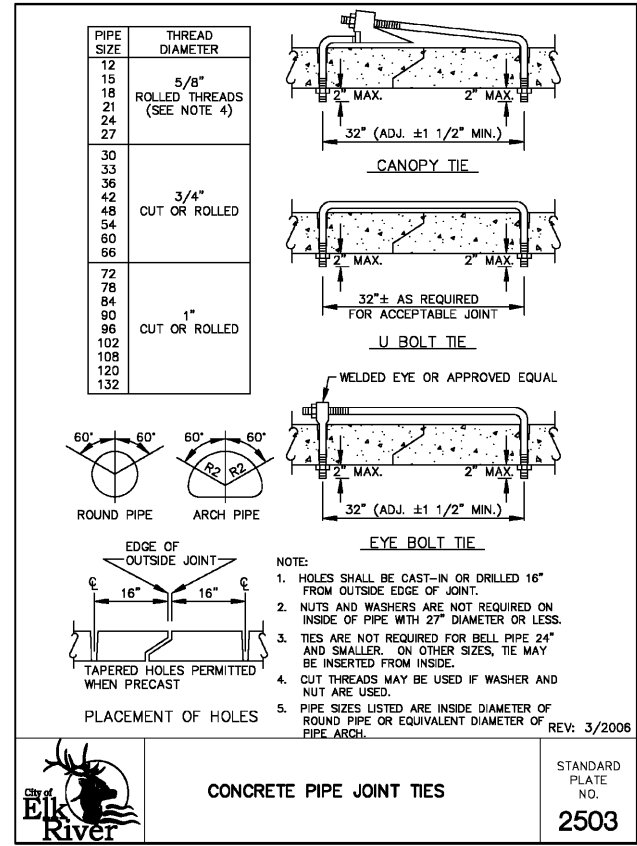
REV: 7/2007



ROCK CONSTRUCTION ENTRANCE BUILDING PERMIT

STANDARD PLATE NO. **2015**

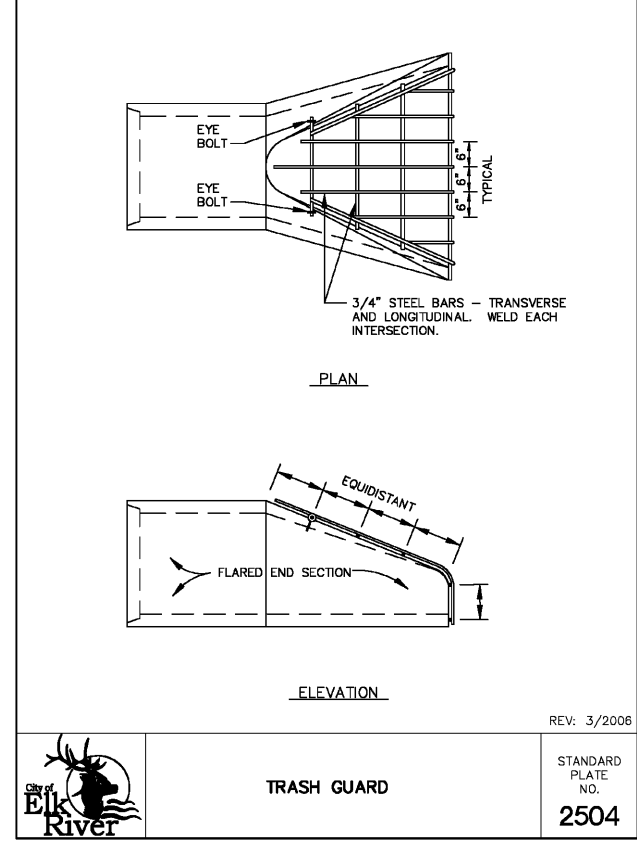
REV: 7/2007



CONCRETE PIPE JOINT TIES

STANDARD PLATE NO. **2503**

REV: 3/2006



TRASH GUARD

STANDARD PLATE NO. **2504**

REV: 3/2006

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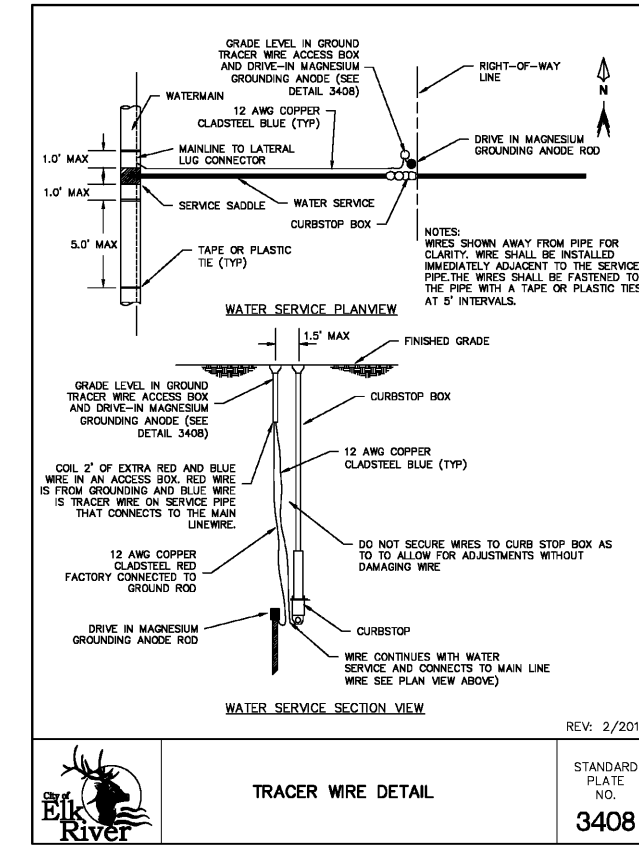
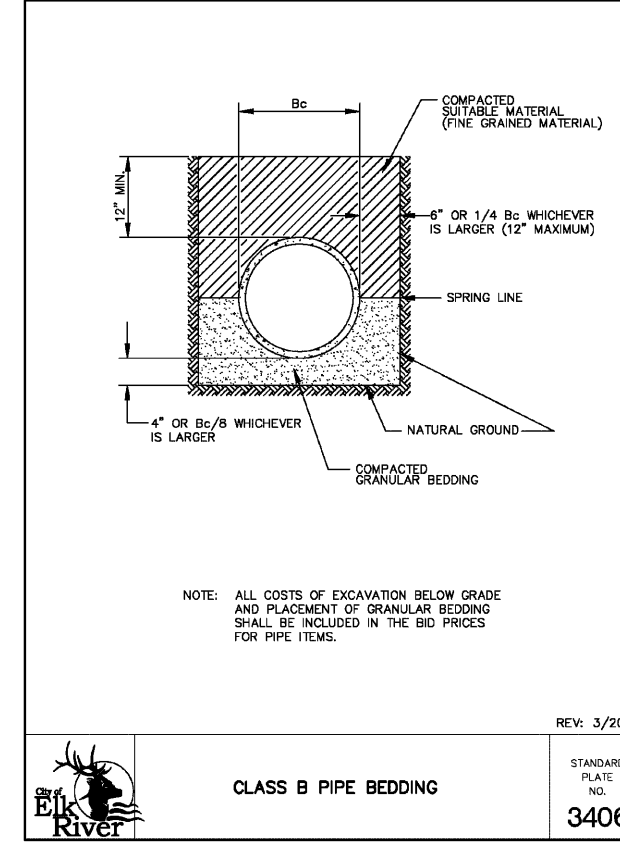
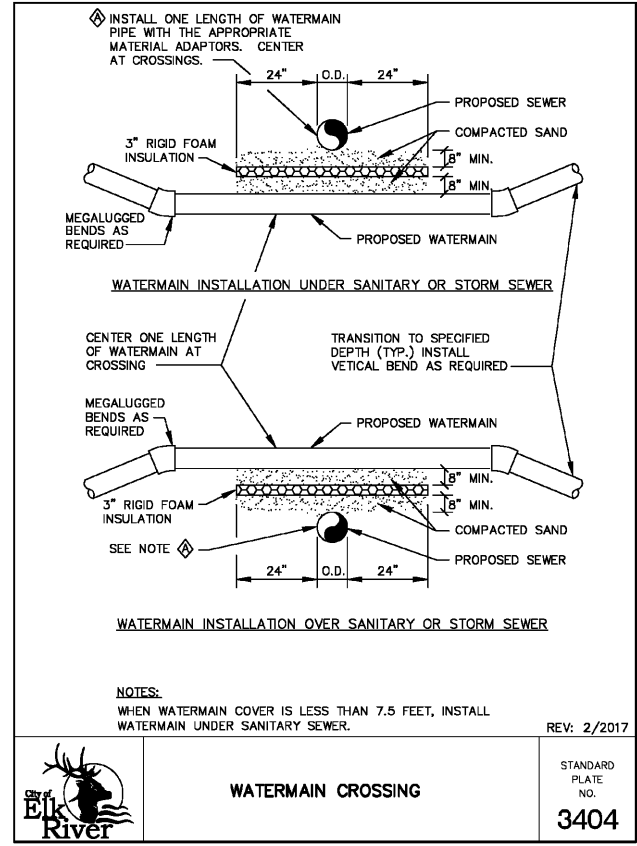
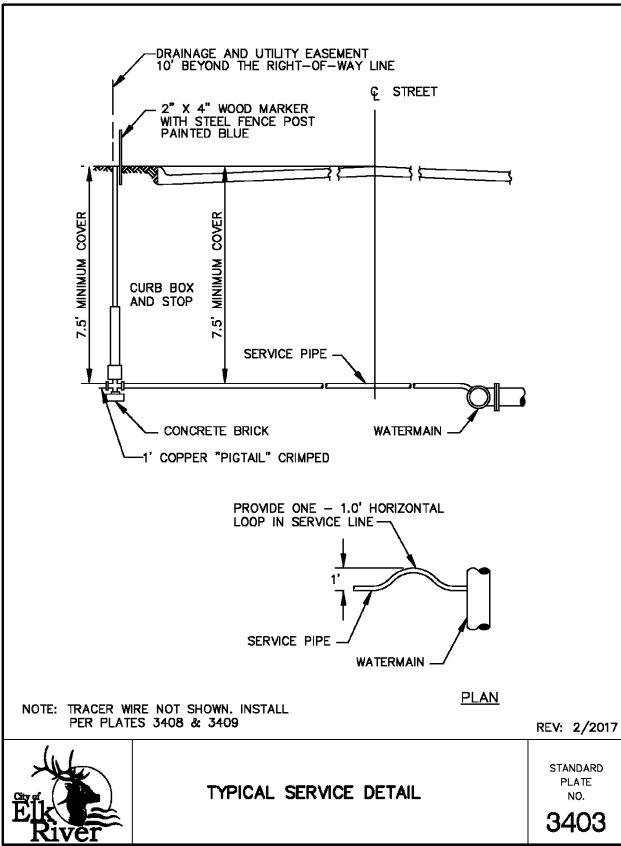
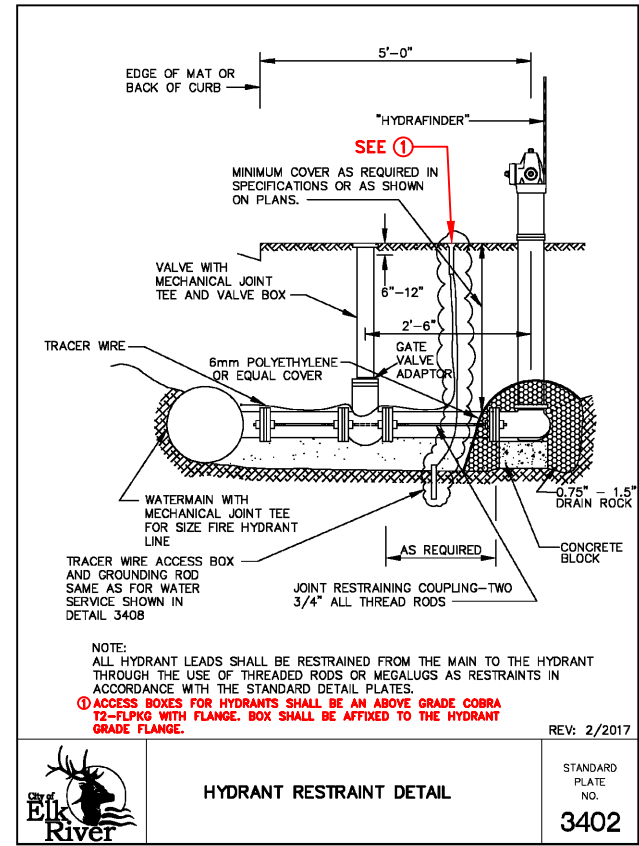
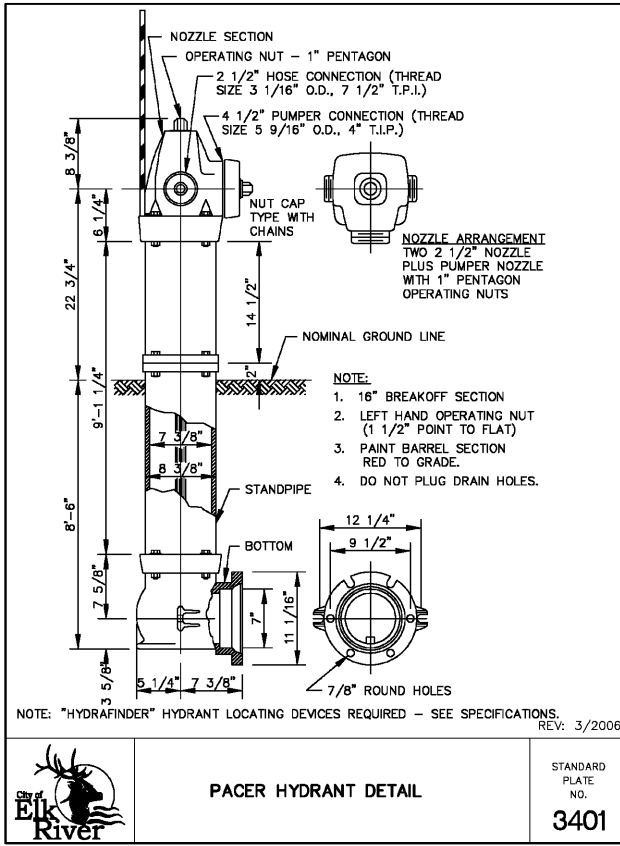
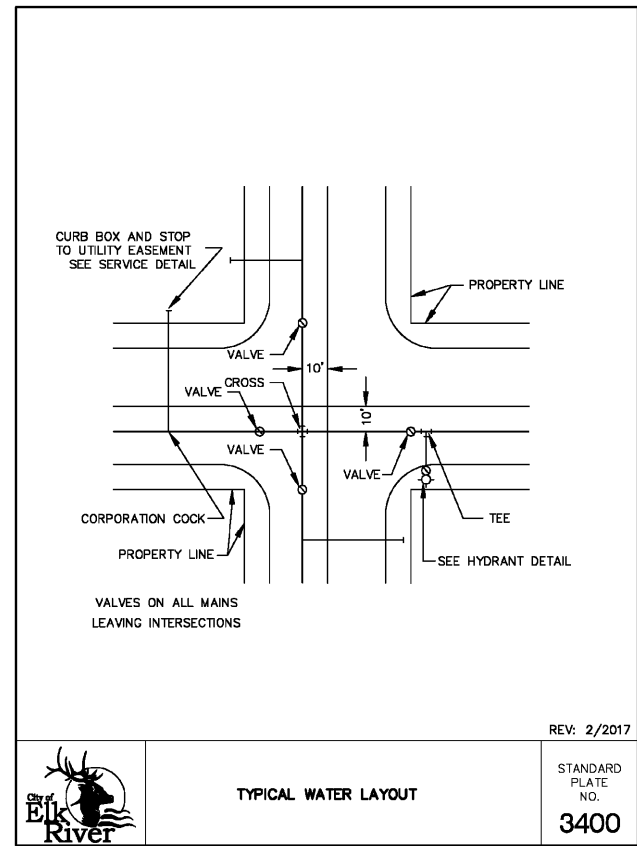
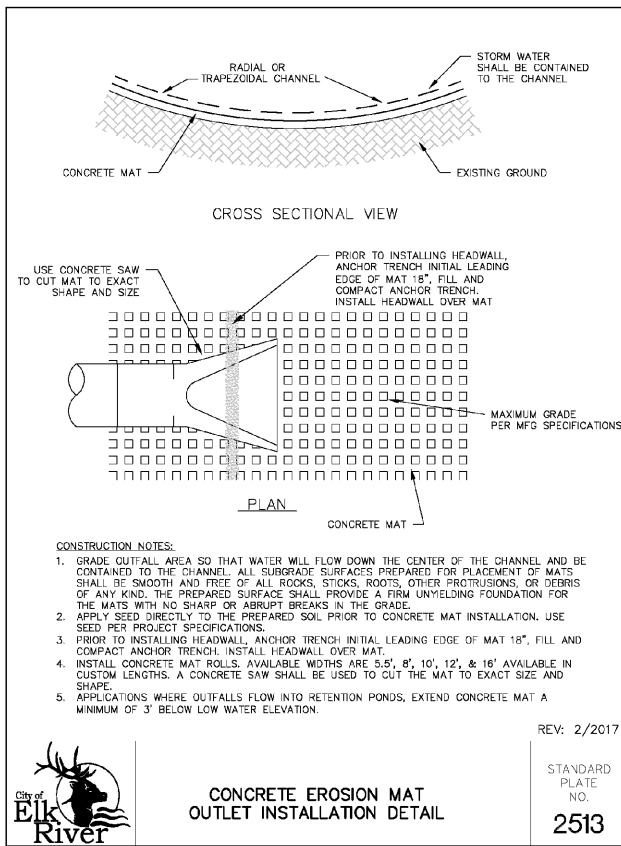
505 First Street, Princeton, MN 55371
0-763.389.4476 RRLSE.com

THE VILLAS AT FILLMORE THIRD ADDITION

DETAILS

CITY OF ELK RIVER, MINNESOTA

SHEET **3** OF **21** SHEETS



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Lic. No. 23461

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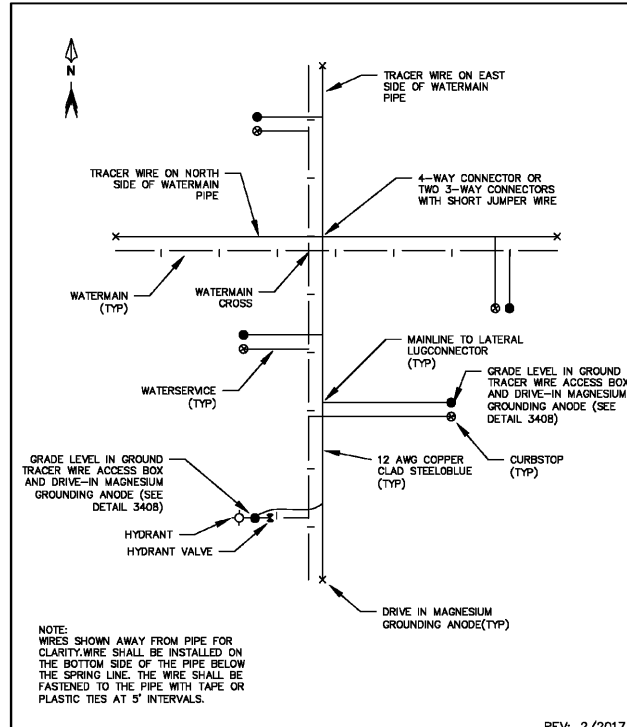
THE VILLAS AT FILLMORE THIRD ADDITION

CITY OF ELK RIVER, MINNESOTA

DETAILS

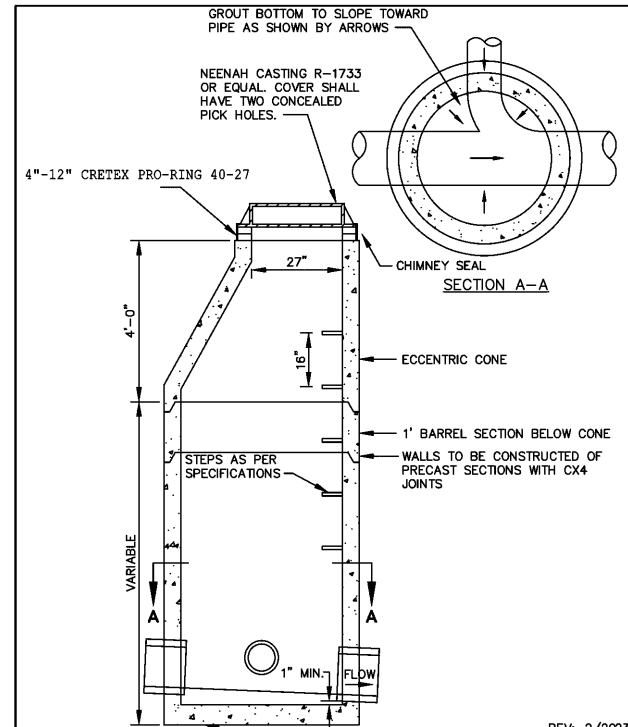
SHEET 4 OF 21 SHEETS

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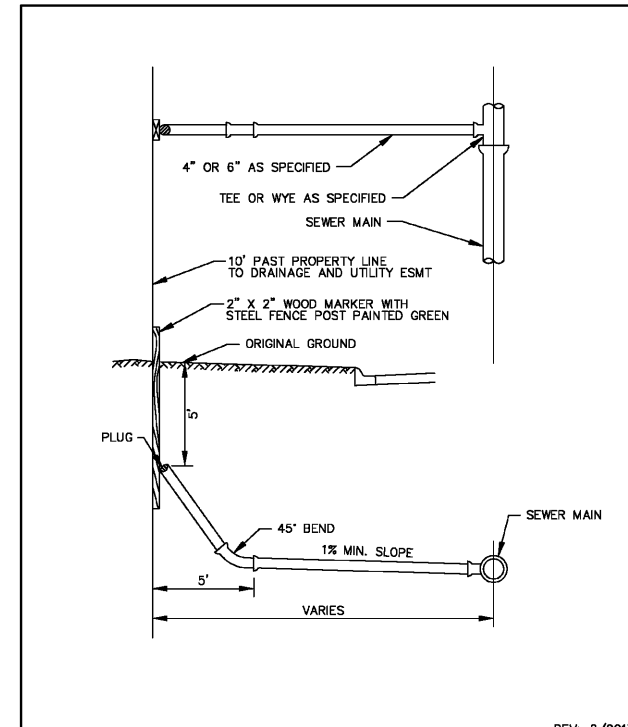
REV: 2/2017

TRACER WIRE LAYOUT DETAIL
STANDARD PLATE NO. **3409**



REV: 2/2023

STANDARD MANHOLE
STANDARD PLATE NO. **4000**



REV: 2/2017

TYPICAL HOUSE SERVICE
STANDARD PLATE NO. **4007**

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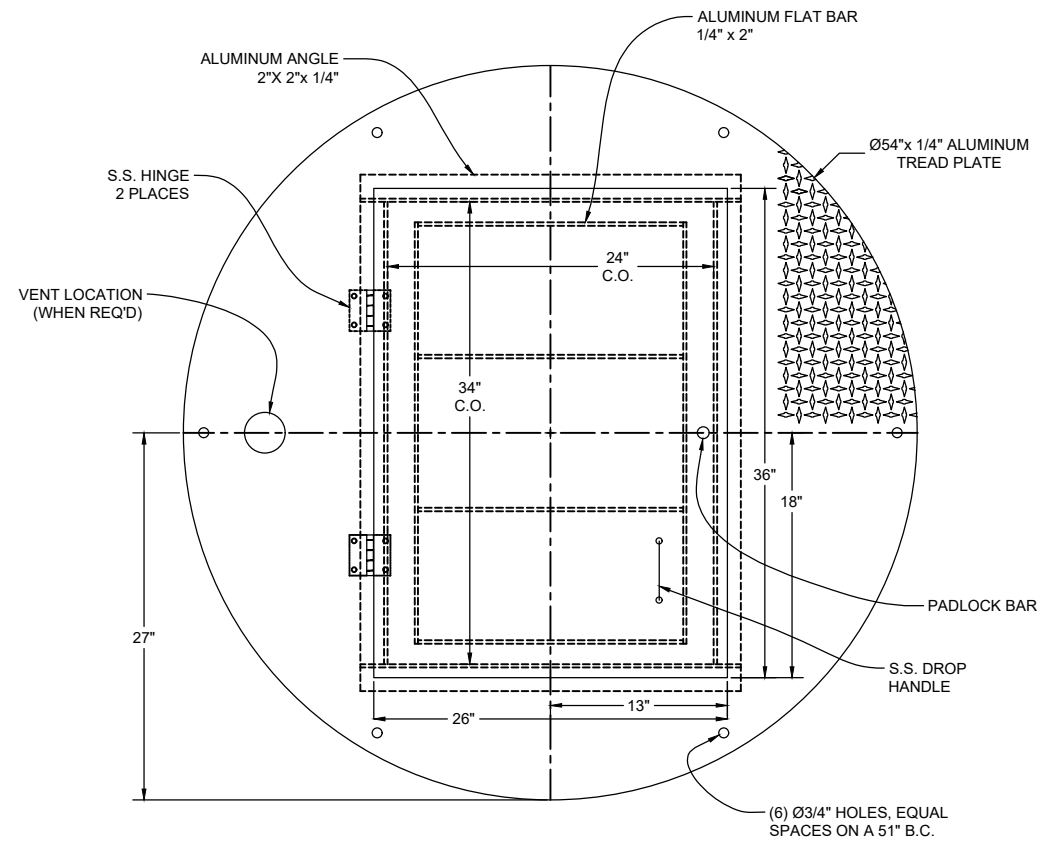
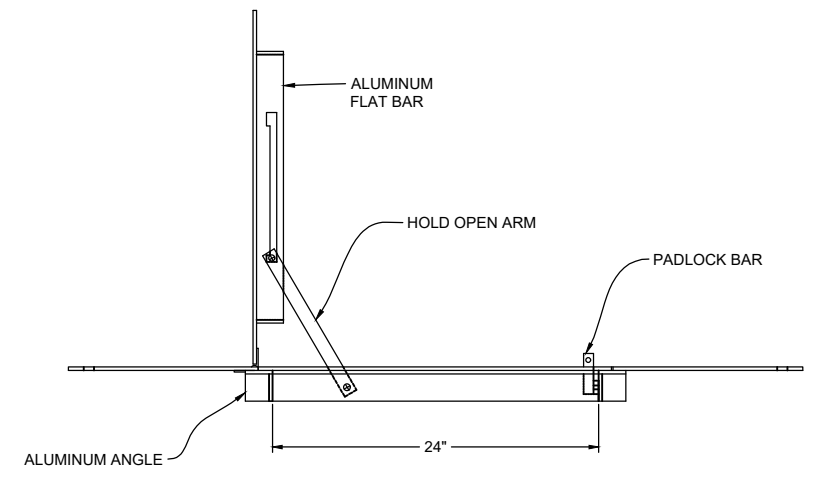
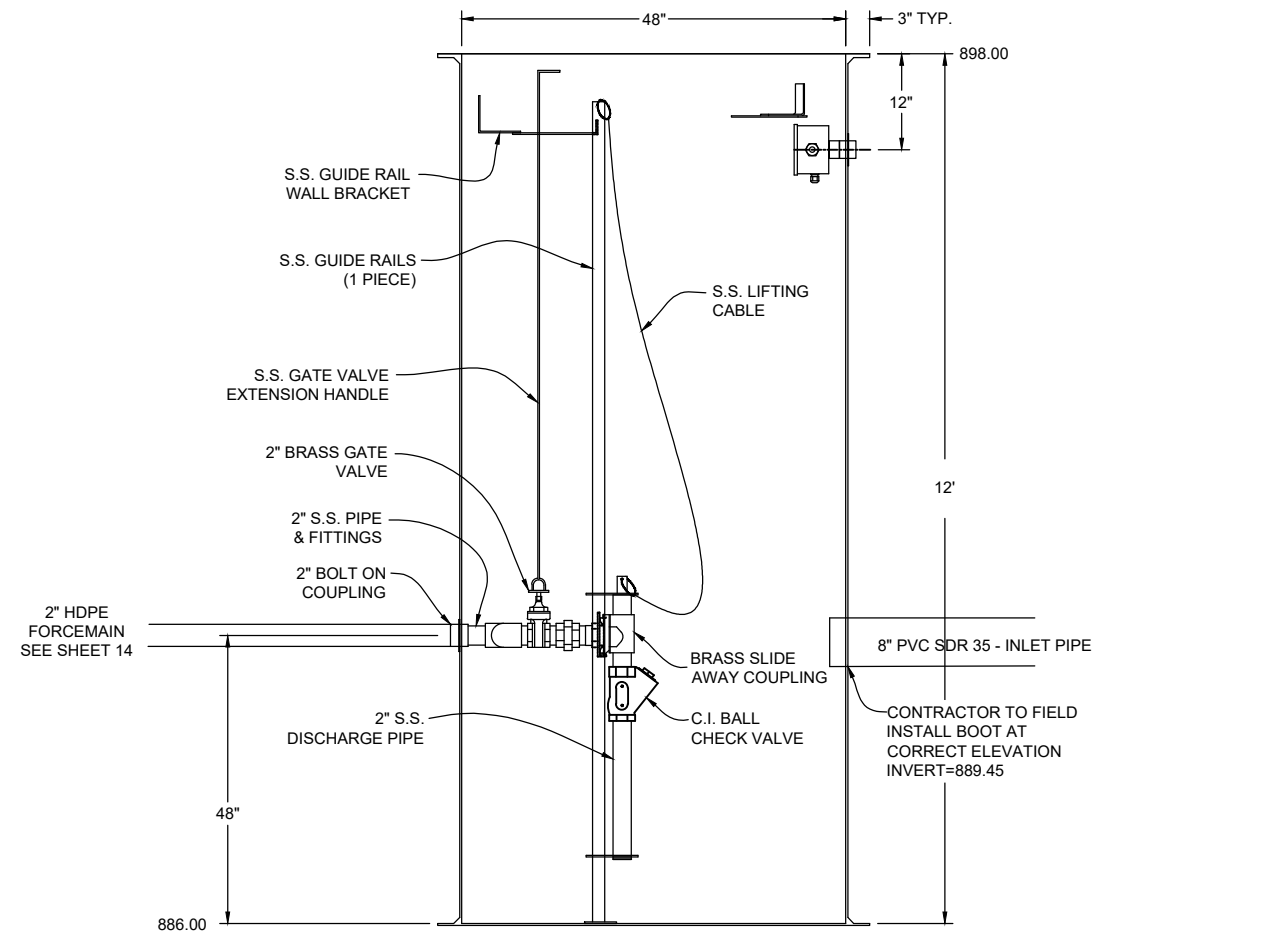
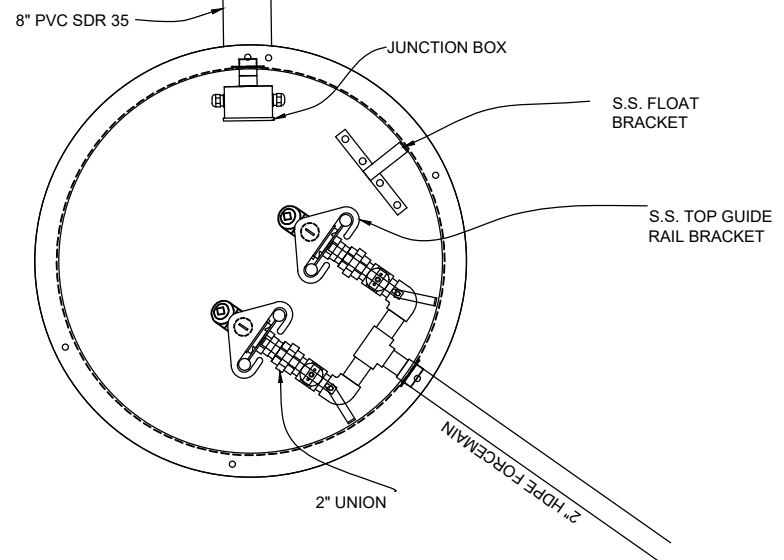


THE VILLAS AT FILLMORE THIRD ADDITION

DETAILS
CITY OF ELK RIVER, MINNESOTA

SHEET 5 OF 21 SHEETS

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-300 PSF LOAD RATING

- GENERAL NOTES:**
- SEE SHEET 10 FOR SANITARY SEWER PLAN
 - OWNER WILL FURNISH THE LIFT STATION AND LIFT STATION COVER. CONTRACTOR SHALL INSTALL LIFT STATION AND LIFT STATION COVER.

SP STEELE PLASTICS, LLC
 1280 Sturgis Road
 Conway, AR 72034
 (501) 327-5122 Fax (501) 327-0807

TITLE	
PROJECT	
JOB NO.	
P.O. NO.	
CUSTOMER	
DWG. NO.	

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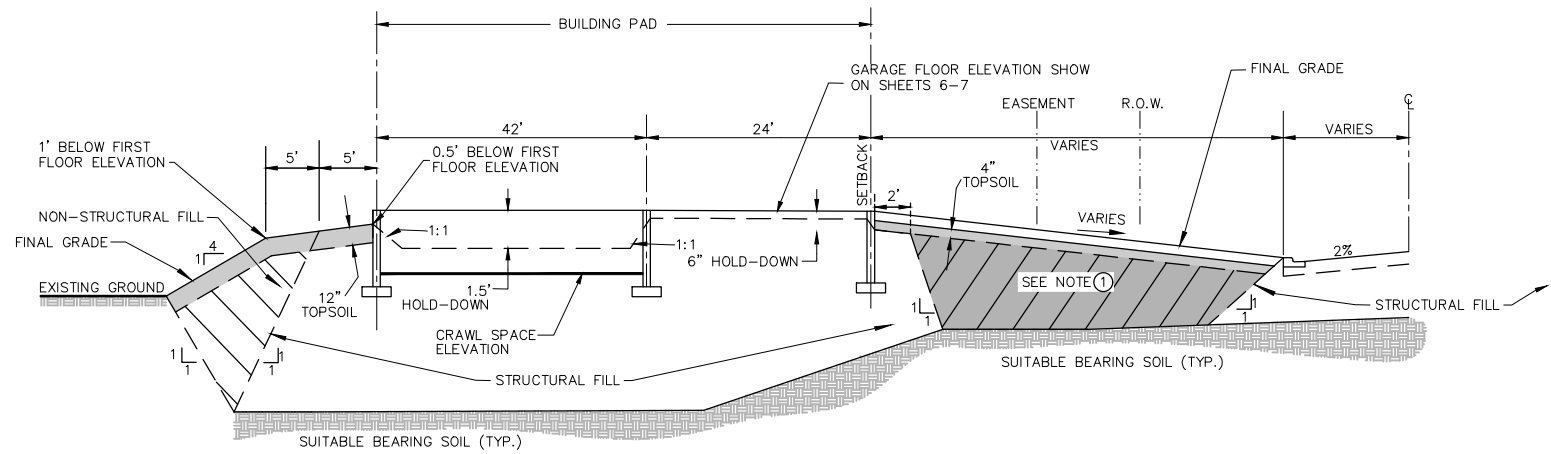
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THE VILLAS AT FILLMORE THIRD ADDITION

DETAILS
 CITY OF ELK RIVER, MINNESOTA

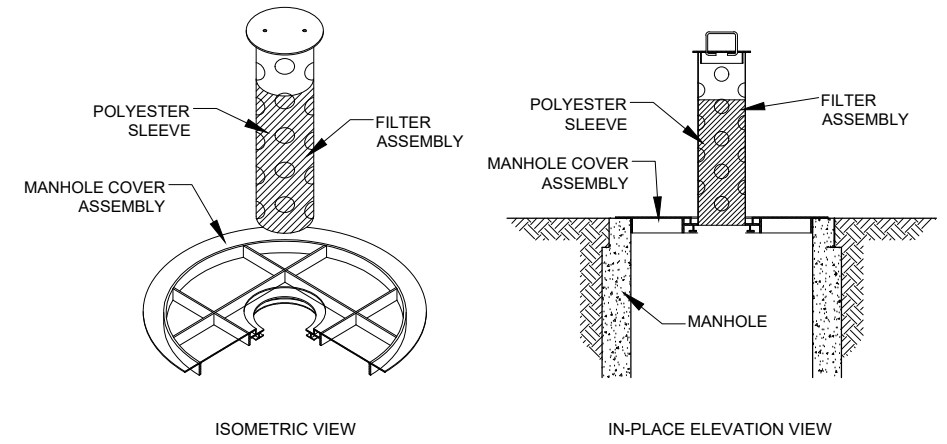
SHEET 6 OF 21 SHEETS



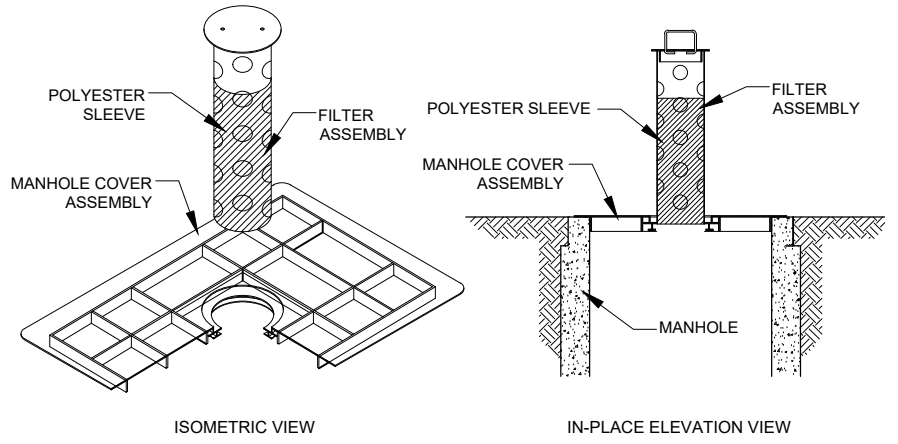
1
7
TYPICAL LOT SECTION

- GENERAL SOIL NOTES:
1. STRIP AND REUSE ALL INPLACE TOPSOIL IN AREAS TO BE DISTURBED BY CONSTRUCTION.
 2. STREET AND LOT FILL SHALL BE CONSTRUCTED WITH STRUCTURAL FILL. ALL STRUCTURAL FILL SHALL BE CONSTRUCTED UNDER THE DIRECTION OF A GEOTECHNICAL ENGINEER. STRUCTURAL FILL FOR THIS PROJECT IS DEFINED IN THE GEOTECHNICAL REPORT. CONTRACTOR SHALL ONLY USE SOIL CLASSIFIED AS SP OR SP-SM IN THE ROAD CORE.
 3. THE FINAL GRADES SHOWN ASSUME A 10 PERCENT SOIL COMPACTION FACTOR ON THE TOTAL FILL. IF THE ACTUAL COMPACTION FACTOR IS GREATER OR LESS THAN 10 PERCENT, THE CONTRACTOR SHALL WORK WITH THE ENGINEER DURING THE GRADING OPERATION TO ADJUST THE FINAL GRADES AS NECESSARY TO BALANCE THE EARTHWORK. ADJUSTMENT TO FUTURE LOT GRADES AND STREETS WILL BE MADE AS NECESSARY.
 4. THE CONTRACTOR SHALL STRIP AND TEMPORARILY STOCKPILE ALL EXISTING TOPSOIL FOR REUSE. SALVAGING TOPSOIL SHALL BE PAID PER ITEM 2106 SALVAGE TOPSOIL. PLACING SALVAGED TOPSOIL OVER THE DISTURBED AREAS AS SHOWN AND PLACING FILL IN OTHER AREAS SHALL BE INCIDENTAL.
 5. SHEET 9 SHOWS THE FINAL GRADING PLAN AFTER THE HOME CONSTRUCTION IS COMPLETE. THESE GRADES REPRESENT THE SURFACE LABELED AS THE FINAL GRADE ON THE TYPICAL LOT SECTION ON SHEET 7. IN GENERAL THIS PROJECT INCLUDES GRADING THE SITE TO THE HOLD-DOWN GRADES THROUGH THE BUILDING PADS AS SHOWN ON THE TYPICAL LOT SECTIONS ON SHEET 7 AND GRADING TO THE FINAL GRADES OUTSIDE THE BUILDING PADS, UNLESS OTHERWISE NOTED, AS SHOWN ON THE TYPICAL LOT SECTIONS ON SHEET 7. NOT ALL THE LOTS FOLLOW THE TYPICAL SECTION.

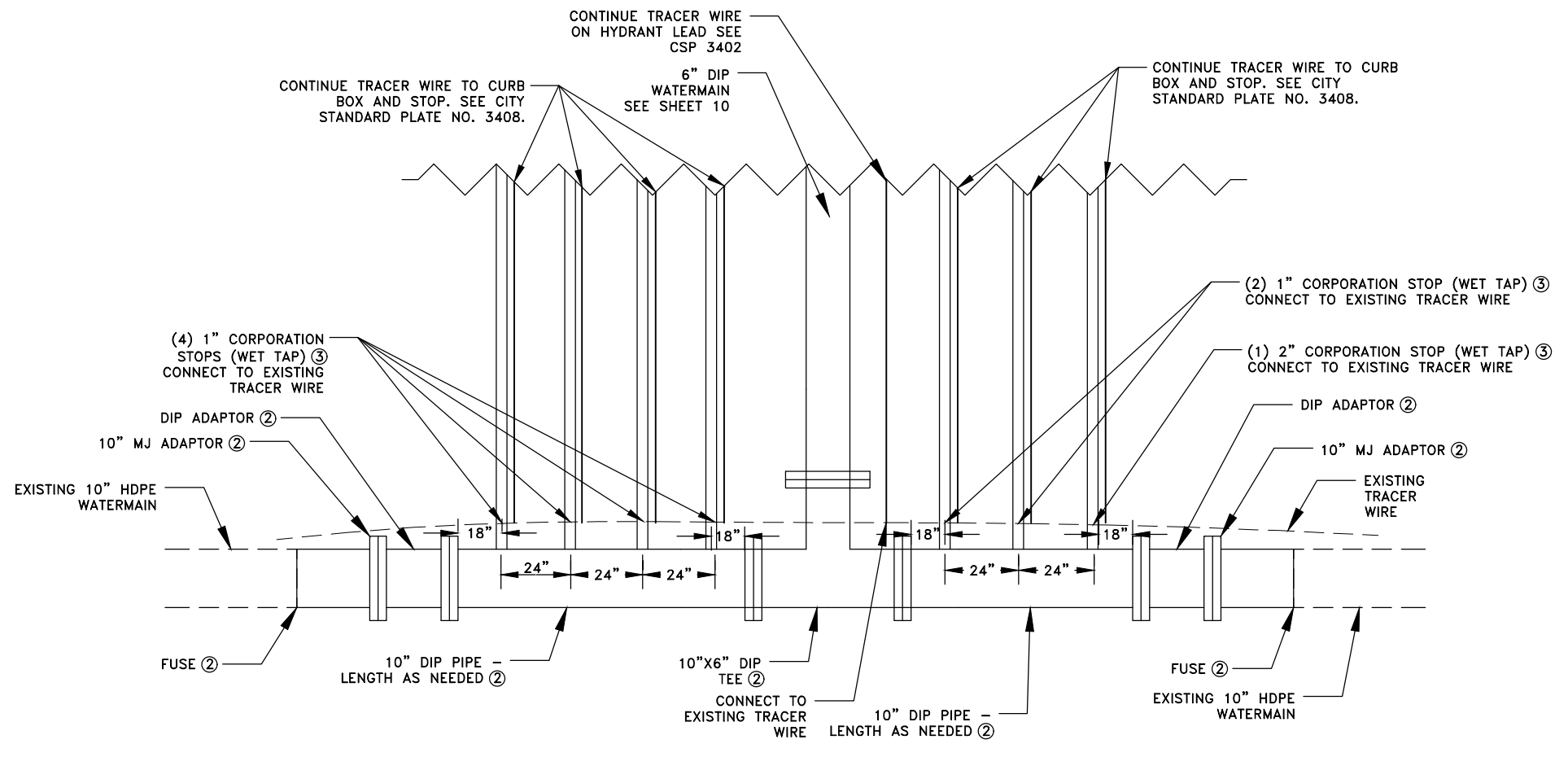
- REFERENCE NOTES:
- 1 ONSITE SILTY/SANDY TOPSOIL WITH LESS THAN 5% ORGANICS CAN BE USED AS FILL IN THESE AREAS.
 - 2 ALL WORK AND MATERIAL SHALL BE INCIDENTAL TO ITEM 2504 CONNECT TO EXISTING WATERMAIN. ITEM 2504 CONNECT TO EXISTING WATERMAIN WILL BE MEASURED AS (1) ONE UNIT PER CONNECTION LOCATION.
 - 3 SERVICE DETAIL SEE CITY STANDARD PLATE 3403. NO SPLICES ARE ALLOWED IN THE SERVICE PIPE FROM THE CORPORATION STOP TO THE CURB BOX AND STOP.



2
7
STORM DRAIN INLET PROTECTION



3
7
STORM DRAIN INLET PROTECTION TYPE 1
PRE-CURB



4
7
WATERMAIN CONNECTION

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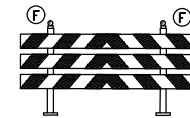


THE VILLAS AT FILLMORE THIRD ADDITION

TYPICAL LOT SECTIONS, SOIL NOTES, AND DETAILS
 CITY OF ELK RIVER, MINNESOTA

SHEET 7 OF 21 SHEETS

LEGEND



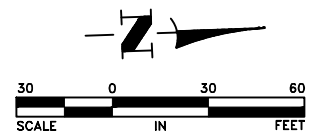
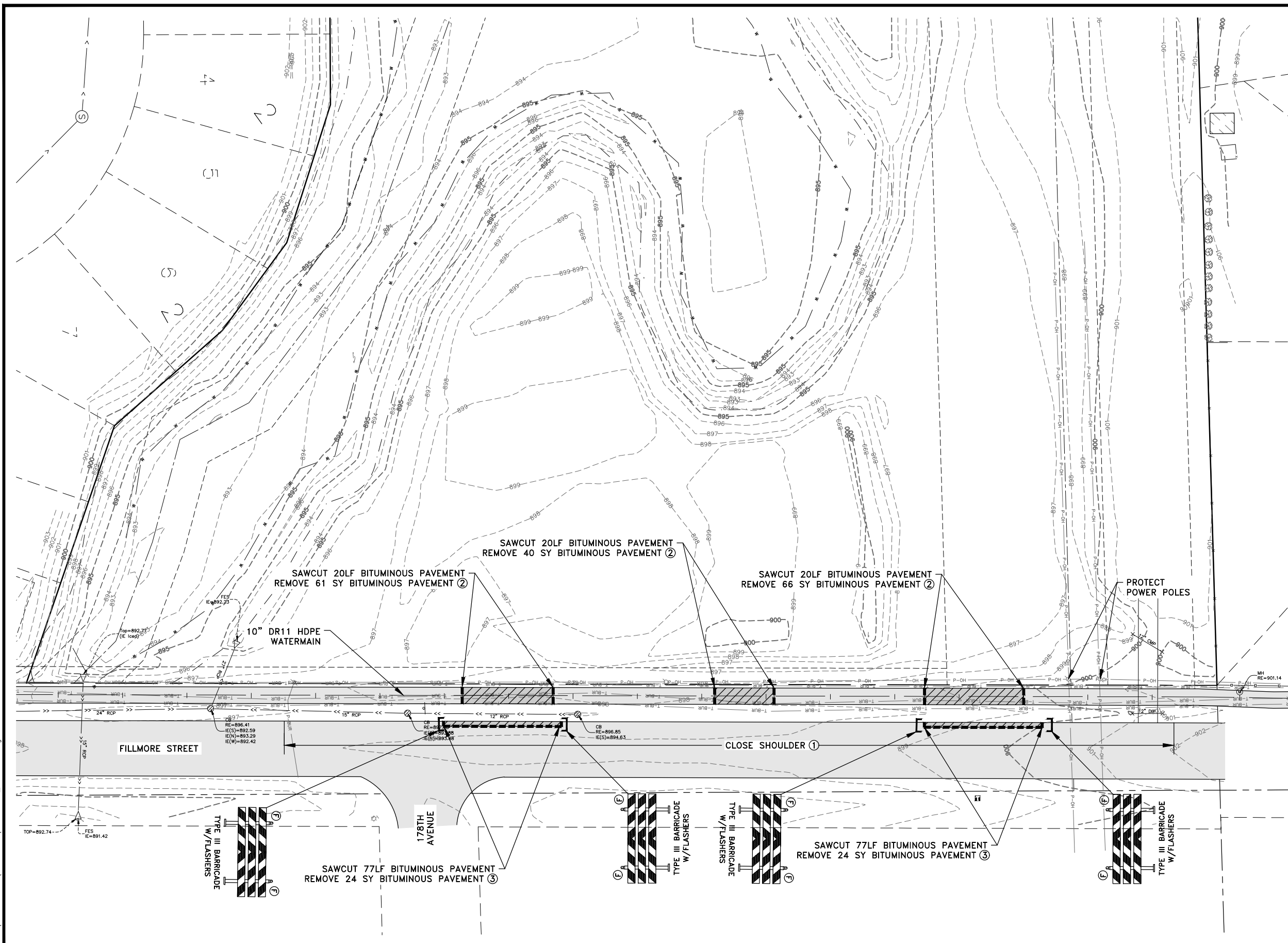
TYPE III BARRICADE
W/FLASHERS

GENERAL NOTES:

- CONTRACTOR SHALL PROVIDE TRAFFIC CONTROL INCLUDING FLAGGERS WHEN NEEDED TO MAINTAIN TRAFFIC ON FILLMORE STREET AT ALL TIMES.

REFERENCE NOTES:

- CONTRACTOR SHALL CLOSE THE SHOULDER PER LAYOUT 8 OF THE MMUTCD.
- CONTRACTOR SHALL PROVIDE TRAIL CLOSED SIGNS WHEN ACTIVELY WORKING ON THE TRAIL. AT ALL OTHER TIMES THE TRAIL SHALL REMAIN OPEN. CONTRACTOR SHALL PROVIDE A TEMPORARY CLASS 5 AGGREGATE SURFACE ON THE TRAIL AFTER REMOVAL OF THE BITUMINOUS.
- CONTRACTOR SHALL FILL REMOVAL AREA WITH CLASS 5 AGGREGATE BASE AFTER REMOVAL OF BITUMINOUS.



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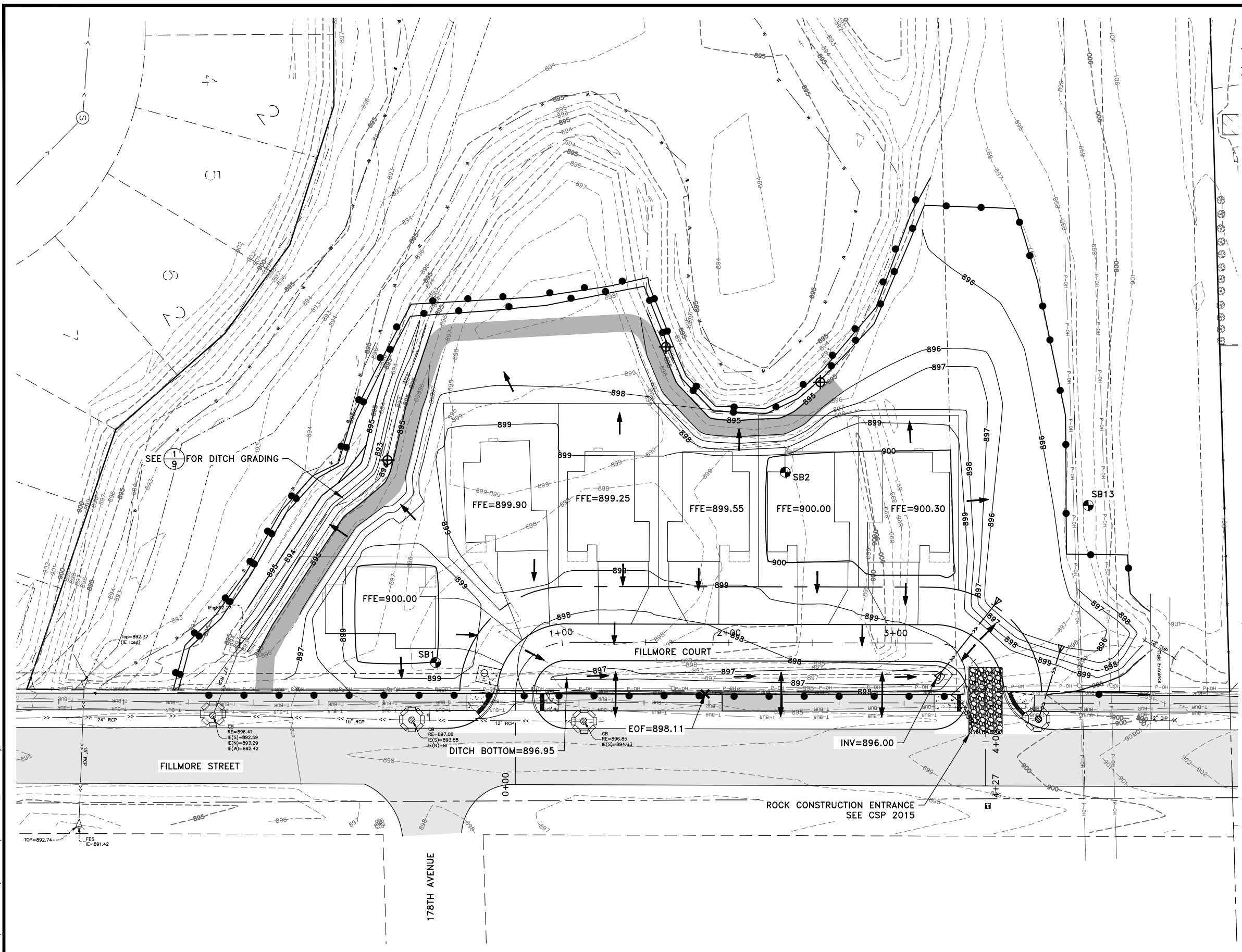
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THE VILLAS AT FILLMORE THIRD ADDITION

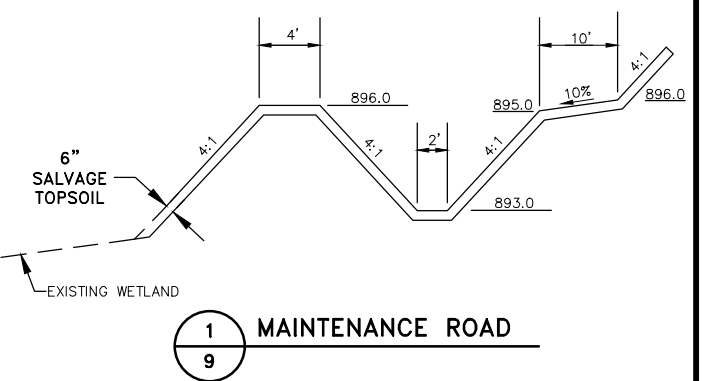
EXISTING CONDITIONS, REMOVALS, AND TRAFFIC CONTROL PLAN
CITY OF ELK RIVER, MINNESOTA

SHEET 8 OF 21 SHEETS



- LEGEND**
- SILT FENCE SEE CSP 2009
 - DOUBLE SILT FENCE SEE CSP 2009
 - INLET PROTECTION SEE CSP 2511, (2/7), AND (3/7)
 - EOF=902.0 EMERGENCY OVERTFLOW
 - X 902.0 SPOT ELEVATION
 - DRAINAGE DIRECTION ARROW
 - FFE=900.00 PROPOSED FINISHED FLOOR ELEVATION
 - WETLAND BUFFER SIGN (3 TOTAL) (1)
 - CONSTRUCT 10' WIDE MAINTENANCE ROAD SEE (1/9)

- GENERAL NOTES:**
1. SEE SHEET 2 FOR INDEX OF CITY STANDARD PLATES (CSP).
 2. SEE SHEET 7 FOR SOIL NOTES AND LOT SECTIONS.
- REFERENCE NOTES:**
- (1) WETLAND BUFFER SIGN AND INSTALLATION KIT SHALL BE PURCHASED FROM THE CITY OF ELK RIVER. THE CURRENT COST IS \$15 PER SIGN.



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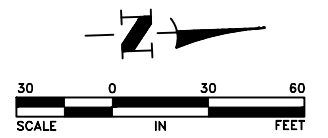
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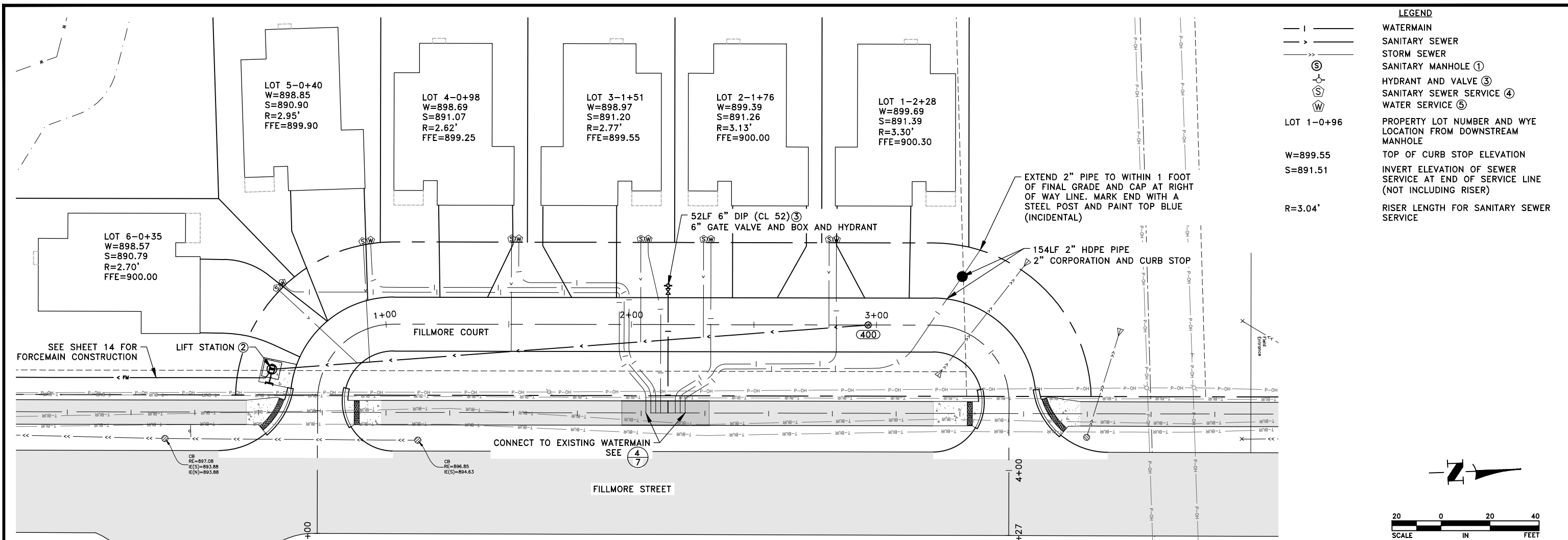


THE VILLAS AT FILLMORE THIRD ADDITION

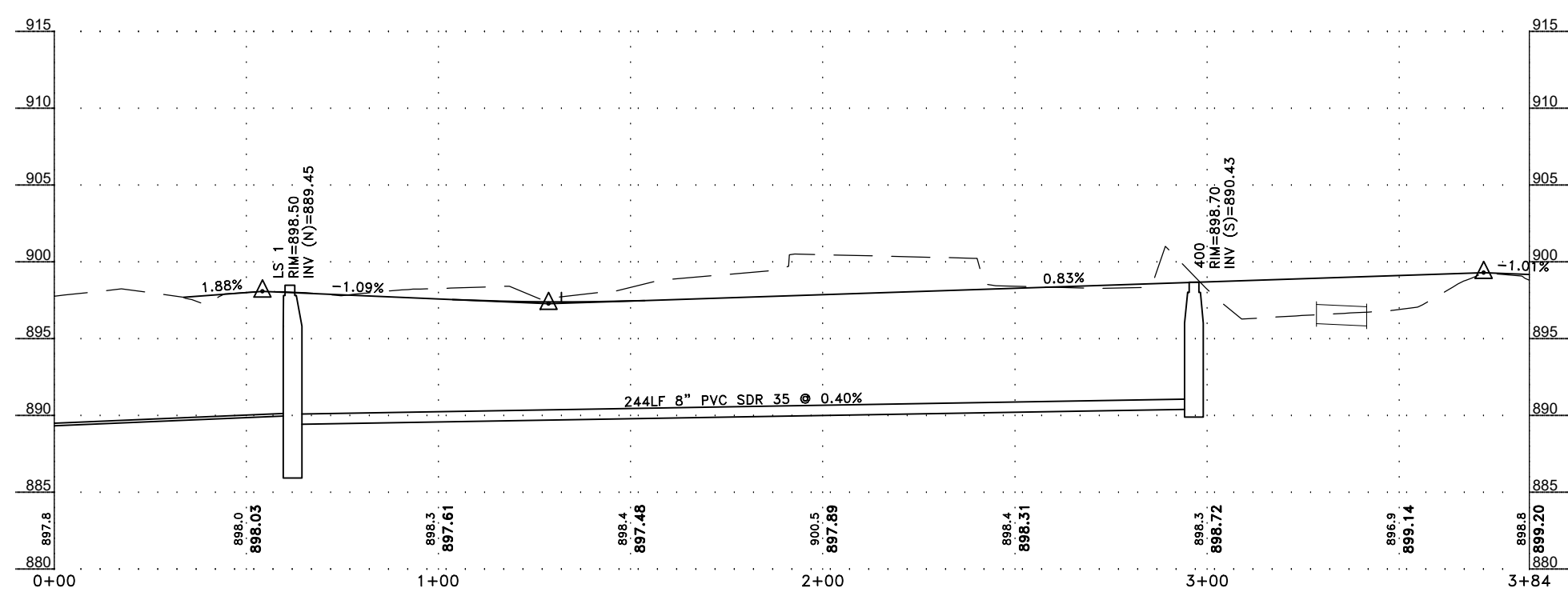
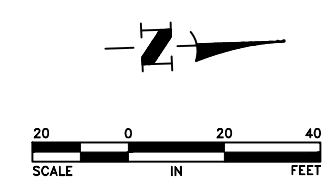
GRADING AND EROSION CONTROL PLAN
 CITY OF ELK RIVER, MINNESOTA

SHEET 9 OF 21 SHEETS





- LEGEND**
- | — WATERMAIN
 - > — SANITARY SEWER
 - >> — STORM SEWER
 - ⊙ SANITARY MANHOLE ①
 - ⊕ HYDRANT AND VALVE ③
 - ⊙ SANITARY SEWER SERVICE ④
 - ⊙ WATER SERVICE ⑤
- LOT 1-0+96 PROPERTY LOT NUMBER AND WYE LOCATION FROM DOWNSTREAM MANHOLE
- W=899.55 TOP OF CURB STOP ELEVATION
- S=891.51 INVERT ELEVATION OF SEWER SERVICE AT END OF SERVICE LINE (NOT INCLUDING RISER)
- R=3.04' RISER LENGTH FOR SANITARY SEWER SERVICE



- GENERAL NOTES:**
1. WATERMAIN AND SANITARY SEWER SHALL BE BEDDED PER CSP 3406.
 2. SEE SHEET 2 FOR INDEX OF CITY STANDARD PLATES (CSP).
 3. ALL WATERMAIN TRUNK, WATER SERVICES, AND HYDRANTS WILL REQUIRE TRACER WIRE. SEE CSP 3402, 3408, AND 3409 FOR ADDITIONAL INFORMATION.
- REFERENCE NOTES:**
- ① CONSTRUCT MANHOLE PER CSP 4000.
 - ② SEE SHEET 6 AND 15 FOR LIFT STATION DETAILS AND LAYOUTS.
 - ③ CONSTRUCT HYDRANT PER CSP 3401 AND 3402.
 - ④ CONSTRUCT SANITARY SEWER SERVICE PER CSP 4007.
 - ⑤ CONSTRUCT WATER SERVICE PER CSP 3403.

Apr 28, 2025 - 8:51am K:\PRIVATE\21-35-36\ENGINEERING\PLAN DWG\21-35-36_WAT AND SAN.dwg

DATE	REVISION

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Craig J. Jochem
CRAIG J. JOCHUM, P.E.
 Lic. No. 23461

Date 4/23/25

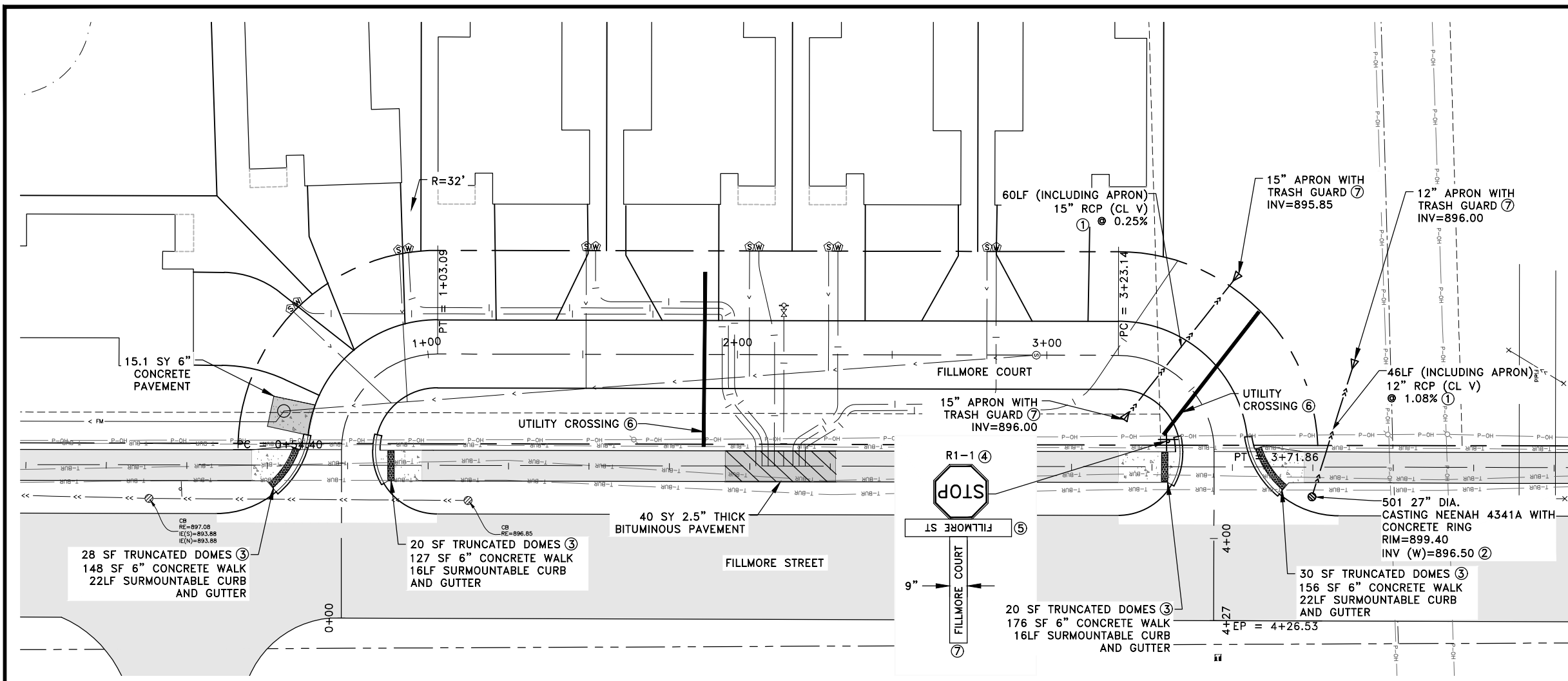
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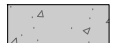
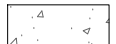

THE VILLAS AT FILLMORE THIRD ADDITION

WATERMAIN AND SANITARY SEWER PLAN
 FILLMORE COURT
 CITY OF ELK RIVER, MINNESOTA

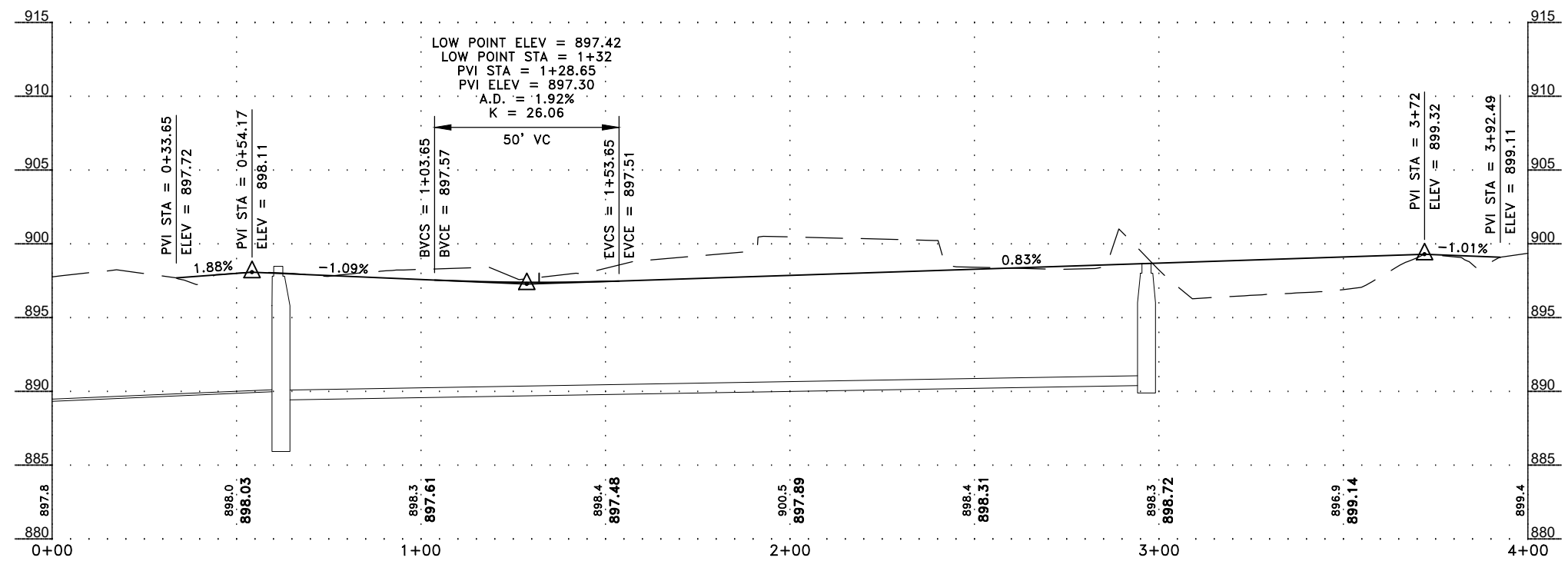
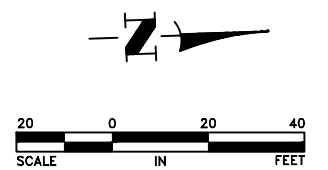
SHEET 10 OF 21 SHEETS



LEGEND

-  BITUMINOUS TRAIL PATCH SEE ①/11
-  6" CONCRETE PAVEMENT SEE ②/11
-  6" CONCRETE WALK SEE ③/11
-  EXISTING BITUMINOUS PAVEMENT

- GENERAL NOTES:**
1. SEE ①/3 AND CSP 2000 FOR STREET CONSTRUCTION.
 2. SEE SHEET 2 FOR INDEX OF CITY STANDARD PLATES (CSP).
REFERENCE NOTES:
① ALL PIPE JOINTS SHALL BE TIED PER CSP 2503.
② CONSTRUCT STRUCTURE PER MNDOT STANDARD PLATE 4006L DESIGN H.
③ SHEETS 13 AND 16-21 FOR PEDESTRIAN CURB RAMP DETAILS.
④ VERIFY SIGN LOCATION WITH CITY. PAID PER ITEM 2564 SIGN TYPE C.
⑤ VERIFY SIGN LOCATION AND LAYOUT WITH CITY PRIOR TO FABRICATING SIGN. PAID AS ITEM 2564 TYPE D.
⑥ SEE NOTE 6 ON SHEET 2.
⑦ SEE CSP 2504 FOR TRASH GUARD CONSTRUCTION. A CONCRETE MAT SHALL BE CONSTRUCTED WITH THE APRON. MAT SHALL BE 12' BY 14' (15" RCP) AND 10' BY 12' (12" RCP). SEE CSP 2513 FOR CONSTRUCTION DETAILS AND NOTE 15 ON SHEET 4.



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DATE	REVISION

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CRAIG J. JOCHUM, P.E.
Lic. No. 23461

Date 4/23/25

DESIGNED BY: CJJ
DRAWN BY: SGJ
CHECKED BY: TAE

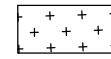


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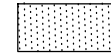
STREET AND STORM SEWER PLAN
FILLMORE COURT
CITY OF ELK RIVER, MINNESOTA

SHEET 11 OF 21 SHEETS

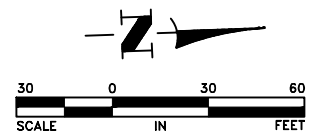
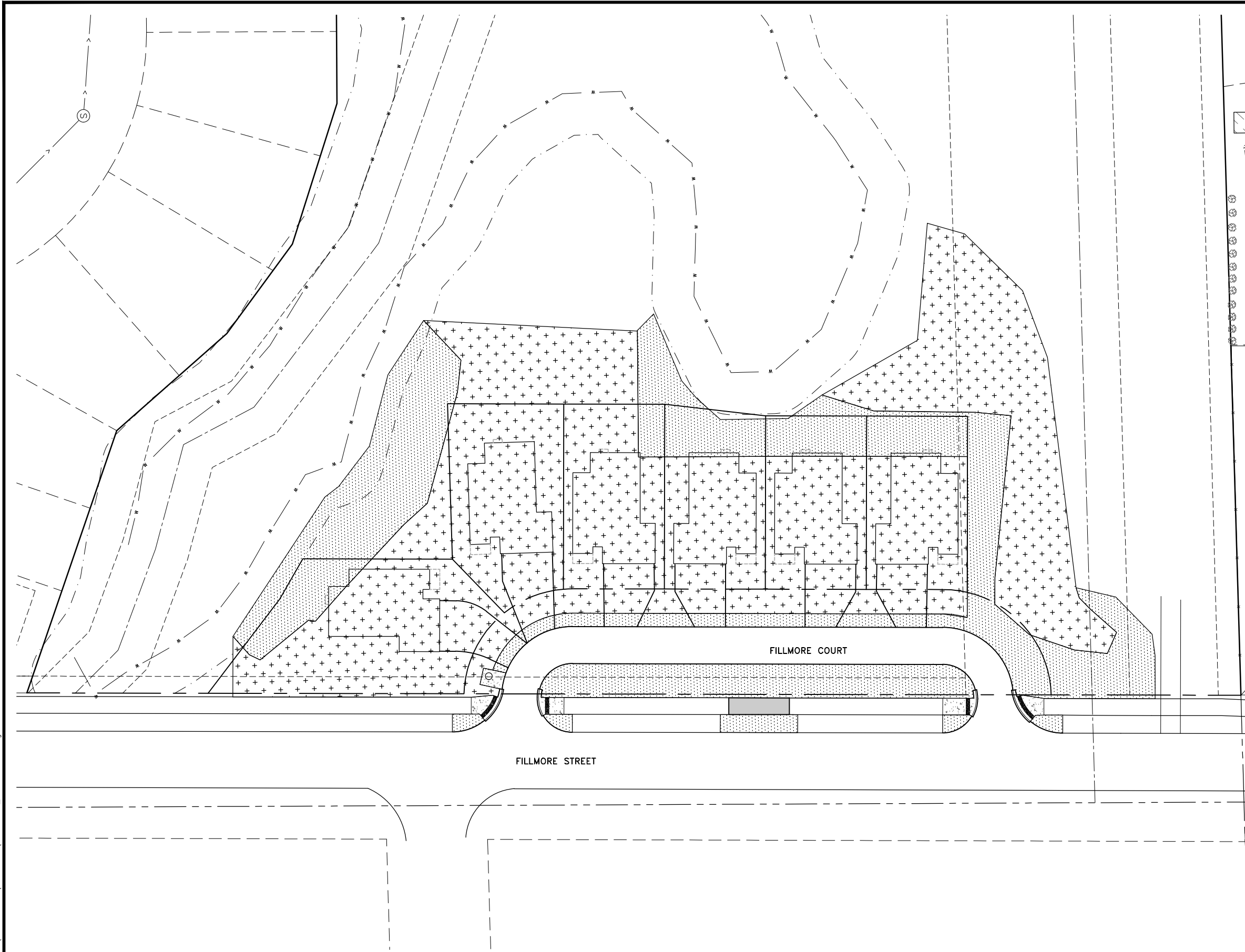
LEGEND



SEED SOUTHERN BOULEVARD (160 POUNDS/ACRE)
 TYPE 1 FERTILIZER (300 POUNDS/ACRE)
 MULCH TYPE 1 - DISK ANCHOR (2 TONS/ACRE)



SEED SOUTHERN BOULEVARD (160 POUNDS/ACRE)
 TYPE 1 FERTILIZER (300 POUNDS/ACRE)
 ROLLED EROSION PREVENTION CATEGORY 25



Apr 28, 2025 - 8:51am
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DATE	REVISION

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

[Signature]
CRAM J. JOCHUM, P.E.
 Date 4/23/25 Lic. No. 23461

DESIGNED BY:
CJJ

DRAWN BY:
SGJ

CHECKED BY:
TAE

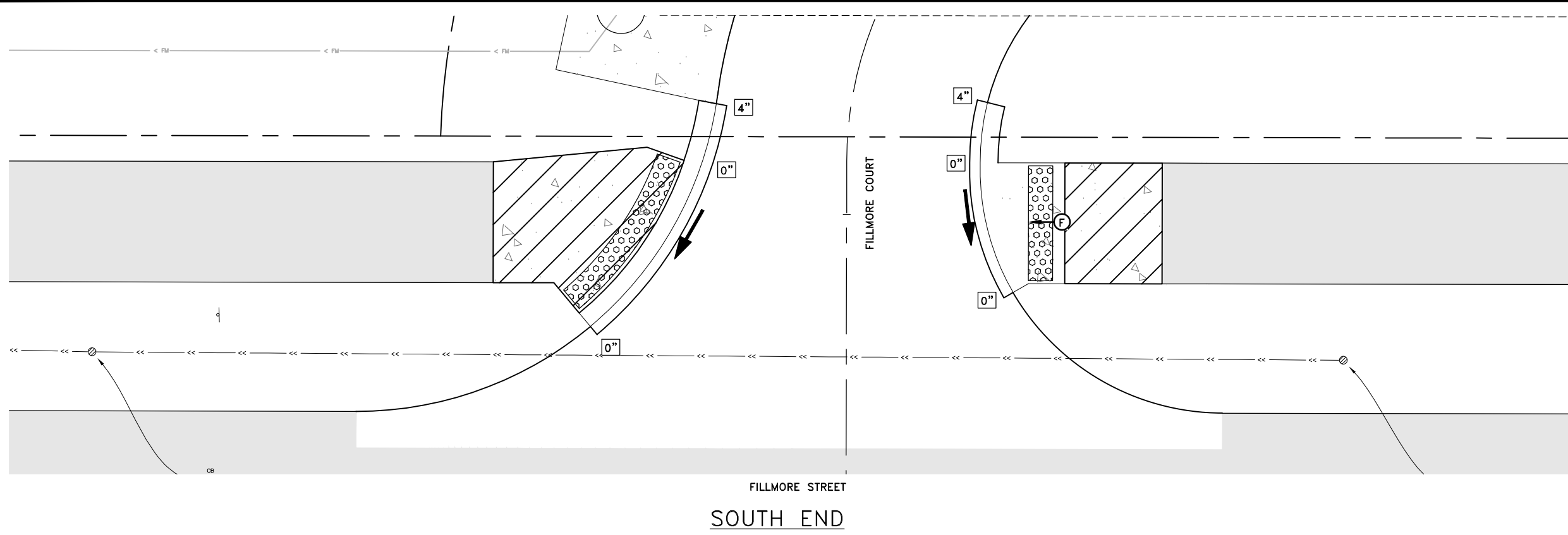


THE VILLAS AT FILLMORE THIRD ADDITION

RESTORATION PLAN
 CITY OF ELK RIVER, MINNESOTA

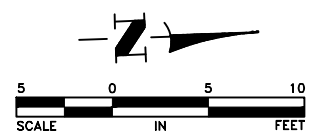
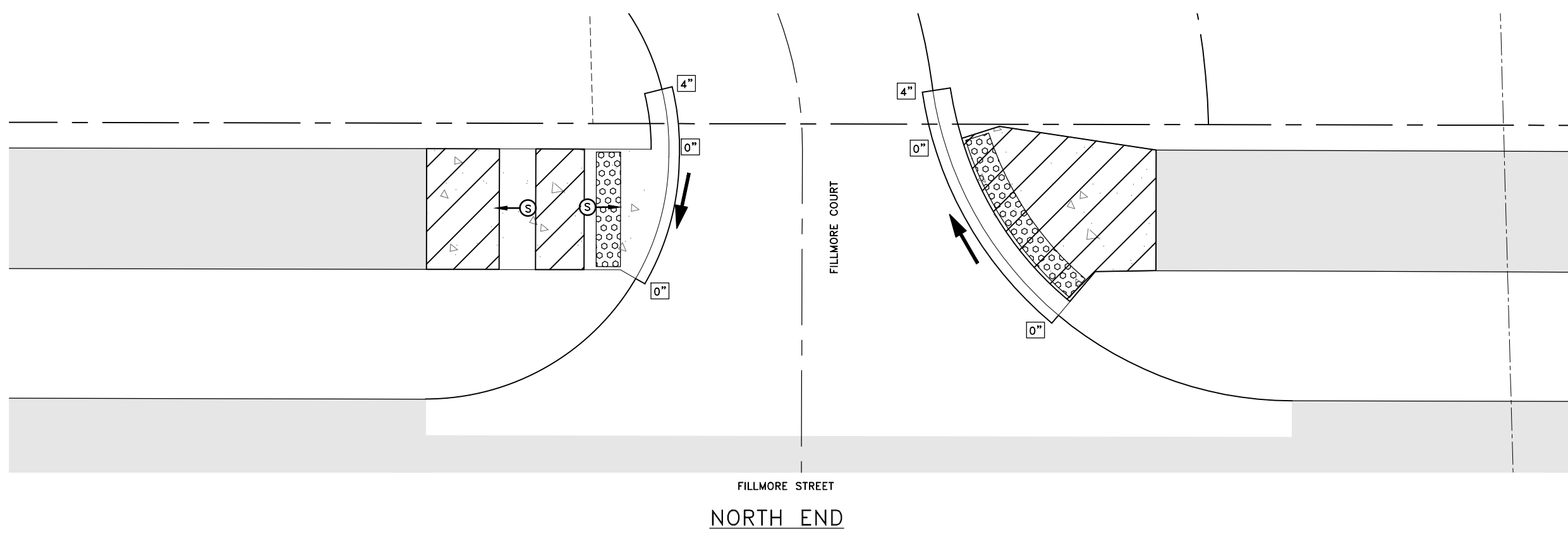
SHEET 12 OF 21 SHEETS

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- LEGEND**
- LANDING AREA - 4'x4' MIN. DIMENSIONS. MAX. 2.0% CROSS SLOPE IN ALL DIRECTIONS.
 - INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MIN. AND 8.3% MAX. IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
 - INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
 - CURB HEIGHT
 - DRAINAGE AREA

GENERAL NOTES:
1. SEE SHEETS 16-21 FOR PEDESTRIAN CURB RAMP DETAILS.



DATE	REVISION

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Craig J. Jochum
CRAG J. JOCHUM, P.E.
 Date 4/23/25 Lic. No. 23461

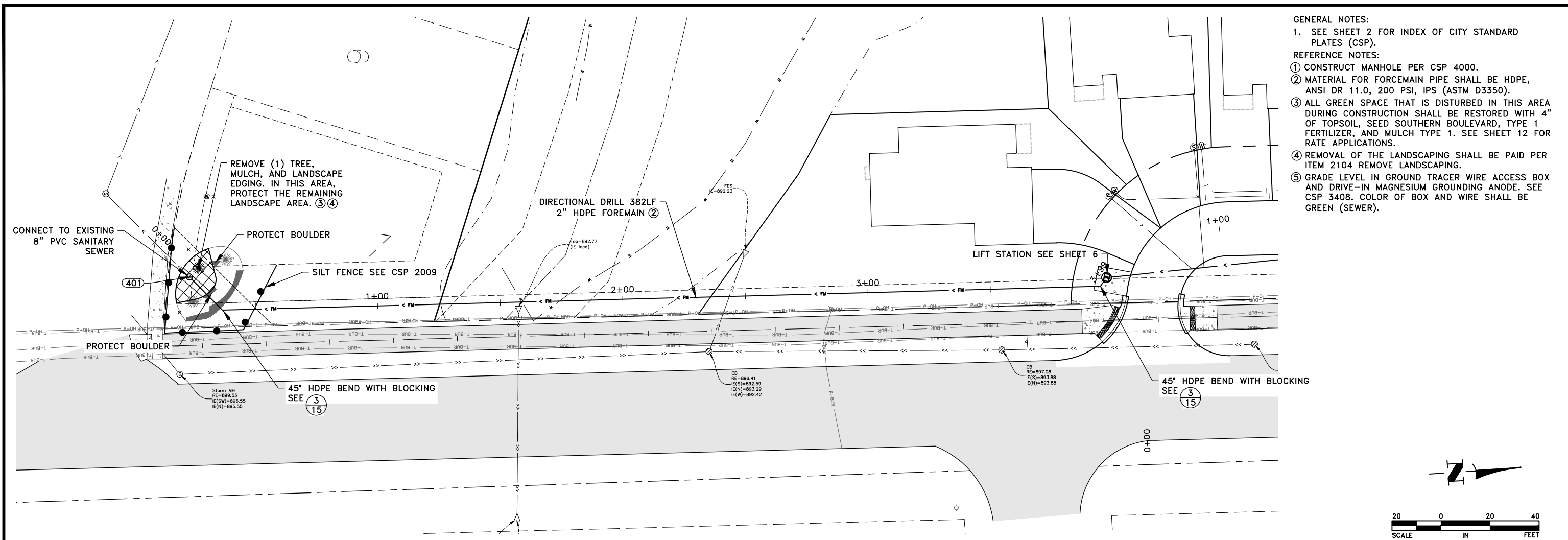
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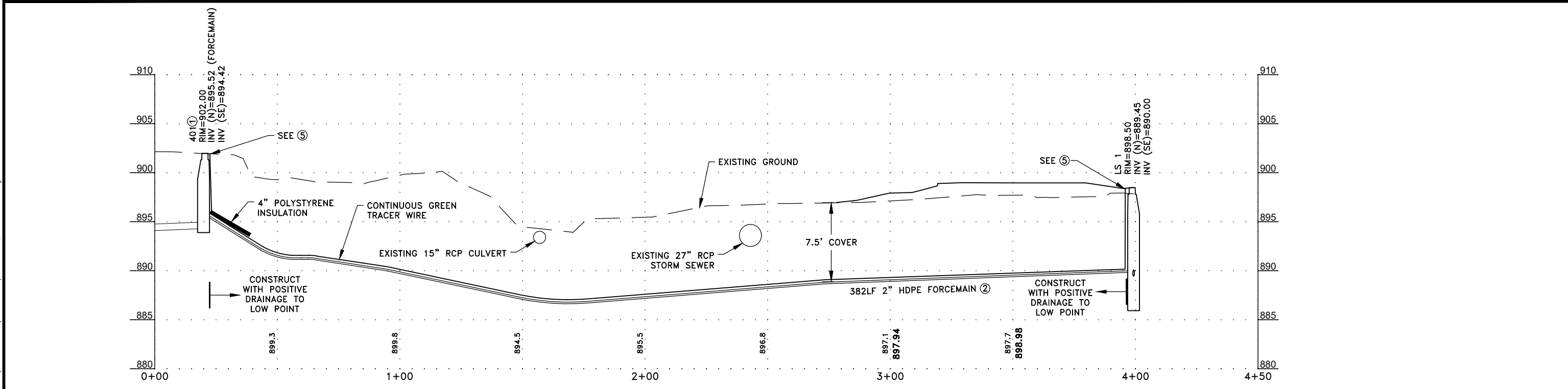
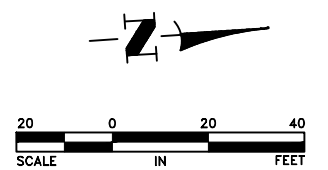
THE VILLAS AT FILLMORE THIRD ADDITION

PEDESTRIAN RAMP CONSTRUCTION PLAN
 CITY OF ELK RIVER, MINNESOTA

SHEET 13 OF 21 SHEETS



- GENERAL NOTES:
- SEE SHEET 2 FOR INDEX OF CITY STANDARD PLATES (CSP).
- REFERENCE NOTES:
- CONSTRUCT MANHOLE PER CSP 4000.
 - MATERIAL FOR FORCEMAIN PIPE SHALL BE HDPE, ANSI DR 11.0, 200 PSI, IPS (ASTM D3350).
 - ALL GREEN SPACE THAT IS DISTURBED IN THIS AREA DURING CONSTRUCTION SHALL BE RESTORED WITH 4" OF TOPSOIL, SEED SOUTHERN BOULEVARD, TYPE 1 FERTILIZER, AND MULCH TYPE 1. SEE SHEET 12 FOR RATE APPLICATIONS.
 - REMOVAL OF THE LANDSCAPING SHALL BE PAID PER ITEM 2104 REMOVE LANDSCAPING.
 - GRADE LEVEL IN GROUND TRACER WIRE ACCESS BOX AND DRIVE-IN MAGNESIUM GROUNDING ANODE. SEE CSP 3408. COLOR OF BOX AND WIRE SHALL BE GREEN (SEWER).



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CRAIG J. JOCHUM, P.E.
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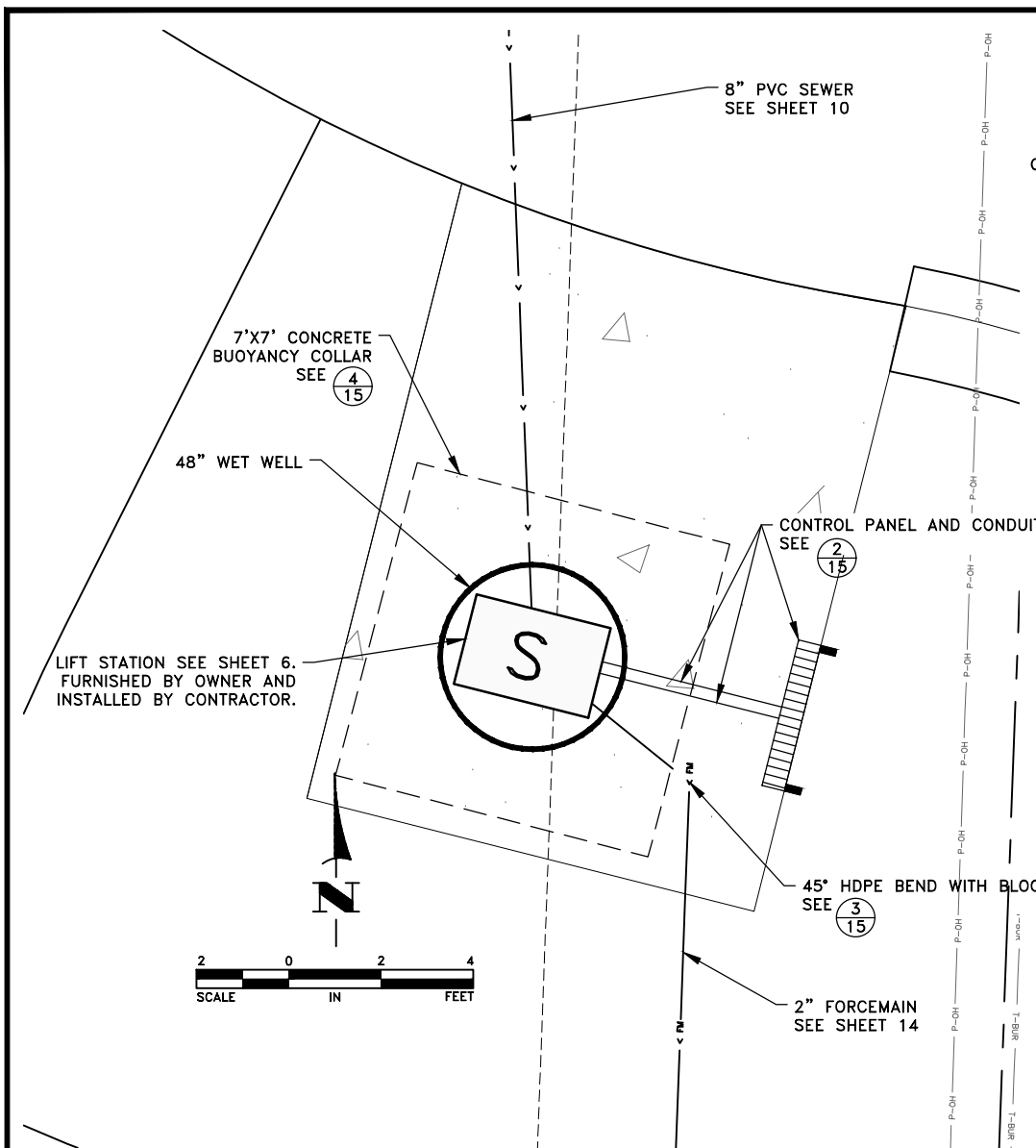
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 505 First Street, Princeton, MN 55371
 0-763.389.4476 RRLSE.com

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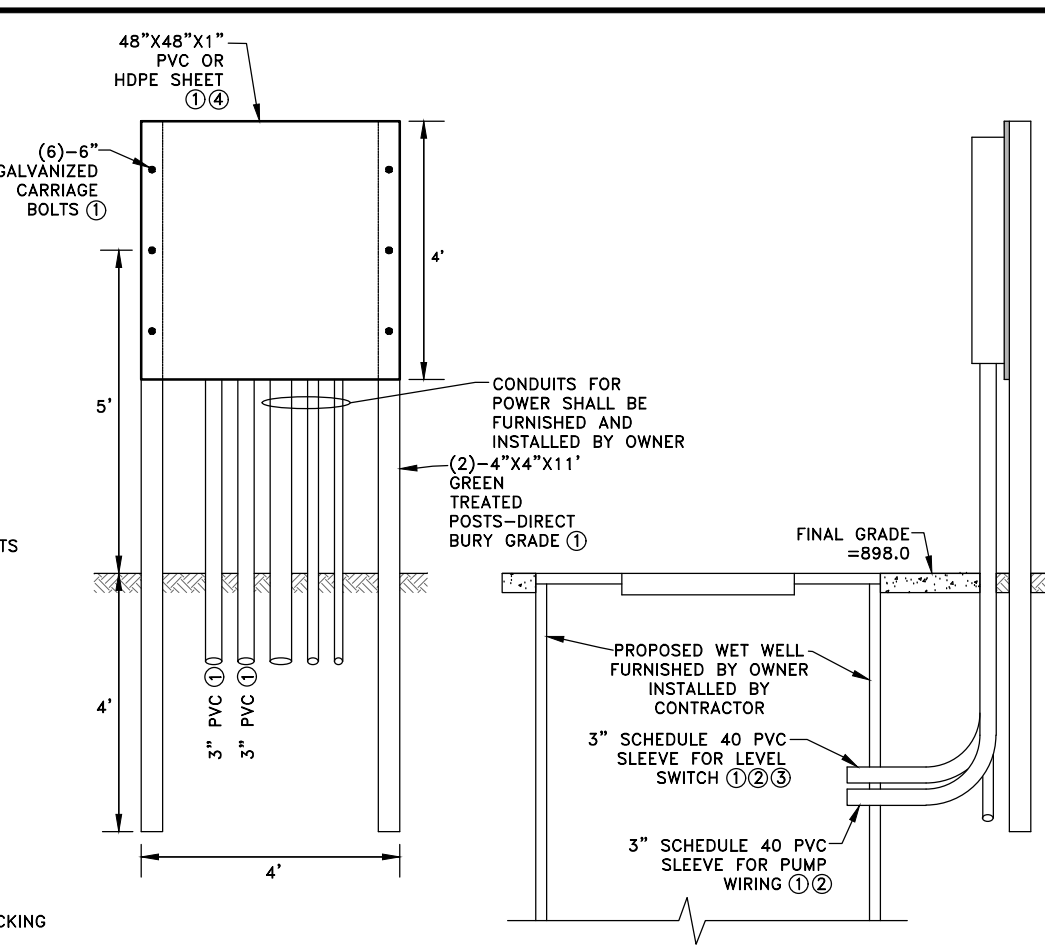
FORCEMAIN CONSTRUCTION PLAN
 CITY OF ELK RIVER, MINNESOTA

SHEET 14 OF 21 SHEETS

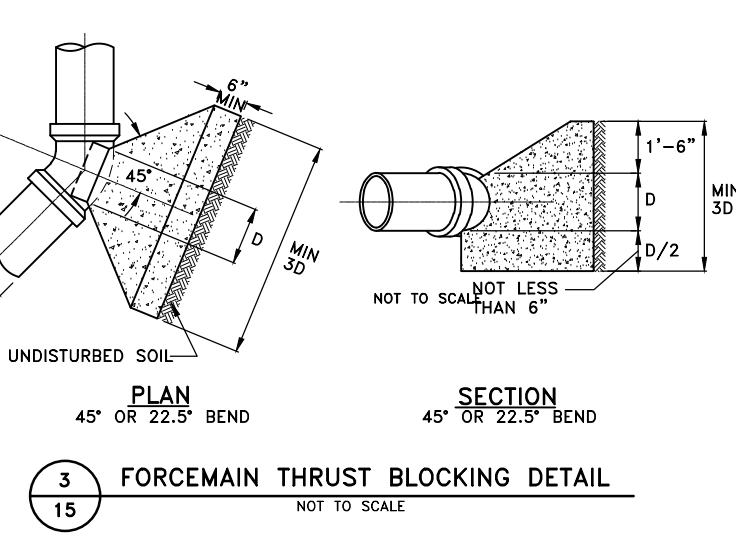
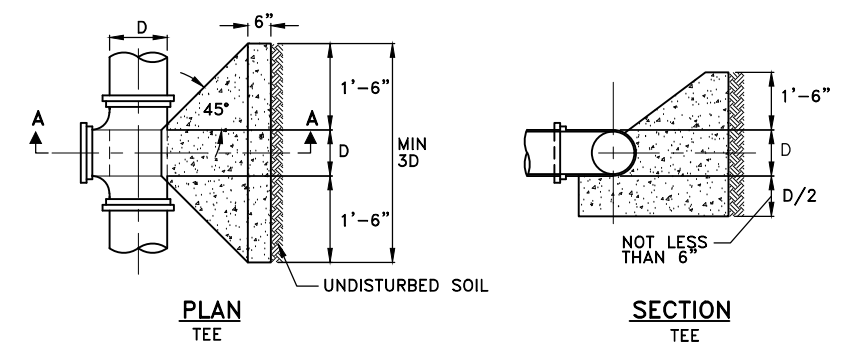
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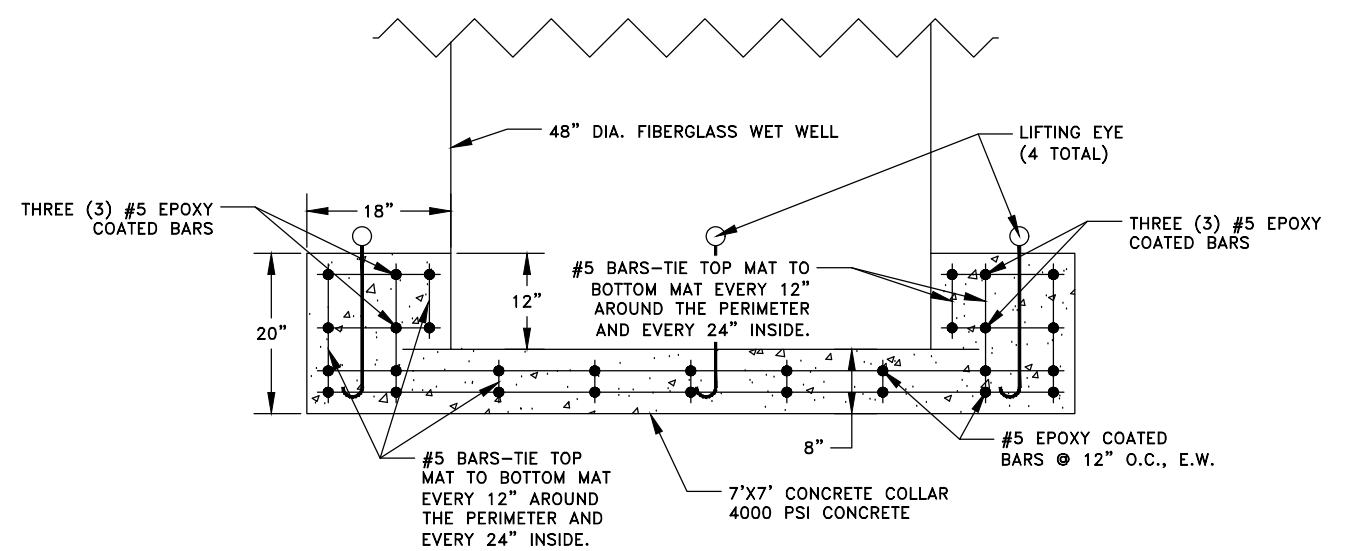
1
15 LIFT STATION LAYOUT



2
15 LIFT STATION CONTROL PANEL CONDUIT AND MOUNTING BOARD DETAIL



3
15 FORCEMAIN THRUST BLOCKING DETAIL
NOT TO SCALE



4
15 BUOYANCY COLLAR

- GENERAL NOTES:**
- SEE SHEET 2 FOR INDEX OF CITY STANDARD PLATES (CSP).
 - CONTRACTOR SHALL COORDINATE ALL WORK WITH OWNERS ELECTRICIAN.
 - ALL MATERIAL AND WORK REQUIRED FOR THE CONSTRUCTION OF THE OWNER PROVIDED LIFT STATION SHALL BE INCLUDED IN ITEM 2503 INSTALL LIFT STATION.
 - THE CONTRACTOR SHALL INSTALL THE TWO RUBBER BOOTS FOR THE 3 INCH CONDUITS AND THE RUBBER BOOT FOR THE 8 INCH PVC SEWER PIPE. THE BOOTS WILL BE PROVIDED BY THE OWNER.
 - THE CONTRACTOR SHALL PROVIDE ALL LABOR, EQUIPMENT, AND MATERIAL TO CONSTRUCT THE BUOYANCY COLLAR. THIS WORK SHALL BE INCIDENTAL TO ITEM 2506 INSTALL LIFT STATION.
- REFERENCE NOTES:**
- FURNISH AND INSTALLED BY CONTRACTOR.
 - CONTRACTOR SHALL RUN THE PUMP WIRES AND THE LEVEL SWITCH WIRES FROM THE LIFT STATION TO THE CONTROL PANEL. THE WIRE AND LEVEL SWITCHES WILL BE PROVIDED BY THE OWNER. THE OWNER WILL FURNISH AND INSTALL THE ABOVE GROUND CONDUIT AT THE CONTROL PANEL AND RUN THE WIRES INTO THE CONTROL PANEL. AFTER COMPLETION OF ALL WORK THE CONTRACTOR SHALL SET THE LEVEL SWITCHES AND ASSIST THE OWNER WITH THE LIFT STATION START UP. THE CONTRACTOR SHALL PROVIDE WATER FOR THE LIFT STATION START UP AND TESTING.
 - CONTRACTOR SHALL INSTALL FOUR FLOATS TO BE USED FOR LEVEL CONTROL. OWNER SHALL PROVIDE FLOATS.
 - PVC SHEET SHALL BE CERTAINTED SAINT GOBAIN OR APPROVED EQUAL. HDPE SHEET SHALL BE BY ePLASTICS OR APPROVED EQUAL.

DATE	REVISION

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 Date 4/23/25 Lic. No. 23461

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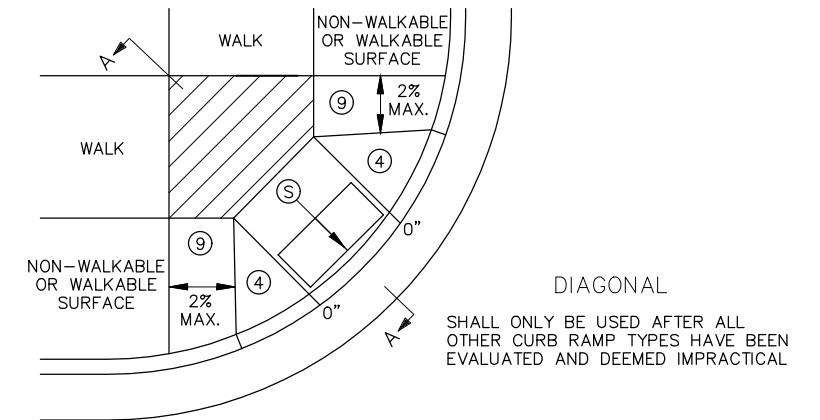
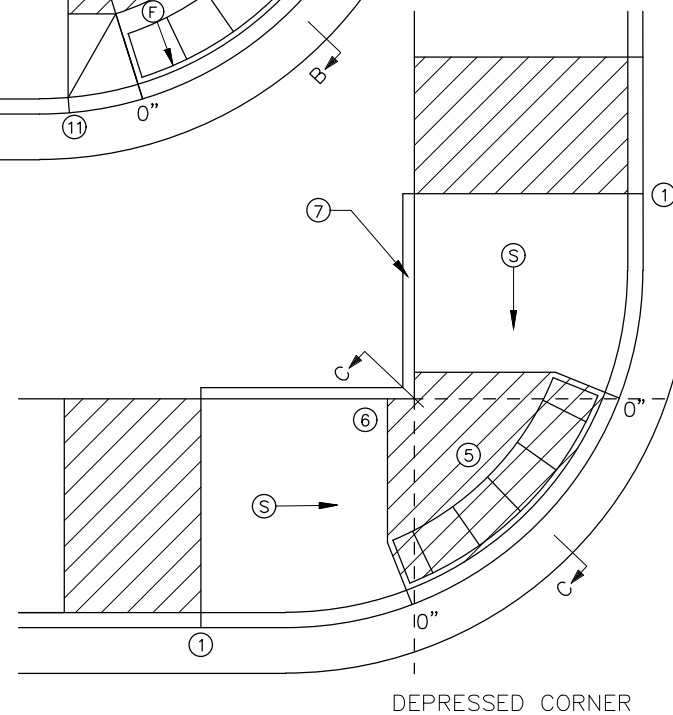
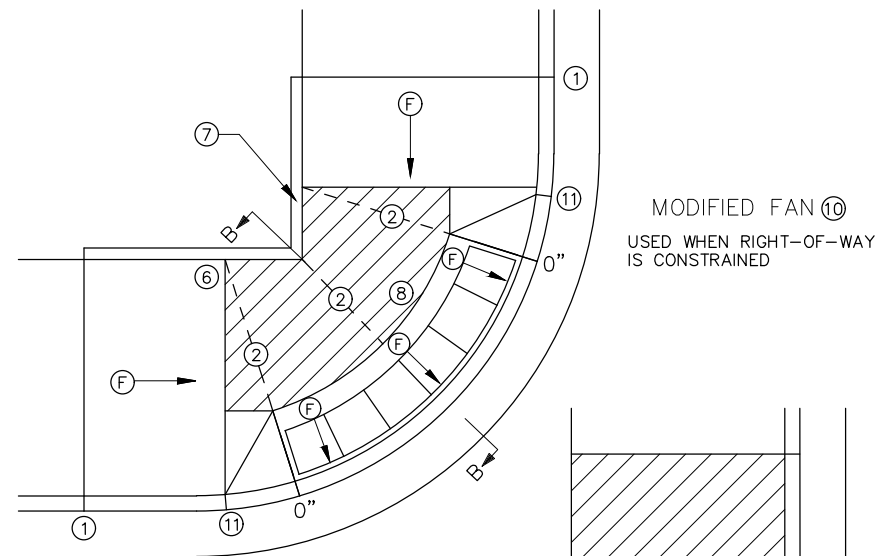
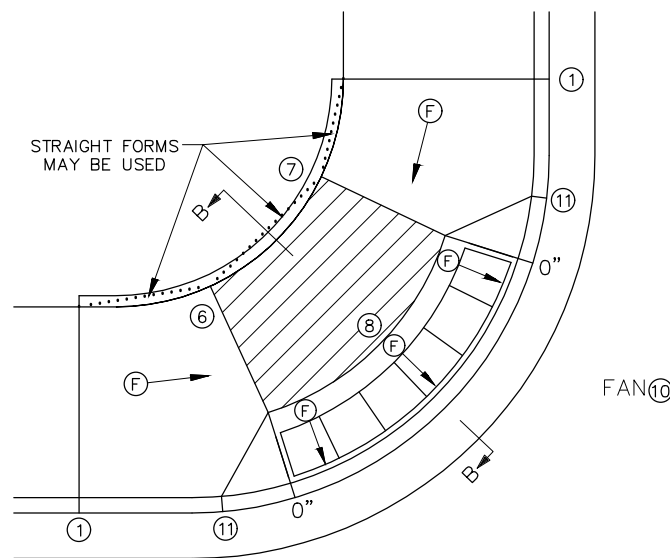
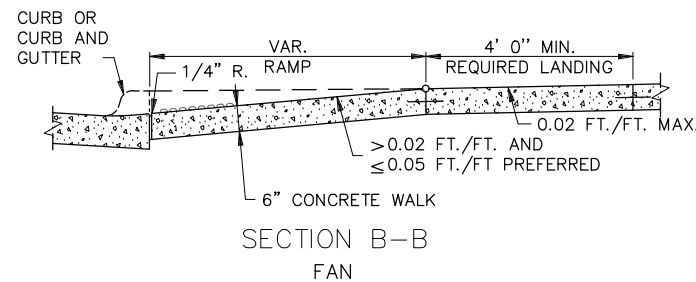
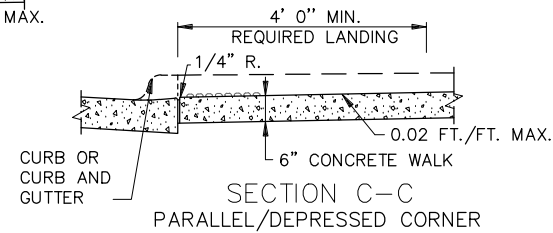
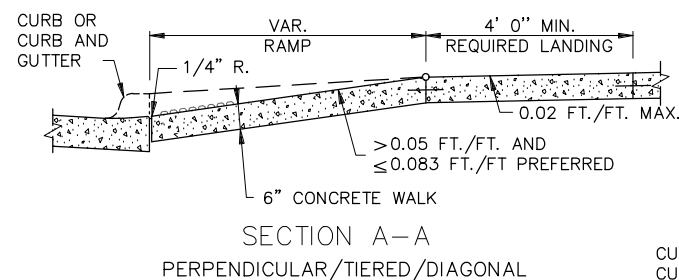
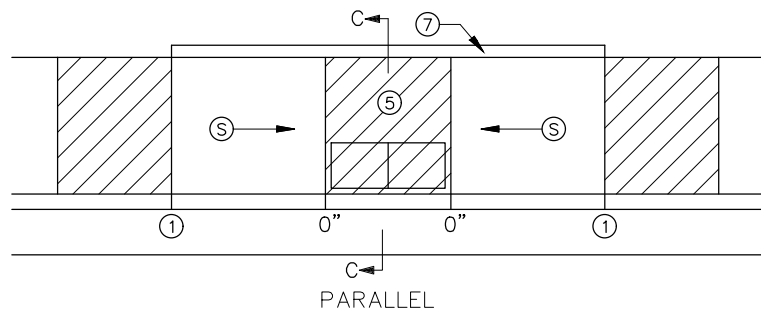
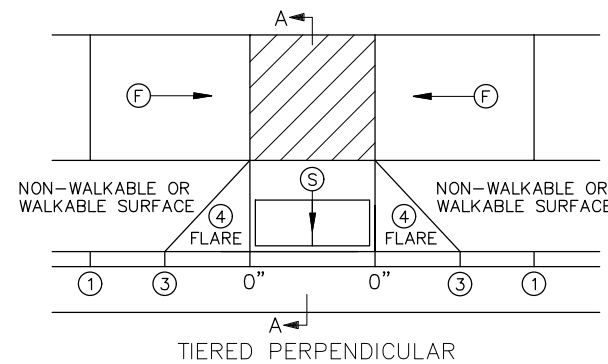
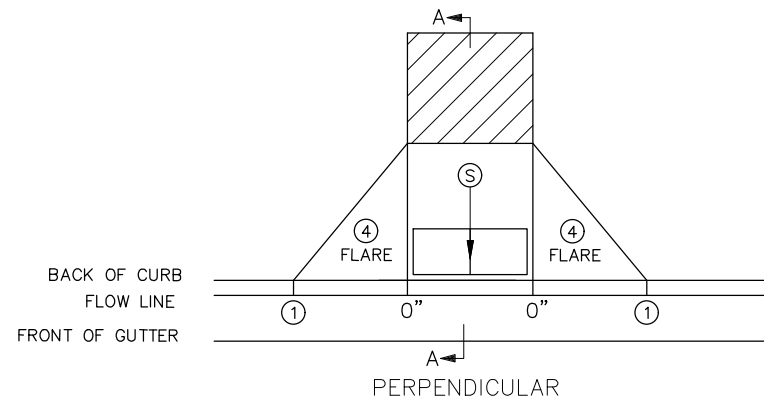


THE VILLAS AT FILLMORE THIRD ADDITION

LIFT STATION LAYOUT AND CONTROL PANEL
 CITY OF ELK RIVER, MINNESOTA

SHEET 15 OF 21 SHEETS

Apr 28, 2025 - 8:50am K:\PRIVATE\2135-36\ENGINEERING\PLAN DWG\2135-36_MNDOT PED RAMP DETAILS.dwg



NOTES:

- LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE GREATER THAN 2%.
- INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6" FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.
- SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30" OF VERTICAL RISE WHEN THE LONGITUDINAL RUNNING SLOPE IS GREATER THAN 5.0%.
- CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOPS OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.
- ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL, THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH. (EXCEPT AS STATED IN 6) BELOW.
- TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, ALL INITIAL LANDINGS AT A TOP OF A RAMPED SURFACE (RUNNING SLOPE GREATER THAN 2%) SHALL BE FORMED AND PLACED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 OF 6 FOR ALL SEPARATELY POURED INITIAL LANDINGS.
- WHEN SIDEWALK IS AT BACK OF CURB, TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE. MAINTAIN POSITIVE BOULEVARD DRAINAGE TO TOP OF CURB.
- ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.
- 4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER THE ENTIRE PAR WIDTH OF SHARED-USE PATHS AND THE ENTIRE PAR WIDTH OF THE WALK WITH THE EXCEPTION OF 3" MAXIMUM ON EACH OUTSIDE EDGE WHICH ENSURES THE DETECTABLE WARNINGS ARE ENCASED IN CONCRETE WHEN ADJACENT TO TURF. WHEN ADJACENT TO CONCRETE FLARES 0" - 3" OFFSET IS ALLOWED.
- WHEN DESIGNING OR ORDERING RECTANGULAR DETECTABLE WARNING SURFACES SHOULD BE 6" LESS THAN THE INCOMING PAR. ARC LENGTH OF THE RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.
- RECTANGULAR DETECTABLE WARNINGS SHALL BE SETBACK 3" FROM THE BACK OF CURB. RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB.

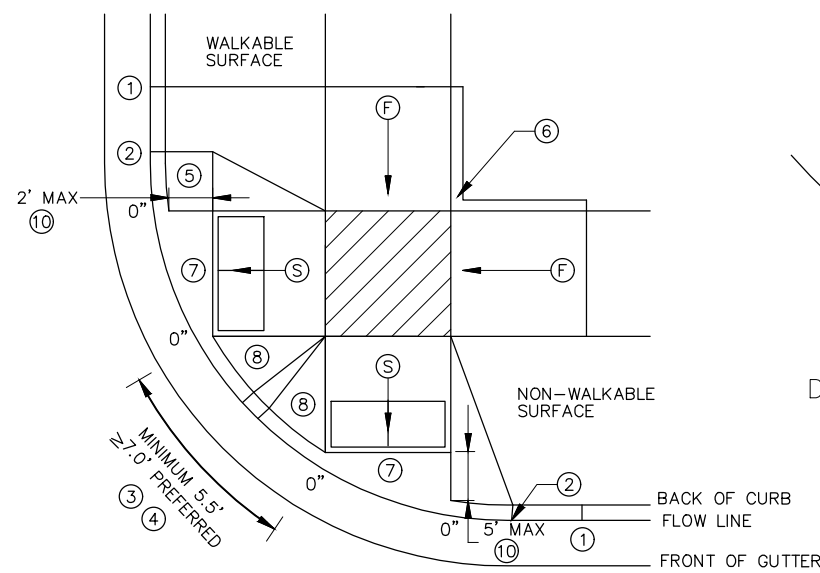
- 1 MATCH FULL HEIGHT CURB.
- 2 4' MINIMUM DEPTH LANDING REQUIRED ACROSS TOP OF RAMP.
- 3 3" HIGH CURB WHEN USING A 3' LONG RAMP, 4" HIGH CURB WHEN USING A 4' LONG RAMP.
- 4 SEE SHEET 4 OF 6, TYPICAL SIDE TREATMENT OPTIONS, FOR DETAILS ON FLARES AND RETURNED CURBS.
- 5 DETECTABLE WARNINGS MAY BE PART OF THE 4' X 4' MIN. LANDING AREA IF IT IS NOT FEASIBLE TO CONSTRUCT THE LANDING OUTSIDE OF THE DETECTABLE WARNING AREA.
- 6 THE GRADE BREAK SHALL BE PERPENDICULAR TO THE BACK OF WALK. THIS WILL ENSURE THAT THE GRADE BREAK IS PERPENDICULAR TO THE DIRECTION OF TRAVEL. (TYPICAL FOR ALL)
- 7 WHEN ADJACENT TO GRASS, GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS LESS THAN 5% RUNNING SLOPE SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
- 8 A 7' MIN TOP RADIUS GRADE BREAK IS REQUIRED TO BE CONSTRUCTIBLE.
- 9 PAVE FULL WALK WIDTH.
- 10 "S" SLOPES ON FANS SHALL ONLY BE USED WHEN ALL OTHER FEASIBLE OPTIONS HAVE BEEN EVALUATED AND DEEMED IMPRACTICAL.
- 11 INTERMEDIATE CURB HEIGHTS TAPER SHALL RISE AT 8-10% TO A MINIMUM 3" CURB HEIGHT. REDUCE INTERMEDIATE CURB HEIGHT TO 2+ INCHES IF NECESSARY TO MATCH ADJACENT BOULEVARD OR SIDEWALK GRADES.

LEGEND	
(S)	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
(F)	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
[Hatched Box]	LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PAR.
X"	CURB HEIGHT

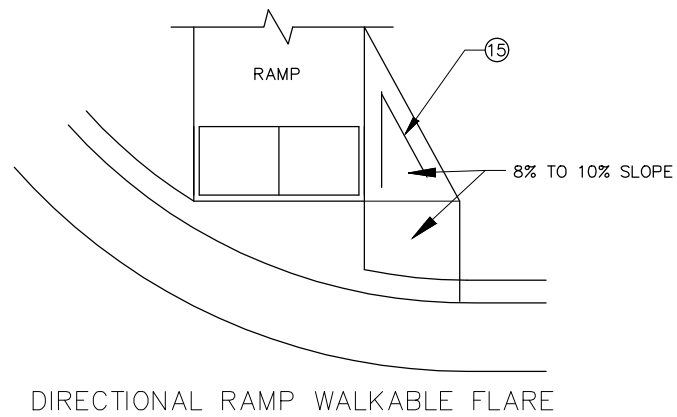
REVISION:
APPROVED: 11-04-2021
Jeff J. Perkins
JEFFREY PERKINS
OPERATIONS DIVISION

m MINNESOTA
DEPARTMENT OF TRANSPORTATION
STANDARD PLAN 5-297.250 1 OF 6
Tom Styrbicki
THOMAS STYRBICKI
STATE DESIGN ENGINEER
APPROVED: 11-04-2021
REVISED:

PEDESTRIAN CURB RAMP DETAILS

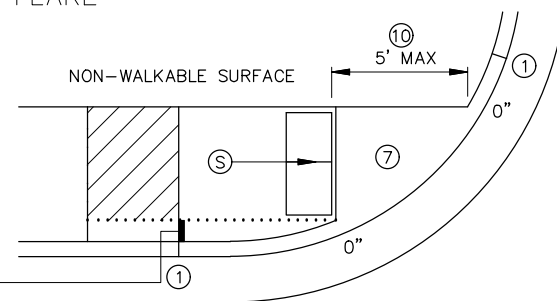


COMBINED DIRECTIONAL

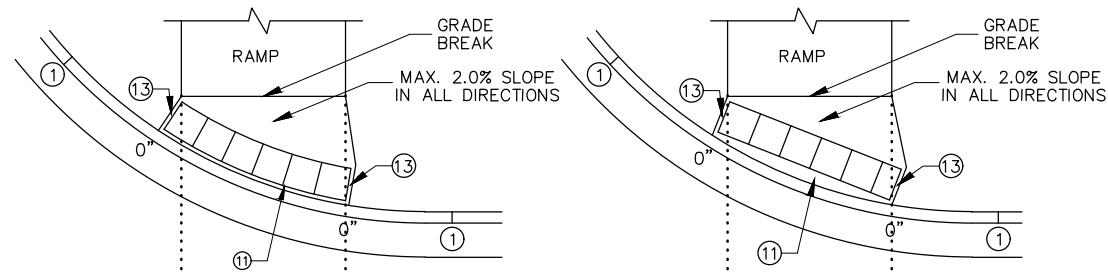


DIRECTIONAL RAMP WALKABLE FLARE

IF NON-CONCRETE BLVD. IS CONSTRUCTED AND IS LESS THAN 2' IN WIDTH AT TOP OF CURB TRANSITION, PAVE CONCRETE RAMP WIDTH TO ADJACENT BACK OF CURB.

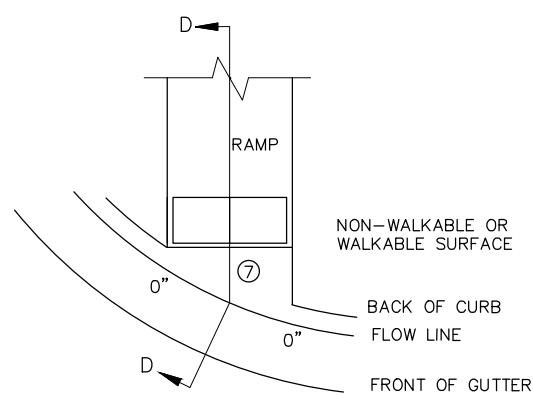


STANDARD ONE-WAY DIRECTIONAL ⑨

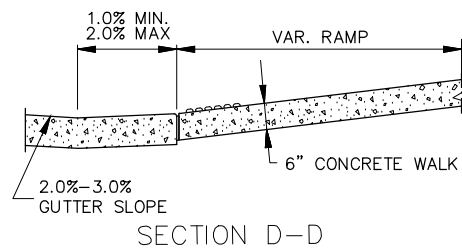


DETECTABLE WARNING PLACEMENT WHEN SETBACK CRITERIA IS EXCEEDED ⑫

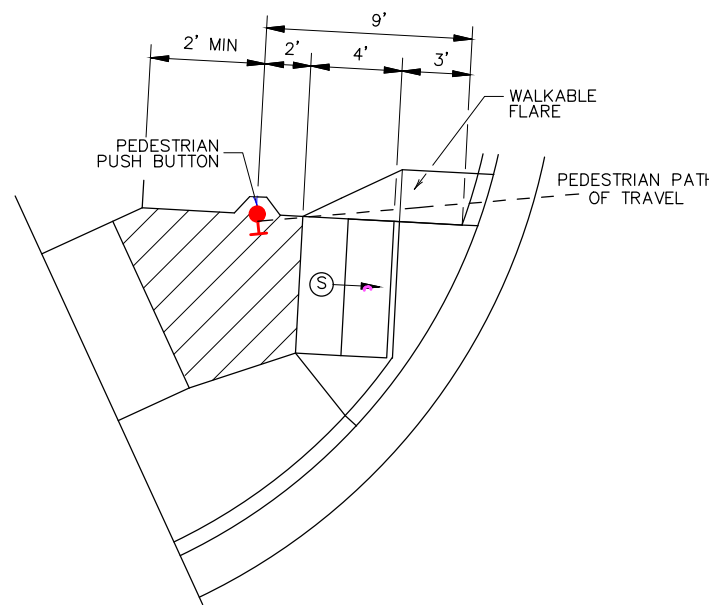
ONE-WAY DIRECTIONAL WITH DETECTABLE WARNING AT BACK OF CURB



CURB FOR DIRECTIONAL RAMPS ⑭



SECTION D-D



SEMI-DIRECTIONAL RAMP ③④⑨

3' DOME SETBACK, 4' LONG RAMP AND PUSH BUTTON 9' FROM THE BACK OF CURB
 PRIMARILY USED FOR APS APPLICATIONS WHERE THE PAR DOES NOT CONTINUE PAST THE PUSH BUTTON (DEAD-END SIDEWALK)

NOTES:

LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE.

INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.

SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30' OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.

CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOP GRADE BREAK OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.

ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL. THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH.

TO ENSURE INITIAL RAMPS AND INITIAL LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS SHALL BE CAST SEPARATELY. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 AND THE ADA SPECIAL PROVISION (PROSECUTION OF WORK).

TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.

WHEN THE BOULEVARD IS 4' WIDE OR LESS, THE TOP OF CURB TAPER SHALL MATCH THE RAMP SLOPES TO REDUCE NEGATIVE BOULEVARD SLOPES FROM THE TOP BACK OF CURB TO THE PAR.

ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.

4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER THE ENTIRE PAR WIDTH OF SHARED-USE PATHS AND THE ENTIRE PAR WIDTH OF THE WALK WITH THE EXCEPTION OF 3" MAXIMUM ON EACH OUTSIDE EDGE WHICH ENSURES THE DETECTABLE WARNINGS ARE ENCASED IN CONCRETE WHEN ADJACENT TO TURF. WHEN ADJACENT TO CONCRETE FLARES 0' - 3' OFFSET IS ALLOWED.

WHEN DESIGNING OR ORDERING RECTANGULAR DETECTABLE WARNING SURFACES SHOULD BE 6" LESS THAN THE INCOMING PAR. ARC LENGTH OF THE RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.

RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB. SEE NOTES ⑩ & ⑪ FOR INFORMATION REGARDING RECTANGULAR DETECTABLE WARNING PLACEMENT.

- ① MATCH FULL CURB HEIGHT.
- ② 3" HIGH CURB WHEN USING A 3' LONG RAMP
4" HIGH CURB WHEN USING A 4' LONG RAMP.
- ③ 3" MINIMUM CURB HEIGHT (5.5' MIN. DISTANCE REQUIRED BETWEEN DOMES)
4" PREFERRED (7' MIN. DISTANCE REQUIRED BETWEEN DOMES).
- ④ THE "BUMP" IN BETWEEN THE RAMPS SHOULD NOT BE IN THE PATH OF TRAVEL FOR COMBINED DIRECTIONAL RAMPS. IF THIS OCCURS MODIFY THE RAMP LOCATION OR SWITCH RAMP TO A FAN/DEPRESSED CORNER.
- ⑤ WHEN USING CONCRETE PAVED FLARES ON THE OUTSIDE OF DIRECTIONAL RAMPS, AND ADJACENT TO A WALKABLE SURFACE, DIRECTIONAL RAMP FLARES SHALL BE USED. SEE THE DETAIL ON THIS SHEET.
- ⑥ GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
- ⑦ MAX. 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK AND DRAIN TO FLOW LINE. SHALL BE CONSTRUCTED INTEGRAL WITH CURB AND GUTTER.
- ⑧ 8% TO 10% WALKABLE FLARE.
- ⑨ PLACE DOMES AT THE BACK OF CURB WHEN ALLOWABLE SETBACK CRITERIA IS EXCEEDED.
- ⑩ FRONT EDGE OF DETECTABLE WARNING SHALL BE SET BACK 2' MAXIMUM WHEN ADJACENT TO WALKABLE SURFACE, AND 5' MAXIMUM WHEN ADJACENT TO NON-WALKABLE SURFACE WITH ONE CORNER SET 3' FROM BACK OF CURB. A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.
- ⑪ RECTANGULAR DETECTABLE WARNINGS MAY BE SETBACK UP TO 9" FROM THE BACK OF CURB WITH CORNERS SET 3" FROM BACK OF CURB. IF 9" SETBACK IS EXCEEDED USE RADIAL DETECTABLE WARNINGS.
- ⑫ FOR DIRECTIONAL RAMPS WITH THE DETECTABLE WARNINGS PLACED AT THE BACK OF CURB, THE DETECTABLE WARNINGS SHALL COVER THE ENTIRE WIDTH OF THE WALK/PATH. THIS ENSURES A DETECTABLE EDGE AND HELPS ELIMINATE THE CURB TAPER OBSTRUCTING THE PATH OF PEDESTRIAN TRAVEL.
- ⑬ THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE BACK OF CURB. MAINTAIN 3" BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- ⑭ TO BE USED FOR ALL DIRECTIONAL RAMPS, EXCEPT WHERE DOMES ARE PLACED ALONG THE BACK OF CURB.
- ⑮ PLACE 2 NO. 4 BARS 4 INCHES FROM SIDE OF FORMS WITH A MINIMUM 2 INCHES OF CONCRETE COVER ALONG EACH SIDE OF FLARE (INCIDENTAL).

LEGEND	
THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.	
Ⓢ	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
Ⓣ	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
[Hatched Box]	LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PAR.
x"	CURB HEIGHT

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REVISION:
 APPROVED: 11-04-2021
 Jeff J. Perkins
 OPERATIONS DIVISION

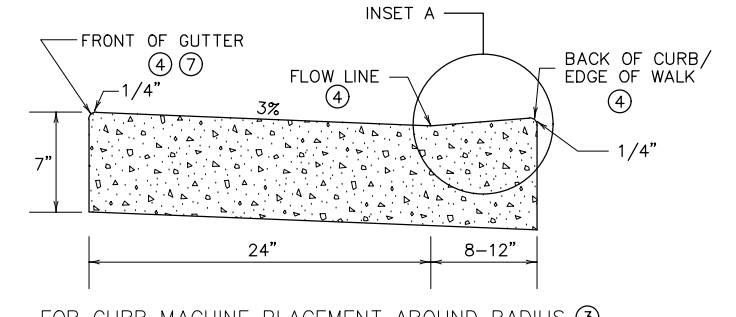
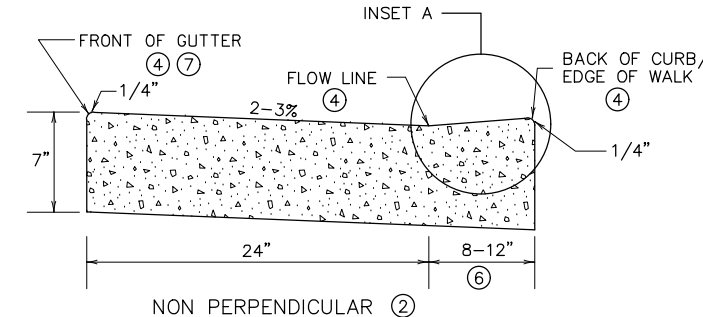
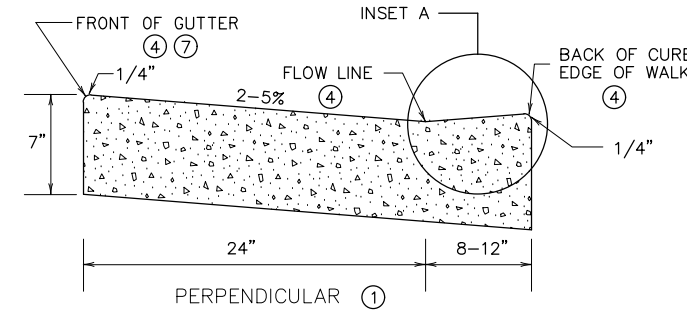
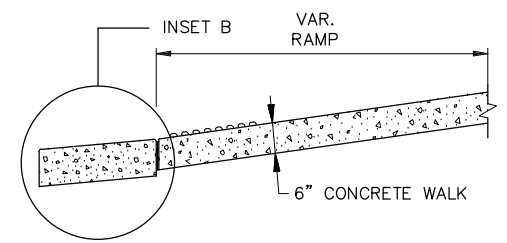
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STANDARD PLAN 5-297.250 | 2 OF 6
 APPROVED: 11-04-2021
 REVISOR:
 SP VALUE

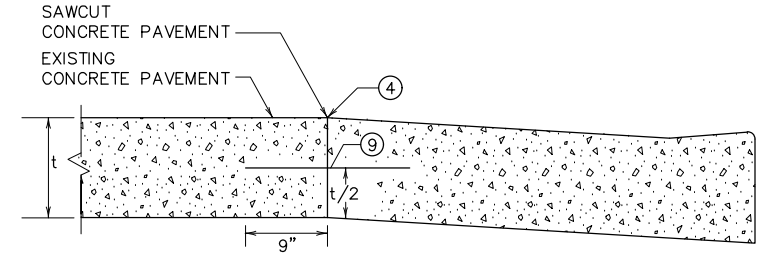
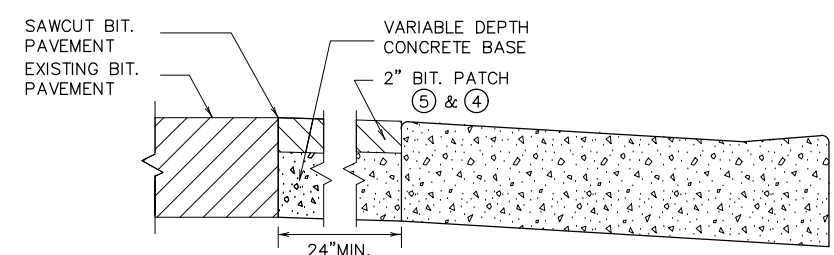
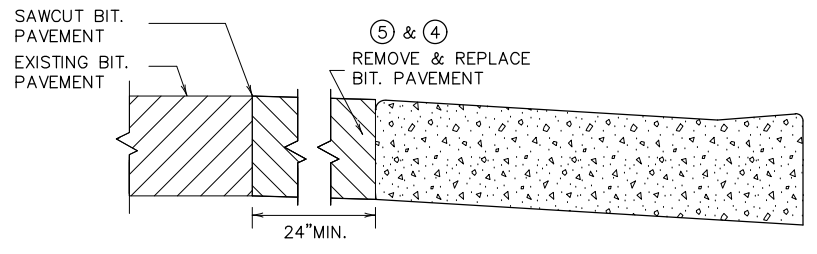
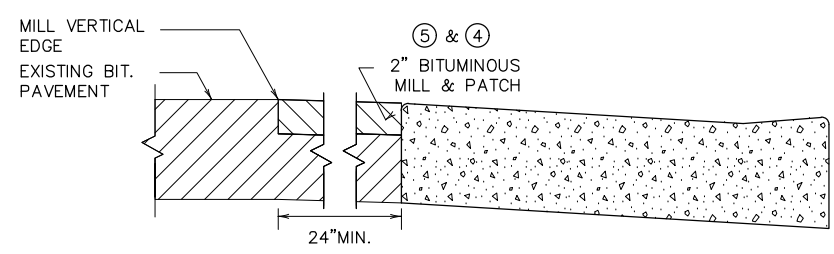
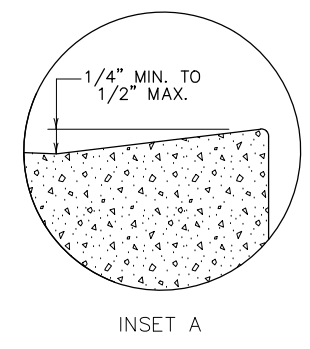
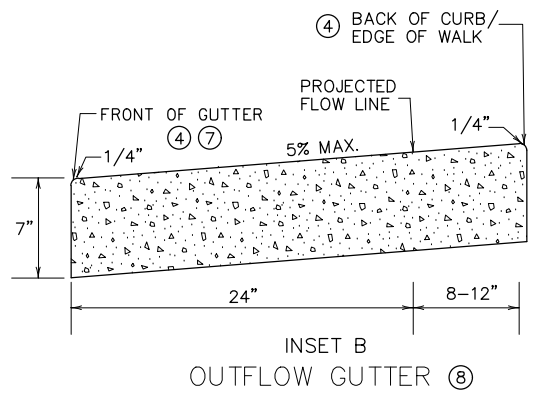
PEDESTRIAN CURB RAMP DETAILS

SHEET NO. 17 OF 21 SHEETS

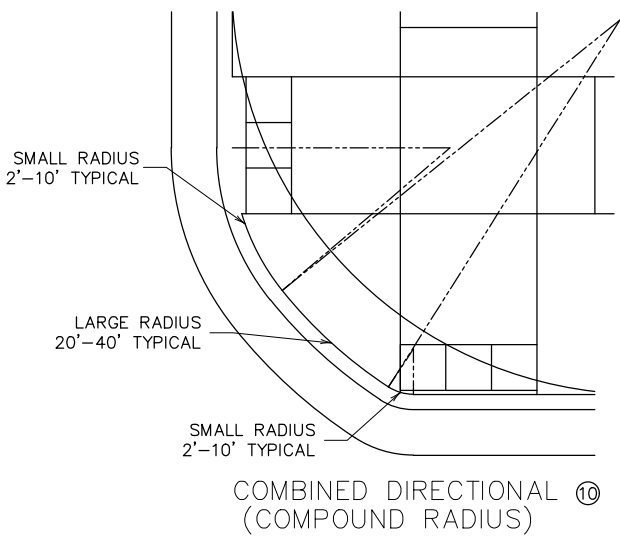
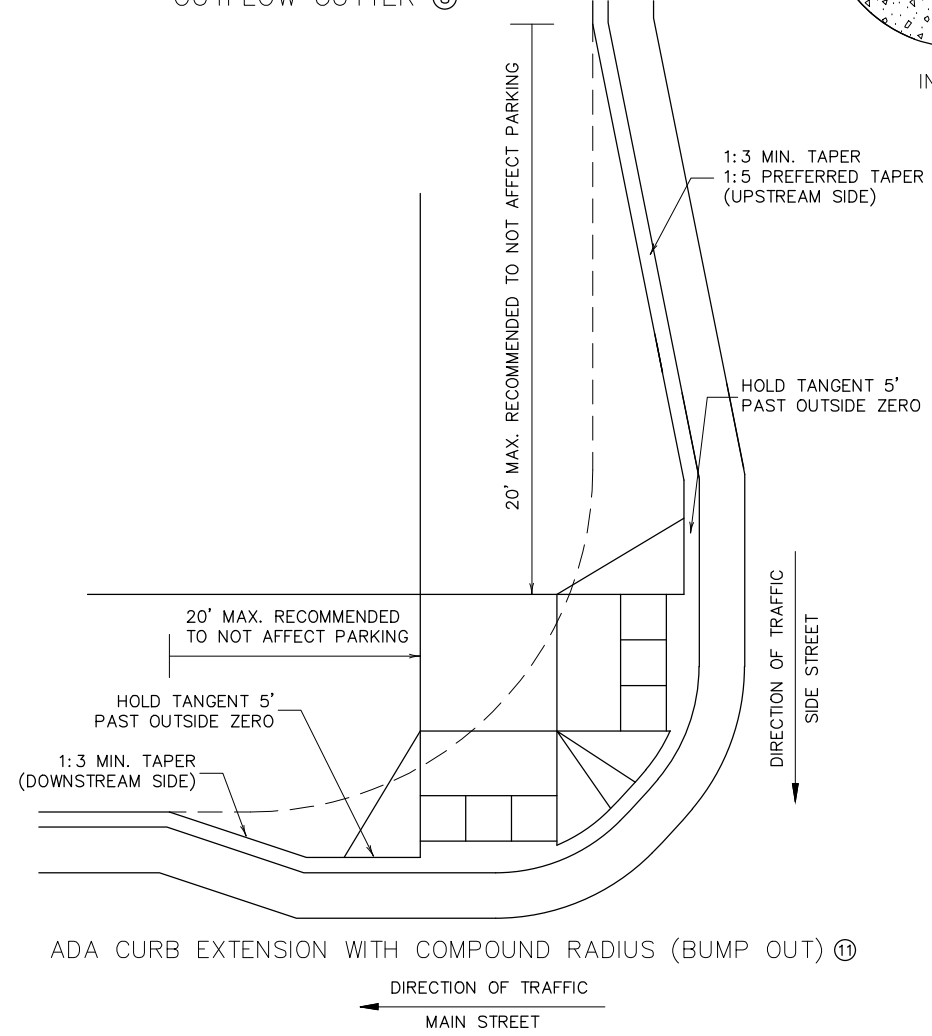
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PEDESTRIAN ACCESS ROUTE CURB & GUTTER DETAIL



PAVEMENT TREATMENT OPTIONS IN FRONT OF CURB & GUTTER FOR USE ON CURB RAMP RETROFITS

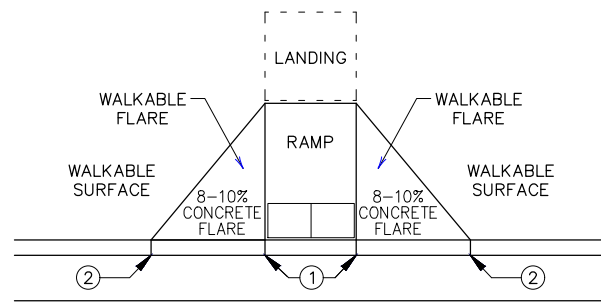


- NOTES:
- POSITIVE FLOW LINE DRAINAGE SHALL BE MAINTAINED THROUGH THE PEDESTRIAN ACCESS ROUTE (PAR) AT A 2% MAXIMUM. NO PONDING SHALL BE PRESENT IN THE PAR.
 - ANY VERTICAL LIP THAT OCCURS AT THE FLOW LINE SHALL NOT BE GREATER THAN 1/4 INCH.
 - ① FOR USE AT CURB CUTS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: PERPENDICULAR, TIERED PERPENDICULAR, PARALLEL, AND DIAGONAL RAMPS.
 - ② FOR USE AT CURB RAMPS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED NON PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: FANS & DEPRESSED CORNERS.
 - ③ BEGIN GUTTER SLOPE TRANSITION 10' OUTSIDE OF ALL CURB RAMPS.
 - ④ THERE SHALL BE NO VERTICAL DISCONTINUITIES GREATER THAN 1/4".
 - ⑤ ELEVATION CHANGE TAKES PLACE FROM THE EXISTING TO NEW FRONT OF GUTTER. PATCH IS USED TO MATCH THE NEW GUTTER FACE INTO THE EXISTING ROADWAY.
 - ⑥ VARIABLE WIDTH FOR DIRECTIONAL CURB APPLICATIONS. SEE SHEET 2 FOR DIRECTIONAL CURB SLOPE REQUIREMENTS.
 - ⑦ TOP FRONT OF GUTTER SHALL BE CONSTRUCTED FLUSH WITH PROPOSED ADJACENT PAVEMENT ELEVATION. TOP 1.5" OF THE GUTTER FACE MUST BE A FORMED EDGE. PAR GUTTER SHALL NOT BE OVERLAID.
 - ⑧ SHOULD BE USED AT VERTICALLY CONSTRAINED AREAS WHEN AT A DRAINAGE HIGH POINT OR SUPER ELEVATED ROADWAY SEGMENTS.
 - ⑨ DRILL AND GROUT NO. 4 EPOXY-COATED 18" LONG TIE BARS AT 30" CENTER TO CENTER INTO EXISTING CONCRETE PAVEMENT 1' MINIMUM FROM ALL JOINTS.
 - ⑩ HELPS PROVIDE TWO SEPARATE RAMPS, REDUCES THE DOME SETBACK LENGTH AND MINIMIZES DIRECTIONAL CURB. THIS RADIUS DESIGN CLOSELY FOLLOWS THE TURNING VEHICLE PATH WHILE OPTIMIZING CURB RAMP LENGTH.
 - ⑪ CURB EXTENSIONS SHOULD BE USED IN VERTICALLY CONSTRAINED AREAS, USUALLY IN DOWNTOWN ROADWAY SEGMENTS WHERE ON-STREET PARKING IS AVAILABLE. CURB EXTENSIONS SHOULD BE CONSIDERED FOR APS INTERSECTIONS WHERE SPACE IS LIMITED. PUSH BUTTONS MUST MEET APS CRITERIA AS DESCRIBED IN THE PUSH BUTTON LOCATION DETAIL SHEET.

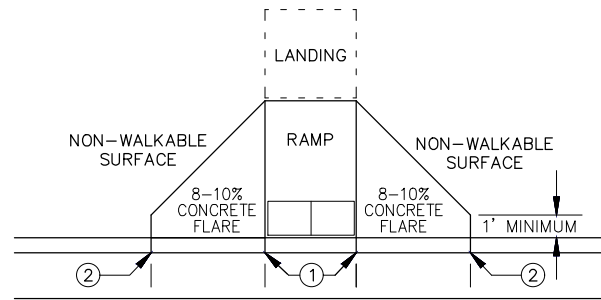
REVISION:
 APPROVED: 11-04-2021
Jeff J. Pel...
 JEFFREY PERKINS
 OPERATIONS DIVISION

	STANDARD PLAN 5-297.250	3 OF 6
		APPROVED: 11-04-2021
DEPARTMENT OF TRANSPORTATION	THOMAS STYRBICKI STATE DESIGN ENGINEER	REVISOR: SP VALUE

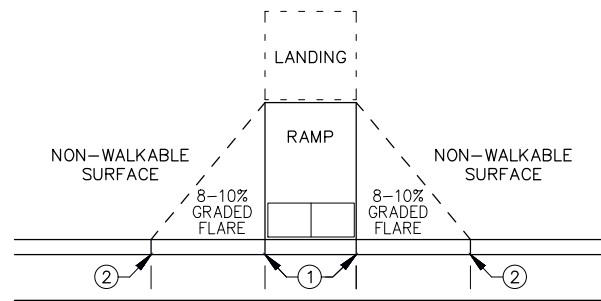
PEDESTRIAN CURB RAMP DETAILS



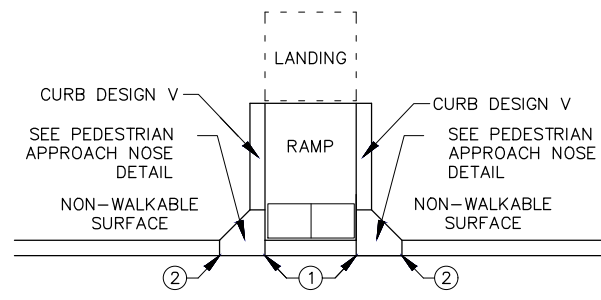
PAVED FLARES
ADJACENT TO WALKABLE SURFACE



PAVED FLARES
ADJACENT TO NON-WALKABLE SURFACE

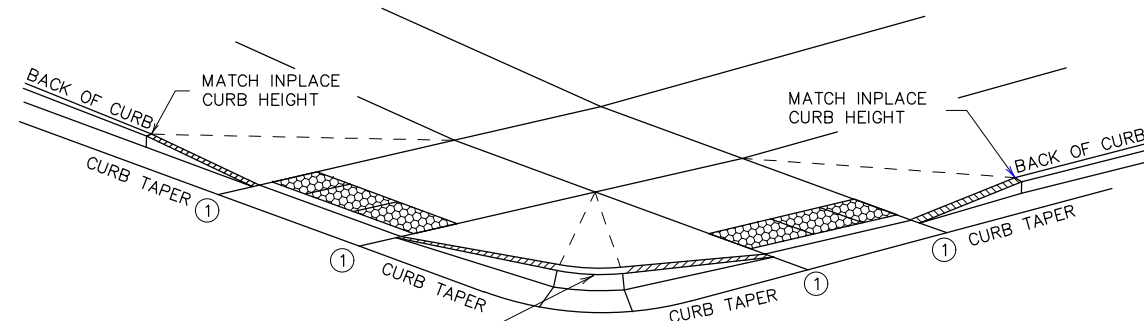


GRADED FLARES



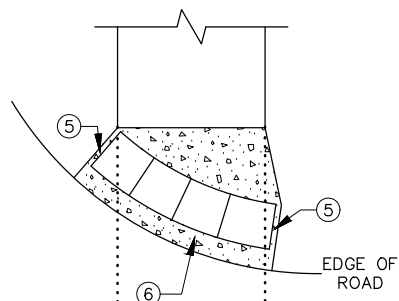
RETURNED CURB ④

TYPICAL SIDE TREATMENT OPTIONS ③ ⑩

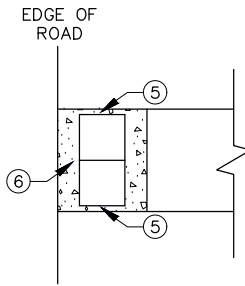


3" MINIMUM CURB HEIGHT, 4" PREFERRED
(MEASURED AT FRONT FACE OF CURB)
FOR A MIN. 6" LENGTH (MEASURED ALONG FLOW LINE)

DETECTABLE EDGE WITH CURB AND GUTTER ⑦

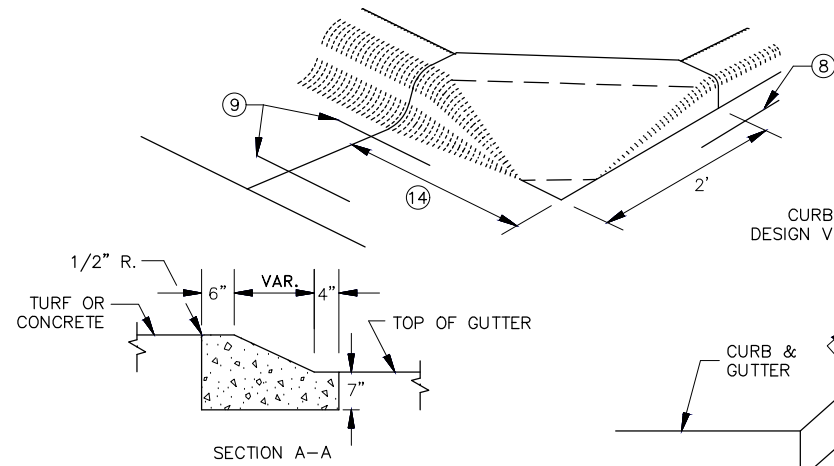


RADIAL DETECTABLE WARNING

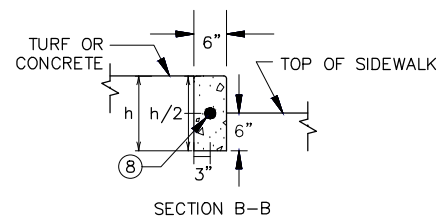


RECTANGULAR DETECTABLE WARNING

DETECTABLE EDGE WITHOUT CURB AND GUTTER

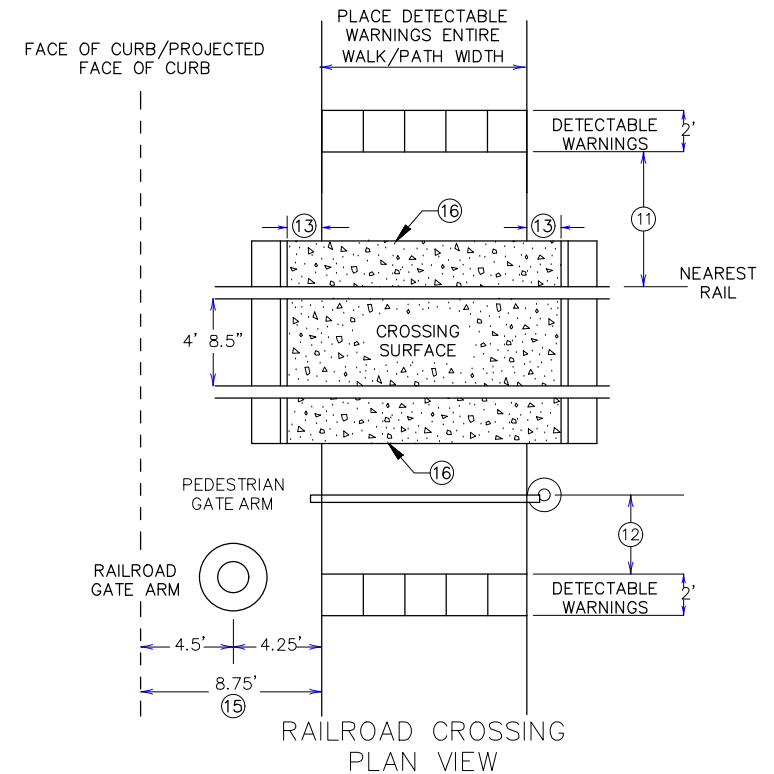


SECTION A-A



SECTION B-B

PEDESTRIAN APPROACH
NOSE DETAIL
(FOR RETURNED CURB
SIDE TREATMENT)



RAILROAD CROSSING
PLAN VIEW

NOTES:

INTERMEDIATE CURB HEIGHTS TAPER SHALL RISE AT 8-10% TO A MINIMUM 3 INCH CURB HEIGHT. INCREASE CURB TAPER LENGTH AT LESS THAN 8% OR REDUCE INTERMEDIATE CURB HEIGHT TO 2+ INCHES IF NECESSARY TO MATCH ADJACENT BOULEVARD OR SIDEWALK GRADES.

SEE STANDARD PLATE 7038 AND THIS SHEET FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.

A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.

CONCRETE FLARE LENGTHS ADJACENT TO NON-WALKABLE SURFACES SHOULD BE LESS THAN 8' LONG MEASURED ALONG THE RAMPS FROM THE BACK OF CURB.

- ① 0" CURB HEIGHT. SEE INSET A ON SHEET 3 OF 6.
- ② FULL CURB HEIGHT.
- ③ SIDE TREATMENTS ARE APPLICABLE TO ALL RAMP TYPES AND SHOULD BE IMPLEMENTED AS NEEDED AS FIELD CONDITIONS DICTATE. THE ENGINEER SHALL DETERMINE THE RAMP SIDE TREATMENTS BASED ON MAINTENANCE OF BOTH ROADWAY AND SIDEWALK, ADJACENT PROPERTY CONSIDERATIONS, AND MITIGATING CONSTRUCTION IMPACTS.
- ④ TYPICALLY USED FOR MEDIANS AND ISLANDS.
- ⑤ WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE EDGE OF ROADWAY. MAINTAIN 3" MAX. BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- ⑥ IF NO CURB AND GUTTER IS PLACED IN RURAL SECTIONS, DETECTABLE WARNINGS SHALL BE PLACED 1' FROM THE EDGE OF BITUMINOUS ROADWAY AND/OR BITUMINOUS SHARED-USE PATH TO PROVIDE VISUAL CONTRAST.
- ⑦ ALL CONSTRUCTED CURBS MUST HAVE A CONTINUOUS DETECTABLE EDGE FOR THE VISUALLY IMPAIRED. THIS DETECTABLE EDGE REQUIRES DETECTABLE WARNINGS WHEREVER THERE IS ZERO-INCH HIGH CURB. CURB TAPERS ARE CONSIDERED A DETECTABLE EDGE WHEN THE TAPER STARTS WITHIN 3" OF THE EDGE OF THE DETECTABLE WARNINGS AND UNIFORMLY RISES TO A 3-INCH MINIMUM CURB HEIGHT. ANY CURB NOT PART OF A CURB TAPER AND LESS THAN 3 INCHES IN HEIGHT IS NOT CONSIDERED A DETECTABLE EDGE AND THEREFORE IS NOT COMPLIANT WITH ACCESSIBILITY STANDARDS.
- ⑧ DRILL AND GROUT 1 - NO. 4 12" LONG REINFORCEMENT BAR (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE V CURB.
- ⑨ DRILL AND GROUT 2 - NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE CURB AND GUTTER.
- ⑩ SIDE TREATMENT EXAMPLES SHOWN ARE WHEN THE INITIAL LANDING IS APPROXIMATELY LEVEL WITH THE FULL HEIGHT CURB (I.E. 6' LONG RAMP FOR 6" HIGH CURB). WHEN THE INITIAL LANDING IS MORE THAN 1" BELOW FULL HEIGHT CURB REFER TO SHEETS 1 & 2 TO MODIFY THE CURB HEIGHT TAPERS AND MAINTAIN POSITIVE BOULEVARD DRAINAGE. CONSTRUCT THESE TAPER AT 0"-3" AT 8-10%, THEN LESS THAN 5% FROM 3" CURB TO FULL CURB HEIGHT.
- ⑪ NEAREST EDGE OF DETECTABLE WARNING SURFACES SHALL BE PLACED 12' MINIMUM TO 15' MAXIMUM FROM THE NEAREST RAIL. FOR SKEWED RAILWAYS IN NO INSTANCE SHALL THE DETECTABLE WARNING BE CLOSER THAN 12' MEASURED PERPENDICULAR TO THE NEAREST RAIL.
- ⑫ WHEN PEDESTRIAN GATES ARE PROVIDED, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE SIDE OF THE GATES OPPOSITE THE RAIL, 2' FROM THE APPROACHING SIDE OF THE GATE ARM. THIS CRITERIA GOVERNS OVER NOTE ⑪.
- ⑬ CROSSING SURFACE SHALL EXTEND 2' MINIMUM PAST THE OUTSIDE EDGE OF WALK OR SHARED-USE PATH.
- ⑭ 3' FOR MEDIANS AND SPLITTER ISLANDS. NOSE CAN BE REDUCED TO 2' ON FREE RIGHT ISLANDS.
- ⑮ SIDEWALK TO BE PLACED 8.75' MIN. FROM THE FACE OF CURB/PROJECTED FACE OF CURB. THIS ENSURES MIN. CLEARANCE BETWEEN THE SIDEWALK AND GATE ARM COUNTERWEIGHT SUPPORTS.
- ⑯ CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPECIFICATION 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE.

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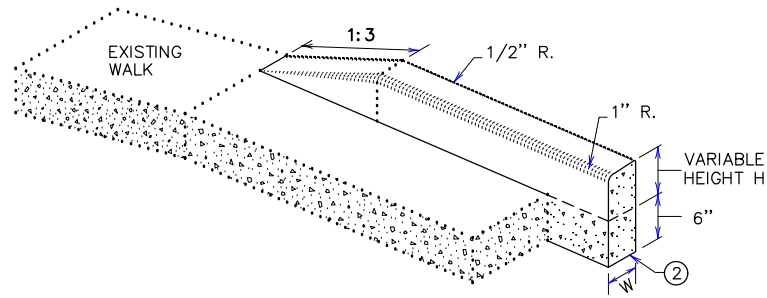
REVISION:
APPROVED: 11-04-2021
Jeff J. Perkins
JEFFREY PERKINS
OPERATIONS DIVISION

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TRANSPORTATION

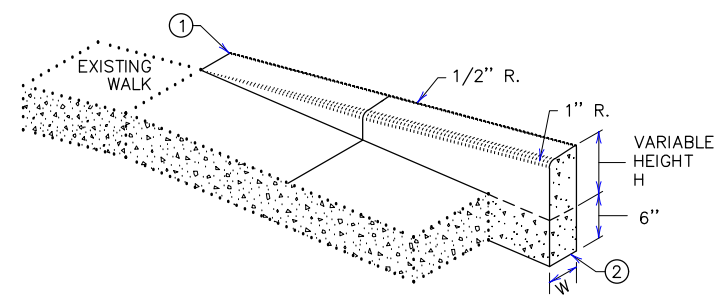
STANDARD PLAN 5-297.250 4 OF 6
Tom Styrbicki
APPROVED: 11-04-2021
REVISED:
THOMAS STYRBICKI
STATE DESIGN ENGINEER
SP VALUE

PEDESTRIAN CURB RAMP DETAILS

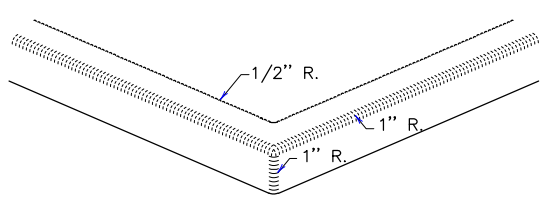
SHEET NO. 19 OF 21 SHEETS



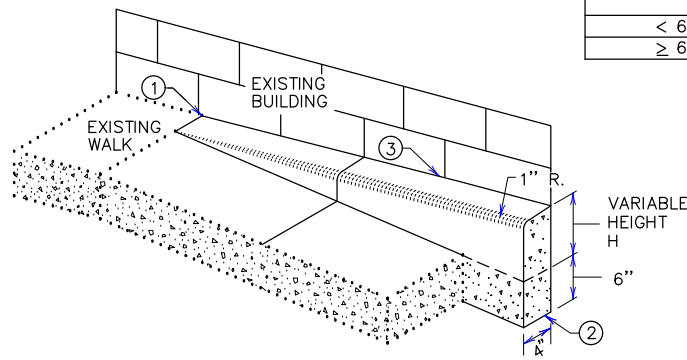
V CURB ADJACENT TO LANDSCAPE
CURB WITHIN SIDEWALK LIMITS



V CURB ADJACENT TO LANDSCAPE
CURB OUTSIDE SIDEWALK LIMITS

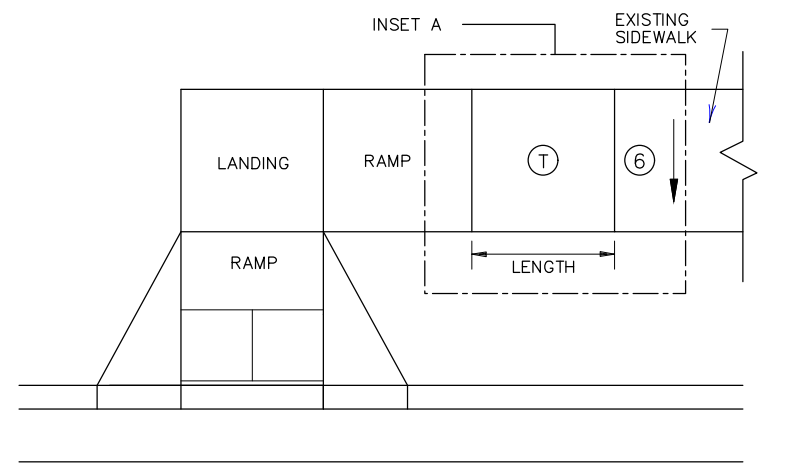


V CURB INTERSECTION

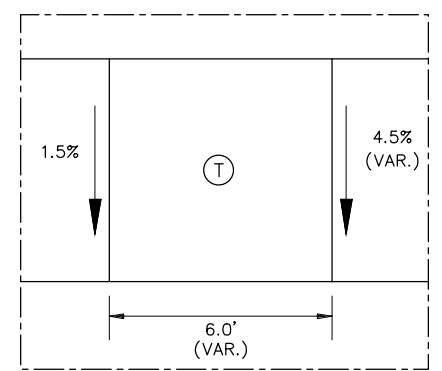


V CURB ADJACENT TO BUILDING
OR BARRIER

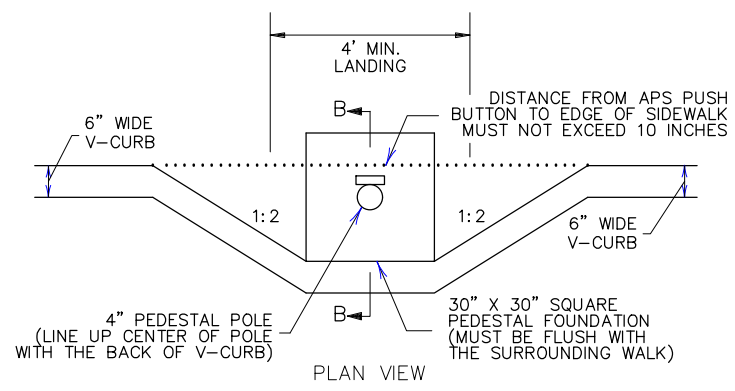
CONCRETE CURB DESIGN V	
CURB HEIGHT H	CURB WIDTH W
< 6"	4"
≥ 6"	6"



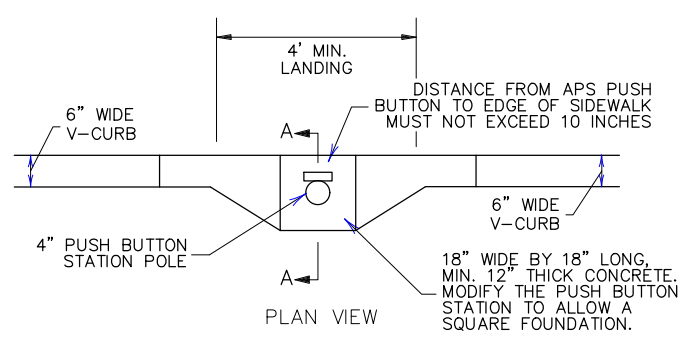
TRANSITION PANEL ④ ⑤



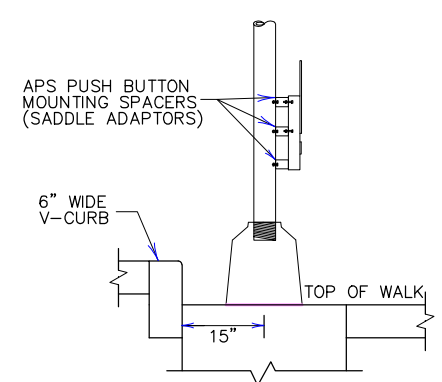
INSET A



PLAN VIEW

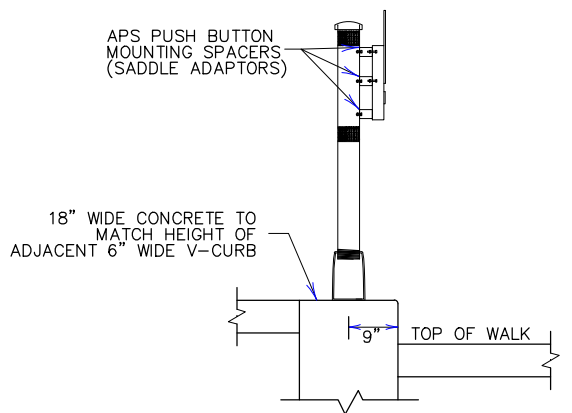


PLAN VIEW



SECTION B-B

SIGNAL PEDESTAL & PUSH BUTTON (V-CURB)



SECTION A-A

PUSH BUTTON STATION (V-CURB)

NOTES:

- A WALKABLE FLARE IS AN 8-10% CONCRETE FLARE THAT IS REQUIRED WHEN THE FLARE IS ADJACENT TO A WALKABLE SURFACE, OR WHEN THE PEDESTRIAN PATH OF TRAVEL OF A PUSH BUTTON TRAVERSES THE FLARE.
- ALL V CURB CONTRACTION JOINTS SHALL MATCH CONCRETE WALK JOINTS.
- WHERE RIGHT-OF-WAY ALLOWS, USE OF V CURB SHOULD BE MINIMIZED. GRADING ADJACENT TURF OR SLOPING ADJACENT PAVEMENT IS PREFERRED.
- V CURB SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS.
- V CURB NEXT TO BUILDING SHALL BE A 4" WIDTH AND SHALL MATCH PREVIOUS TOP OF SIDEWALK ELEVATIONS.
- ① END TAPERS AT TRANSITION SECTION SHALL MATCH INPLACE SIDEWALK GRADES.
- ② ALL V CURB SHALL MATCH BOTTOM OF ADJACENT WALK.
- ③ CONSTRUCT USING APPROVED EXPANSION MATERIAL PER MNDOT TYPE A-E EXPANSION. LEAVE A MINIMUM 1/2" TOP GAP AND SEAL WITH MNDOT APPROVED SILICONE PER MNDOT SPEC 3722.
- ④ THE MAX. RATE OF CROSS SLOPE TRANSITIONING IS 1' LINEAR FOOT OF SIDEWALK PER HALF PERCENT CROSS SLOPE. WHEN PAR WIDTH IS GREATER THAN 6' OR THE RUNNING SLOPE IS GREATER THAN 5%, DOUBLE THE CALCULATED TRANSITION LENGTH.
- ⑤ TRANSITION PANELS ARE TO ONLY BE USED AFTER THE RAMP, OR IF NEEDED, LANDING ARE AT THE FULL CURB HEIGHT (TYPICAL SECTION).
- ⑥ EXISTING CROSS SLOPE GREATER THAN 2.0%.

LEGEND	
↓	THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.
Ⓢ	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
▨	LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PARS.
Ⓣ	TRANSITION PANEL(S) - TO BE USED FOR TRANSITIONING THE CROSS-SLOPE OF A RAMP TO THE EXISTING WALK CROSS-SLOPE. RATE OF TRANSITION SHOULD BE 0.5% PER 1 LINEAR FOOT OF WALK. SEE THIS SHEET FOR ADDITIONAL INFORMATION.

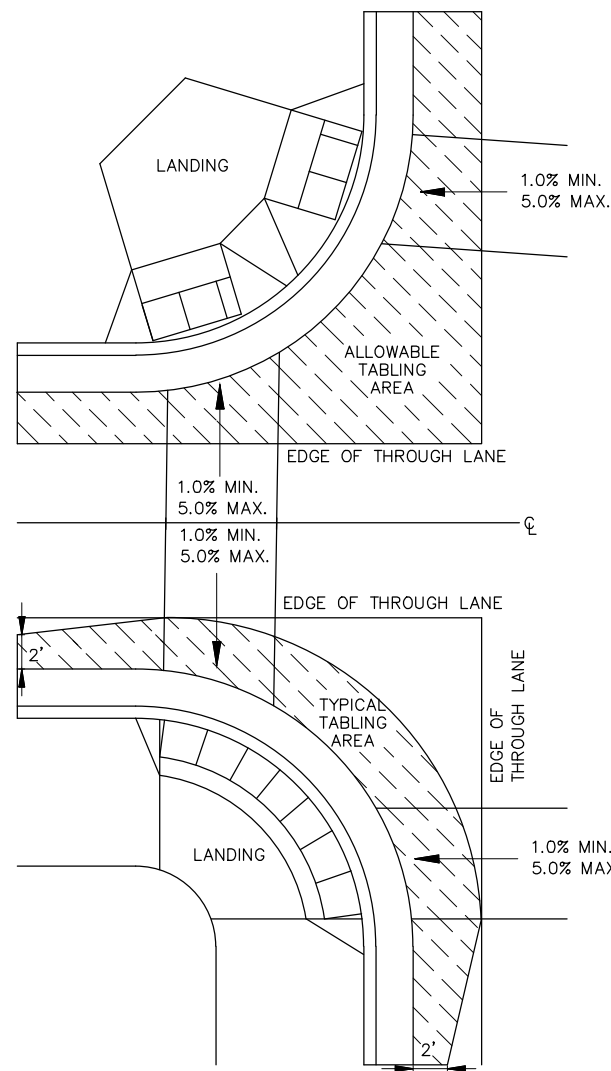
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REVISION:
APPROVED: 11-04-2021
Jeff J. Perkins
JEFFREY PERKINS
OPERATIONS DIVISION

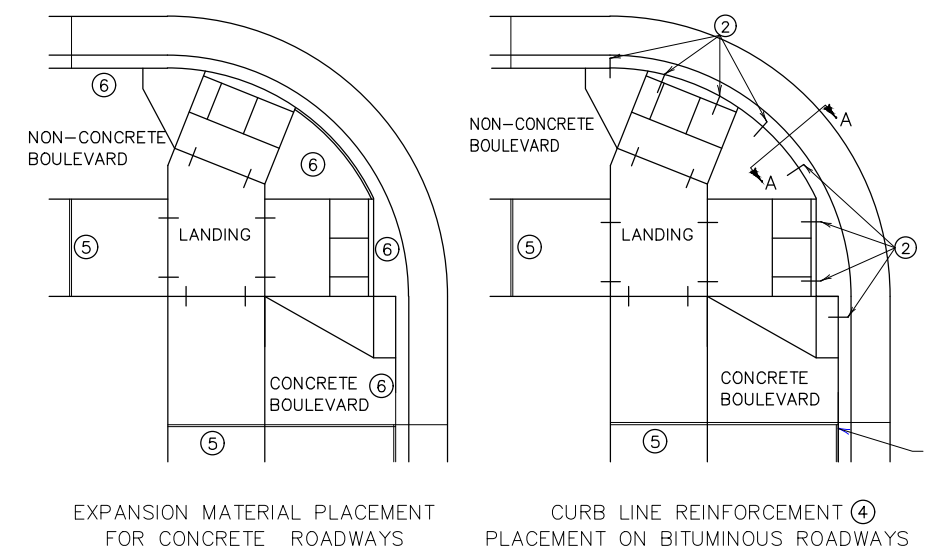
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OF
TRANSPORTATION

STANDARD PLAN 5-297.250 5 OF 6
Tom Styrbicki
THOMAS STYRBICKI
STATE DESIGN ENGINEER
APPROVED: 11-04-2021
REVISED:
SP VALUE

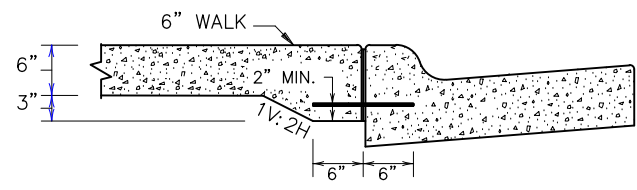
PEDESTRIAN CURB RAMP DETAILS



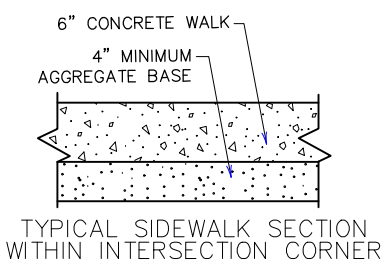
CURB LINE AND ROAD CROSSING ADJUSTMENTS



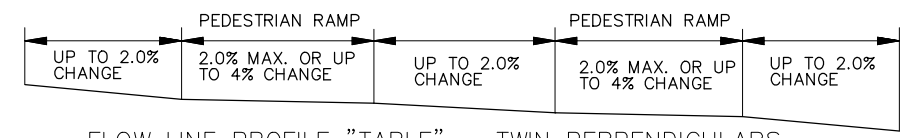
EXPANSION MATERIAL PLACEMENT FOR CONCRETE ROADWAYS CURB LINE REINFORCEMENT ④ PLACEMENT ON BITUMINOUS ROADWAYS



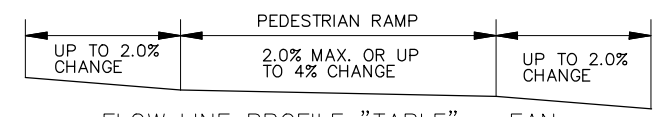
SECTION VIEW A-A THICKENED SECTION THROUGH CURB RAMP FLARES



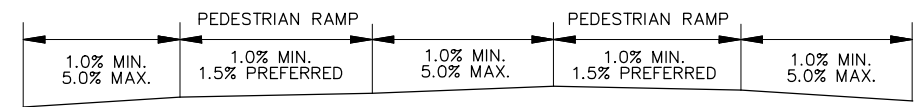
TYPICAL SIDEWALK SECTION WITHIN INTERSECTION CORNER



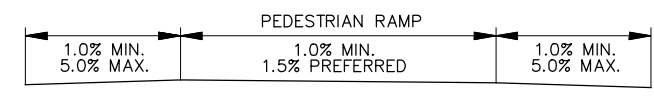
FLOW LINE PROFILE "TABLE" - TWIN PERPENDICULARS



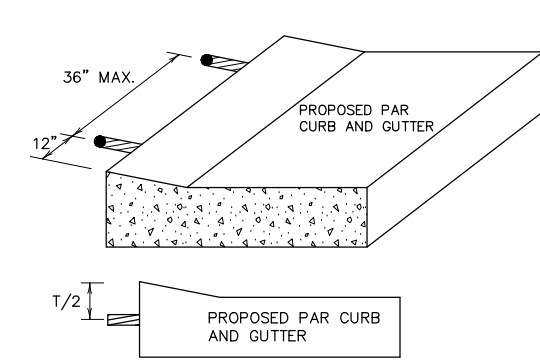
FLOW LINE PROFILE "TABLE" - FAN



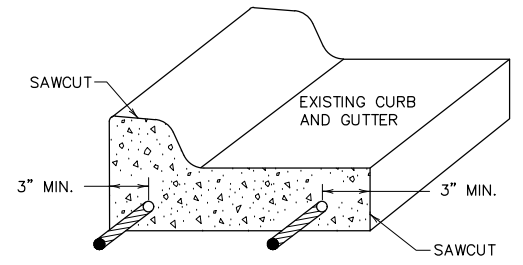
FLOW LINE PROFILE RAISE - TWIN PERPENDICULARS



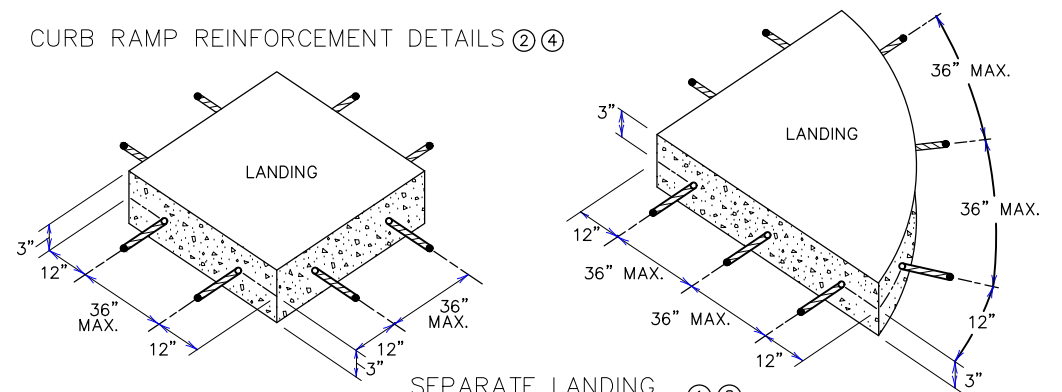
FLOW LINE PROFILE RAISE - FAN



CURB RAMP REINFORCEMENT DETAILS ② ④



CURB AND GUTTER REINFORCEMENT ③



SEPARATE LANDING POUR REINFORCEMENT ① ②

GENERAL NOTES:

- "TABLING" OF CROSSWALKS MEANS MAINTAINING LESS THAN 2% CROSS SLOPE WITHIN A CROSSWALK, IS REQUIRED WHEN A ROADWAY IS IN A STOP OR YIELD CONDITION AND THE PROJECT SCOPE ALLOWS.
- RECONSTRUCTION PROJECTS: ON FULL PAVEMENT REPLACEMENT PROJECTS "TABLING" OF ENTIRE CROSSWALK SHALL OCCUR WHEN FEASIBLE.
- MILL & OVERLAY PROJECTS: "TABLING" OF FLOW LINES, IN FRONT OF THE PEDESTRIAN RAMP, IS REQUIRED WHEN THE EXISTING FLOW LINE IS GREATER THAN 2%. WARPING OF THE BITUMINOUS PAVEMENT CAN NOT EXTEND INTO THE THROUGH LANE. TABLE THE FLOW LINE TO 2% OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA:
 - 1) 1.0% MIN. CROSS-SLOPE OF THE ROAD
 - 2) 5.0% MAX. CROSS-SLOPE OF THE ROAD
 - 3) "TABLE" FLOW LINE UP TO 4% CHANGE FROM EXISTING SLOPE IN FRONT OF PEDESTRIAN RAMP
 - 4) UP TO 2% CHANGE IN FLOW LINE FROM EXISTING SLOPE BEYOND THE PEDESTRIAN CURB RAMP
- STAND-ALONE ADA RETROFITS: FOLLOW MILL & OVERLAY CRITERIA ABOVE HOWEVER ALL PAVEMENT WARPING IS DONE WITH BITUMINOUS PATCHING ON BITUMINOUS ROADWAYS AND FULL-DEPTH APRON REPLACEMENT ON CONCRETE ROADWAYS.
- RAISING OF CURB LINES SHOULD OCCUR IN VERTICALLY CONSTRAINED AREAS. RAISE THE CURB LINES ENOUGH TO ALLOW COMPLIANT RAMPS OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA:
 - 1) 1.0% MIN. AND 5.0% MAXIMUM CROSS-SLOPE OF THE ROAD
 - 2) 1.0% MIN. FLOW LINE (ON EITHER SIDE OF PEDESTRIAN RAMP) TO MAINTAIN POSITIVE DRAINAGE
 - 3) 5.0% RECOMMENDED MAX. FLOW LINE
 - 4) LONGITUDINAL THROUGH LANE ROADWAY TAPERS SHOULD BE 1" VERTICAL PER 15' HORIZONTAL

NOTES:

- ① TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, ALL INITIAL LANDINGS AT A TOP OF A RAMPED SURFACE (RUNNING SLOPE GREATER THAN 2%) SHALL BE FORMED AND PLACED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON THIS SHEET FOR ALL SEPARATELY POURED INITIAL LANDINGS.
- ② DRILL AND GROUT NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) AT 36" MAXIMUM CENTER TO CENTER MINIMUM 12" SPACING FROM CONSTRUCTION JOINTS. BARS TO BE ADJUSTED TO MATCH RAMP GRADE. BARS TO BE PAID BY EACH.
- ③ DRILL AND GROUT 2 - NO. 4 X 12" LONG (6" EMBEDDED) REINFORCEMENT BARS (EPOXY COATED). REINFORCEMENT REQUIRED FOR ALL CONSTRUCTION JOINTS. BARS TO BE PAID BY EACH.
- ④ THIS CURB LINE REINFORCEMENT DETAIL SHALL BE USED ON BITUMINOUS ROADWAYS. FOR CONCRETE ROADWAYS, SEE NOTE 6.
- ⑤ CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPECIFICATION 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE.
- ⑥ USE AN APPROVED TYPE F (1/4 INCH THICK) SEPARATION MATERIAL. SEPARATION MATERIAL SHALL MATCH FULL HEIGHT DIMENSION OF ADJACENT CONCRETE.

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REVISION:
 APPROVED: 11-04-2021
 Jeff J. Perkins
 OPERATIONS DIVISION

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 OF
 TRANSPORTATION

STANDARD PLAN 5-297.250 6 OF 6
 APPROVED: 11-04-2021
 REVISOR:
 SP VALUE
 THOMAS STYRBICKI
 STATE DESIGN ENGINEER

PEDESTRIAN CURB RAMP DETAILS

SHEET NO. 21 OF 21 SHEETS



Request for Action

To
Planning Commission

Item Number
4.4

Meeting Date
May 27, 2025

Prepared By
Zack Carlton, Community Development Director

Item Description
Ordinance Amendment: PUD Amendment for Tall Pines P.U.D. Second Addition

Reviewed by
Ryan Sandhoefner
Chris Leeseberg

Action Requested

Recommend, by motion, approval of an ordinance amending the PUD agreement for Tall Pines, subject to the following conditions:

1. Staff approval of all civil plans identifying infrastructure improvements required to meet post-construction stormwater requirements.
2. All improvements must be completed by July 31, 2025.
3. The ordinance amending the PUD will not be recorded until the improvements have been completed, and the city has verified that the pond meets post-construction stormwater requirements.

Background/Discussion

The Tall Pines Planned Unit Development (PUD) was approved in phases between 2017 and 2020 and included a number of deviations from city ordinances, including reduced setbacks from stormwater ponds. During the construction process, city engineering staff noted that a proposed pond was not functioning as designed. The pond was holding water, although it was designed as a dry pond. This change in functionality impacts the required pond setbacks for multiple homes within the PUD. These encroachments were not approved as part of the original PUD reviews.

In order for the property to come into compliance with city processes, the pond needs to be updated to function as intended, or the developer must receive approval for additional structures within the required setback. The developer has requested approval of additional encroachments into the required pond setbacks. This change also requires modifications to the existing pond to ensure compliance with state stormwater standards. Staff included a number of conditions of approval requiring the pond improvements prior to the PUD amendment being recorded.

Financial Impact

None

Mission/Policy/Goal

Support the growth and development of the community.
Ethical, efficient, and responsible.

The Elk River Vision

A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity



Attachments

1. Location Map
2. Applicant's Narrative
3. Stormwater Pond Modifications



Project Location Map

Roger Derrick - Tall Pines 2nd Ordinance Amendment

Case No: OA 25-06

NARRATIVE

We are requesting approval of the designation change of our pond from a “dry pond” to a “wet pond”, together with approval of two structures and trails that encroach upon the 25-foot pond setback (as shown on the attached exhibit).

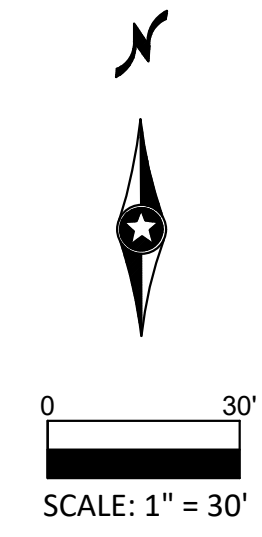
In 2021, the pond was part of our Tall Pines townhouse project. At that time, it was to be a “wet pond” with a waterproof liner. The liner was necessary because of the pond’s proximity to a City well. The pond was dug in the fall of 2021 as part of the grading of Tall Pines. In the spring of 2022, after the pond was dug but before the liner was installed, it was discovered that the City well was not too close after all, so the liner was not required.

During that time, the title company had a title objection because there was a shed situated partially on our property and partially on the vacant property next door. I tracked down the owners of the property to obtain their approval to remove the shed. They were three young people from Russia in their 30s. The man had been adamant that he wanted to develop the property himself and started building a house, which was immediately condemned because he didn’t bother to get a building permit. (Evidently, building permits are not required in Russia.) I managed to reach his wife by phone. She spoke English but her husband and his brother only spoke Russian. She told me he had a stroke, at 37 years, and now could not develop their property. Everyone seemed to want this property developed along with ours because it was a long, narrow, difficult piece with a wetland in the middle. You will recall that we discussed various land uses. It was determined that townhouse use would be the best. We submitted a couple different plans before we got one that really worked. Then, our engineer told us that the addition of this small parcel to the pond runoff makes the pond too small based on the engineer manual calculations. Our engineer redesigned the pond to make it a “dry pond”, which now conformed with the engineer’s manual. Once everything was completed, it was discovered that the pond water did not percolate down to the water table as required for a “dry pond”. Because the pond does not perc but stays full of water during the wet season, it seems a “wet pond” designation is appropriate after all.

Thank you.

LEGEND:

- 20' SETBACK TO BUFFER
(PER CITY ORDINANCE)
- 25' BUFFER FROM STORMWATER POND HIGHWATER LEVEL
(PER CITY ORDINANCE)



GENERAL NOTES:

Sec. 30-1852. - Performance standards.

a. Wetland buffer strip. A minimum 25-foot buffer strip from the delineated boundary of a wetland or ordinary highwater level of a stormwater pond is hereby established. Wetland buffer strips shall meet the following standards:

1. No principle or accessory structures, patios, paving, or other impervious surfaces shall be permitted within the wetland buffer strip.
2. Alterations are prohibited within the wetland and wetland buffer strip, unless a vegetation management plan or other approval is granted by the city.
3. The applicant shall establish and maintain wetland and wetland buffer strip vegetation in accordance with the requirements found in section 30-1853.
4. The applicant shall maintain the required wetland buffer strip monuments at each lot line where it crosses a wetland buffer strip and at 200-foot intervals along the edge of the wetland buffer strip. Each monument shall state "Wetland Buffer: Under the City Code, no mowing, clearing or removal of vegetation in the buffer area is allowed. Call the City of Elk River for more information." The city reserves the right to eliminate this requirement if it deems it impractical.
5. The city, at its sole discretion, following the city's master park trail plan, may allow nature trails comprised of natural materials, to be located within the wetland buffer strip.
6. Leisure activities such as hiking, nature studies, canoeing, boating and horseback riding, including facilities such as nature trails and docks which allow for such activities are permitted in the wetland buffer strip.
7. Final plats for all subdivisions shall provide an easement over all wetlands and wetland buffer strips for the purposes of identification and protection.
8. Properties platted prior to June 19, 2006, shall be exempt from the vegetation management requirements described in subsections ii-iv above.

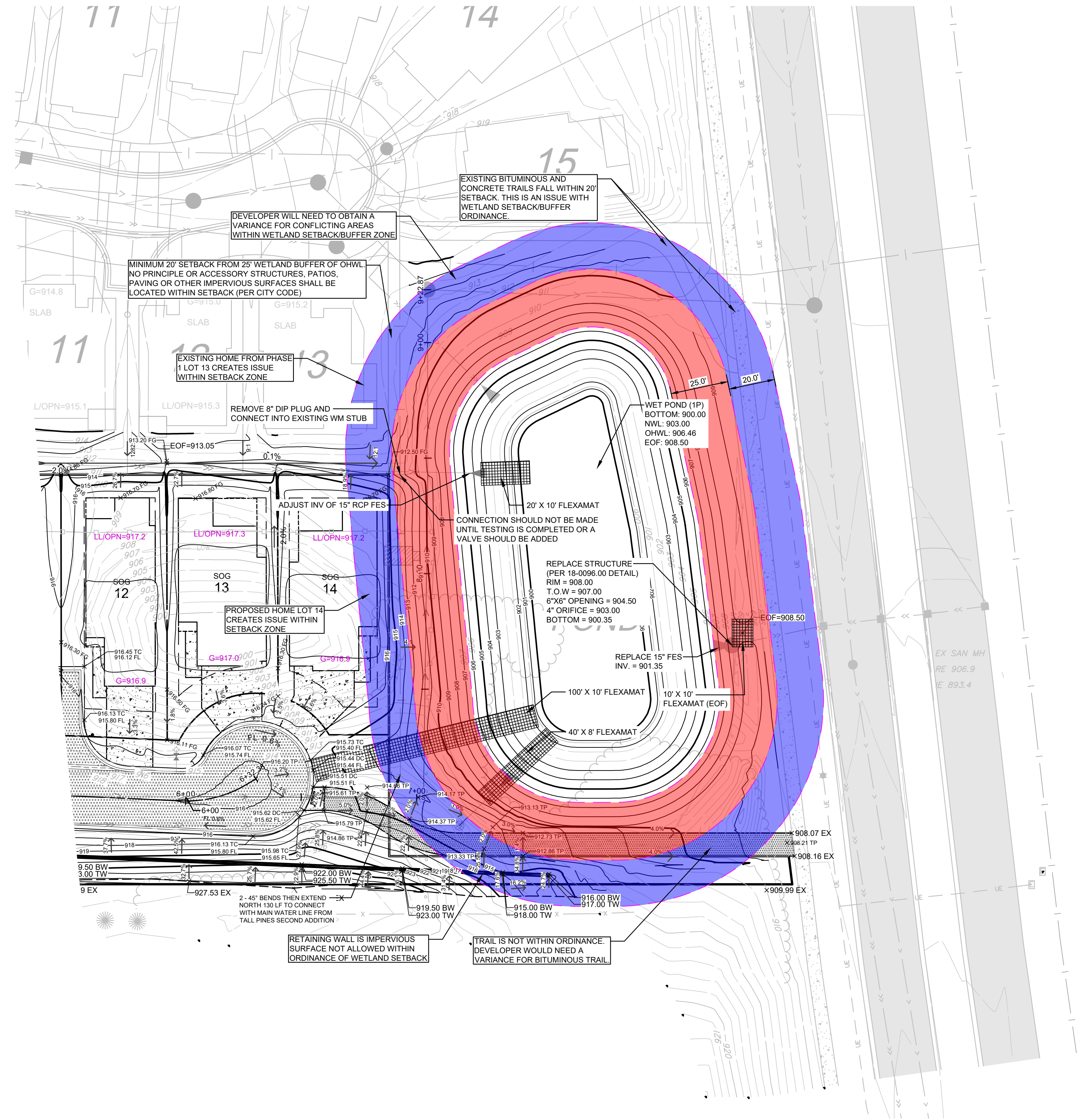
b. Wetland buffer strip setback. A minimum 20-foot setback from wetland buffer strips is hereby established by the city.

1. No principle or accessory structures, patios, paving, or other impervious surface shall be located within this setback.
2. Nature trails, at the city's approval, may be located within the wetland buffer strip setback.
3. On property that was platted prior to June 19, 2006, uncovered decks and permeable pavement can be located within this setback.

c. Exceptions. Properties platted prior to adoption of the wetland buffer strip and wetland buffer strip setback performance standards set forth in this section may not be configured to accommodate the required performance standards. For this reason, exceptions to the performance standards for properties platted prior to June 19, 2006, may be approved by the planning manager pursuant to the site plan review process. Applicants for site plan review shall pay the fee specified in the city's fee schedule. Site plan approvals and required conditions shall be made by the planning manager on a case by case basis, with the goal of maintaining consistency with the performance standards to the maximum extent reasonably possible. Appeals from staff determinations and conditions may be made pursuant to the appeal procedures set forth in section 30-634.

d. Fences. Fences may encroach into the wetland buffer strip setback, but at no time shall they be allowed within the wetland buffer strip or in delineated wetlands. Properties platted prior to June 19, 2006, shall be exempt from this requirement.

(Ord. No. 06-07, § 2, 6-19-2006; Ord. No. 16-24, § 1, 11-21-2016; Ord. No. 17-07, § 1, 4-17-2017; Ord. No. 18-12, § 1, 7-16-2018)



REV NO.	DATE	DESCRIPTION

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly Licensed Professional Engineer under the laws of the State of Minnesota.

Signed: Christopher J. Dahm Lic. No. 58828
Date: _____

BOGART, PEDERSON & ASSOCIATES, INC.
LAND SURVEYING
ENVIRONMENTAL SERVICES
13076 FIRST STREET, BECKER, MN 55508-9322
TEL: 763-262-8822 FAX: 763-262-8844

**STORMWATER POND REDESIGN
EVERGREEN TOWNHOMES, LLC.**
City of ELK RIVER, Sherburne County, MN

WET POND REDESIGN OPTION

SHEET NO.
**OPTION
3**



Request for Action

To
Planning Commission

Item Number
4.5

Meeting Date
May 27, 2025

Prepared By
Zack Carlton, Community Development Director

Item Description
Modification of Legal Nonconforming Advertising Signs (Billboards)

Reviewed by
Chris Leeseberg

Action Requested

Recommend, by motion, approval of the request to reduce, relocate, and modify legal nonconforming advertising signs (billboards) subject to the following conditions:

1. The city does not require the demolitions or relocation of a billboard until the land beneath the structure is included in a final plat.
2. Billboards must be removed from land included in a final plat for Oakwater Ridge and slated for dedication as a frontage road prior to releasing the plat for recording.
3. Billboards relocated and constructed on the north side of Highway 10 must be done in accordance with the even spacing exhibit to improve compliance under the current city codes.
4. At no time shall there be an increase in the total number of billboard structures, faces, or sign area on the subject properties.

Background/Discussion

The subject property is in the same location as the proposed Oakwater Ridge/Specht Farm subdivision proposed by Capstone homes. This request seeks approval from the city to move existing billboards on the land slated for residential development. The proposal will support the residential subdivision by removing the billboards from the area proposed for a future frontage road as the property is developed.

The subject properties currently have 21 billboard structures along Highway 10, 14 of which are located on the south side of Highway 10 and seven are located on the north side of Highway 10. The existing billboards vary in size, height, and spacing and are considered legal nonconforming structures as they were in place prior to the current ordinance. The current ordinance prohibits all billboards on residentially zoned properties, and limits billboards to commercial and industrial lots of at least 1-acre, with each billboard requiring their own lot.

The proposal removes all 14 billboards from the south side of Highway 10 to support the Oakwater Ridge subdivision and replaces the seven structures on the north side of the highway with four new double-sided billboards. The new billboards would be spaced further apart to improve compliance with city codes, which require 1,200 feet between billboard structures.

City ordinance Sec. 30-854(d) states:

(d) Relocation or alteration. The city council may, upon application of a property owner, permit the alteration or

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A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity



relocation of a legal nonconforming sign if, after a public hearing by the planning commission and city council, the council determines that total compliance with the current sign regulations is not reasonable but the alteration or relocation of the nonconforming signs is in greater conformity with the existing sign regulations and is in the best interest of the community.

While the proposal does not bring the property into full compliance with city codes, it does move the property significantly closer to compliance. Additionally, it will improve the visual appearance of the Highway 10 corridor by reducing sign clutter and supporting the proposed Oakwater Ridge subdivision.

Financial Impact

None

Mission/Policy/Goal

Support the growth and development of the community.
Appropriately govern in an ever-changing environment.

Attachments

1. Location Map
2. Applicant's Narrative
3. Application Exhibits



Project Location Map

Specht

General Planning Permit

Case No: GP 25-05



April 28, 2025

Zack Carlton, Community Development Director
City of Elk River
13065 Orono Parkway
Elk River, MN 55330

Via Email and Online
zcarlton@ElkRiverMN.gov

Re: Change of Nonconforming Use – 16723 US-10

Dear Mr. Carlton:

We represent Security Bank and Trust, Trustee of the Doris Specht and the Erwin T. Specht Trust (the “Applicant”), the owner of the parcel at 16723 US-10 (“Property”). On behalf of the Applicant, we are seeking City Council approval to alter and relocate certain legal nonconforming signs on the Property located along Highway 10 near Specht Lane in Elk River, Minnesota (“Project”).

The billboards are currently legally nonconforming due to their proximity to one another, which does not comply with the current sign regulations. The Project seeks to substantially reduce the number of signs on and around the Property and reconstruct a smaller number of modern, uniform signs in greater conformity with the City Code and community standards.

Proposed Project

There are currently 21 billboard structures around the Property along Highway 10, 14 of which are located on the south side of Highway 10 and seven are located on the north side of Highway 10 on the Property. The billboards vary in size, height, and spacing, creating a cluttered and inconsistent visual appearance along the highway corridor.

The Project proposes to remove all 14 sign structures on the south side of the highway and replace the seven sign structures on the north side with four new back-to-back billboards on the Property, evenly spaced. The new signs would have a smaller footprint and a more streamlined design. The Project would result in a net reduction of 17 sign structures, 20 sign faces, and 4,291 square feet of signage. The new sign structures would be spaced approximately 350 feet from one another, instead of the current spacing which ranges from 219 to 227 feet on the north side of Highway 10.

The Project brings the nonconforming use of the billboards to greater conformity with the City’s sign regulations and promotes the intent of the ordinance to reduce visual clutter, enhance safety and aesthetics, and support the character of the highway corridor.

Applicable Standards and Considerations

The City Code under Section 30-854(d) allows the City Council, following a public hearing, to approve the alteration or relocation of a legal nonconforming sign if total compliance is not reasonable and the proposed change brings the signs into greater conformity and serves the

community's best interest. The following factors support approval of the Project under the City Code:

- Total compliance is not reasonable due to the long-standing legal nonconforming status of the signs and physical limitations of the Property.
- The proposed signs are fewer in number, more uniform in appearance, better spaced, and significantly reduce overall signage, supporting the City's goals for sign regulation.
- The proposed changes improve the visual landscape, reduce visual clutter for highway users, and promote compatibility with adjacent land uses (which include agricultural, commercial, and industrial uses).

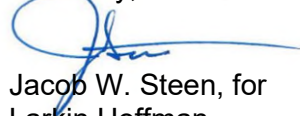
Further, under Section 30-697, legally nonconforming uses may continue through repair, replacement, maintenance, or improvement, and Section 30-698 permits limited expansion provided that the cost does not exceed 25 percent of the assessed value of the improvements and that the expansion does not reduce compatibility with surrounding properties. The Project complies with these provisions by modernizing the signage design, reducing the number and size of signs, and improving compatibility with adjacent land uses.

Conclusion

Based on the above analysis, the Project meets the criteria set forth in Section 30-854(d) of the City Code for the alteration and relocation of legal nonconforming signs, as full compliance with current sign regulations is not reasonable, and the proposed changes bring the signage into greater conformity with those regulations. The Project is also consistent with the purpose and intent of the City's sign ordinance and comprehensive plan, and supports community goals related to visual quality, safety, and land use compatibility. The Project is also consistent with Sections 30-697 and 30-698 of the City Code, as it involves the improvement of a longstanding legal nonconforming use, remains within the allowable limits for modifications, and does not reduce compatibility with adjacent uses.

We respectfully request City Council approval of the proposed modifications. Please do not hesitate to reach out with any questions. Thank you for your time and consideration.

Sincerely,



Jacob W. Steen, for
Larkin Hoffman

Direct Dial: 952-896-3239

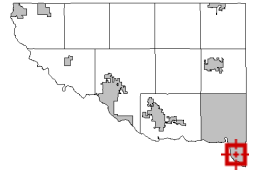
Direct Fax: 952-842-1738

Email: jsteen@larkinhoffman.com

Enclosure



Overview



Legend

Public Water Inventory

- Not Classified
- General Development
- Natural Environment
- Recreational Development
- Parcels
- Streams

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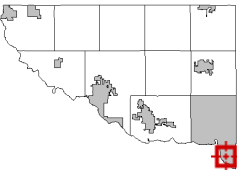
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Overview



Legend

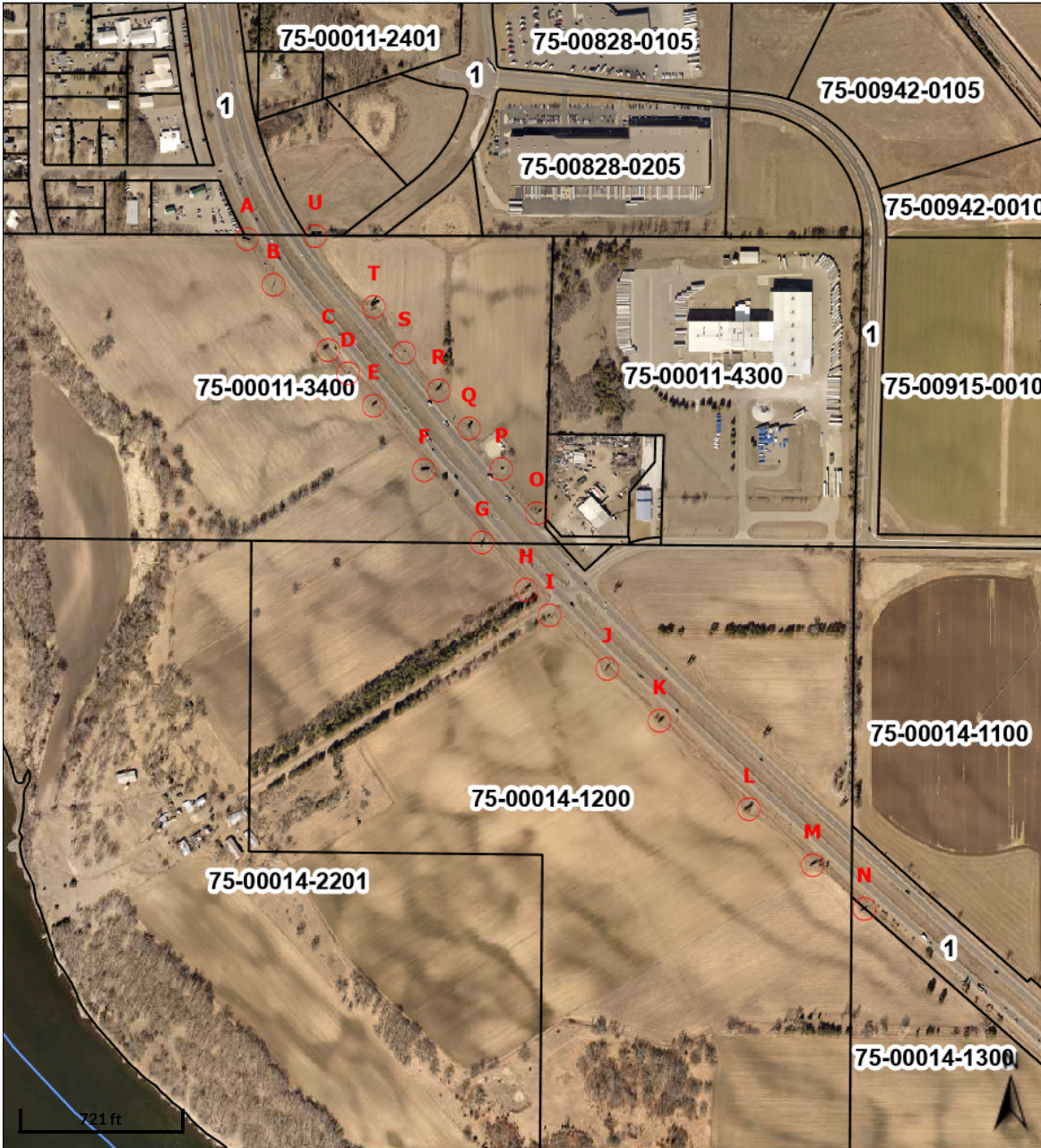
- Public Water Inventory**
- Not Classified
 - General Development
 - Natural Environment
 - Recreational Development
 - Parcels
 - Streams

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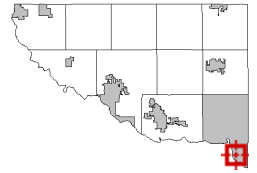
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Overview



Legend

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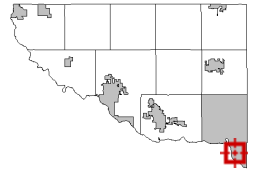
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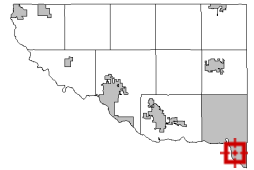
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Request for Action

To
Planning Commission

Item Number
5.1

Meeting Date
May 27, 2025

Prepared By
Zack Carlton, Community Development Director

Item Description
Downtown Master Plan

Reviewed by
Chris Leeseberg

Action Requested

Recommend, by motion, approval of the Downtown Master Plan.

Background/Discussion

City staff, along with representatives from multiple boards and commissions, have been working closely with Bolton and Menk to create a 20-year vision for downtown Elk River. The plans are outlined in the 2024 Downtown Elk River Master Plan and have been recommended for adoption by the plan subcommittee.

The city held a public open house on April 22, 2025, and the Planning Commission had a preliminary review of the plan and recommendations later that evening. The full plan is attached and staff are looking for a recommendation to move the document forward for consideration by the City Council.

Financial Impact

None

Mission/Policy/Goal

Support the growth and development of the community.

Attachments

- I. Draft Downtown Master Plan

The Elk River Vision

A welcoming community with revolutionary and spirited resourcefulness, exceptional service, and community engagement that encourages and inspires prosperity



DOWNTOWN MASTER PLAN

Elk River, Minnesota
Draft Report as of April 21, 2025

Prepared for:
City of Elk River

Prepared by:
Bolton & Menk
Leland Consulting

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ACKNOWLEDGEMENTS

Steering Committee

- Allison McDowell (Downtown Business Representative)
- Deana McLean (Downtown Business Representative)
- Jennifer Wagner (City Council Representative)
- Lance Lindberg (Heritage Preservation Commission Representative)
- Lynn Caswell (Housing and Redevelopment Authority Representative)
- Perry Beise (Planning Commission Representative)
- Jill Larson-Vito (Elk River Resident)

City Staff

- Zack Carlson (Community Development Director)
- Chris Leeseberg (Senior Planner)
- Brent O’Neil (Economic Development Director)
- Josh Mollan (Economic Development Specialist)

City Leadership

- John J. Dietz - Mayor
- Cory Grupa - Ward 1
- J. Brian Calva - Ward 2
- Mike Beyer - Ward 3
- Jennifer Wagner - Ward 4



1 INTRODUCTION

THE PURPOSE OF THIS DOWNTOWN PLAN

The purpose of this Plan is to create a vision and series of action steps for the Downtown Area of Elk River — a vision that responds to the needs of today, and prepares for the desires of the future.

Downtown is of particular importance to the community because it is the psychological center of the community. Comprised of small and medium sized properties, it is in many ways a place of opposites - a place of pride and community gatherings; a core separated from the rest of the city; a beautiful river that lacks a strong presence; major regional visibility offered by infrastructure that bisects the area; a community that takes pride in both its downtown, and country identities.

“Elk River’s attractive and well-programmed downtown will be a destination for residents as well as tourists interested in enjoying a night out for shopping, dining, and entertainment.

The downtown will leverage its waterfront to anchor events and draw thousands to the area.

Pedestrian connections throughout the downtown will be enhanced, encouraging visitors to park once and explore on foot.”

-Elk River 2040 Comprehensive Plan

It is also of particular importance because downtown Elk River, like many smaller downtown, sits at an inflection point in its history. As it wrestles with a post-COVID economic identity, downtown is seeking to become more than just a place to work and shop - but also one in which people can live, gather, celebrate, and play.

With nearly all of the private land in the area developed, or encumbered by highways and rail, Downtown Elk River must creatively pursue strategies of redevelopment, creative connectivity, and people-oriented public spaces. The purpose of this Downtown Plan therefore is to provide clear, attainable guidance to align private and public investments towards goals of a more connected, inviting, vibrant destination for residents and visitors.



ELK RIVER THROUGH THE YEARS

A Trading Hub

Like many present day river cities, Elk River originated as a hub for fishing and trading. The earliest human activity saw the Ojibwa and Dakota people using the land for hunting, fishing, and traveling along the Mississippi. Early European settlers eventually set up trade posts along the Mississippi and Elk River, and the Red River Trail, leveraging the confluence of each for travel and commerce. Over time, trading turned to farming, which eventually gave way to industry. Rail arrived in Elk River in the 1870's, and with it came a shift of the economic center of the city to "Lower Town", the current historic downtown area.

Shortly after World War II, regional and state-wide paved roadways were introduced to the city. Highway 10, originally called the Jefferson Highway, was constructed in its current location along the northern edge of the downtown core. A few years later, Highway 169 was introduced. Even in its early days the highway saw major weekend backups extending miles out of town.

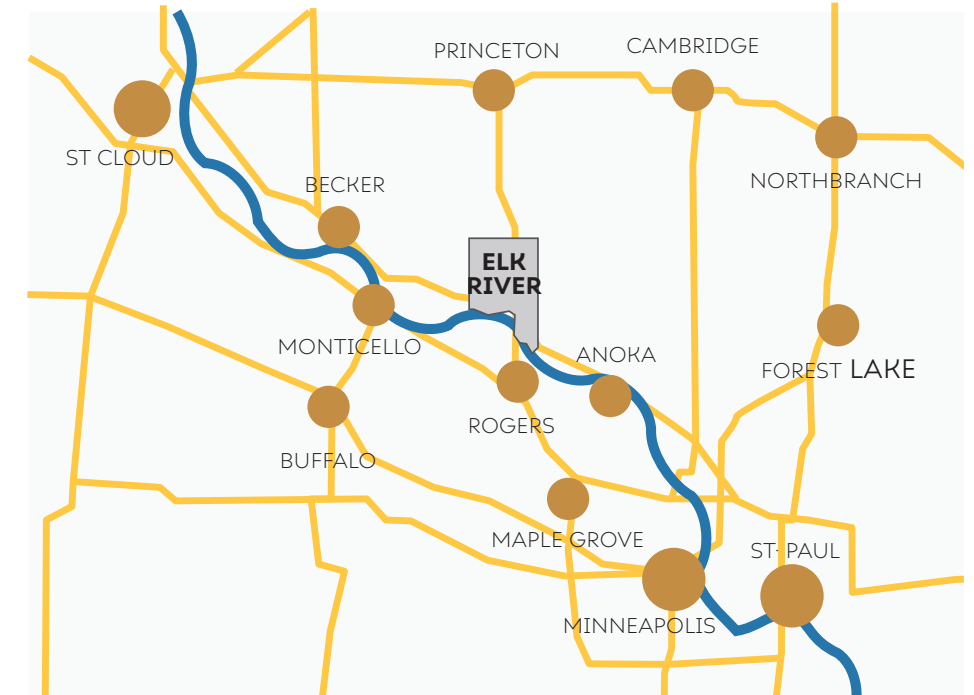
ENERGY CITY

Even before Elk River was established as a city in 1974, energy production played a major role in the regional economy. The first rural nuclear power station went online in the area in 1960, and subsequent energy innovations and sector growth help the city achieve the unofficial title of "energy city" in the late 1990's.

CITY AND COUNTRY FLOW TOGETHER

Elk River continues to embrace the converging of city and country within its borders. With near proximity to both St. Cloud and the Minneapolis/St. Paul Metro, Elk River is never more than a short journey away from city amenities. It also offers plenty of wide open country land for those seeking respite from city-living. With something for everyone, and anchored by the Mississippi and Elk Rivers, the city truly reflects the moniker of "Where City and Country Flow Together".

The Region



Elk River is just outside the official Twin Cities Metropolitan Region, however it relies on surrounding communities for much of its daily retail and dining needs. Located 30 miles from Downtown Minneapolis, Elk River is on the northern edge of the regions developed areas. Because of this, the City has adopted a tagline of "where the city and country flow together. Both of the city's major highways, Hwy 10 and Hwy 169, provide direct access from Minneapolis and St. Paul, north to regional centers of commerce and recreation in greater Minnesota.



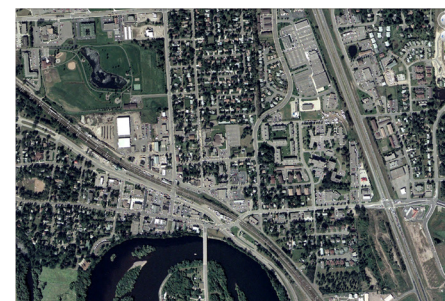
1943



1957



1991



2004



2004

DOWNTOWN ELK RIVER

Heart of the City

Downtown has been the economic and cultural heart of the city, ever since the first trains rolled into town in the 1870's. Today, the downtown core area - the area on Main Street west of Highway 10 - is a series of walkable blocks with main street commercial buildings and apartment housing. This commercial heart quickly transitions to a single family neighborhood to the west.

North of the core, is a mix of industrial and small commercial, eventually transitioning to a small residential neighborhood. To the east of the core then is a small commercial corridor along Main Street, connecting Highway's 10 and 169.

Downtown faces a series of infrastructure and natural challenges. Bounded by Highway 169 to the east, the downtown is bisected by Highway 10 and the rail line, right through its core. The difficulties each presents in terms of connectivity and development, cannot be overstated. Further, the Mississippi bounds the area to the south. While there are immense opportunities to better connect to the river, today, the downtown only minimally addresses the river.

Despite these challenges, there are numerous opportunities for the city to guide transformational redevelopment, promote enhanced multimodal connectivity, and provide destination public space experiences.



Scale of Opportunity

The scale of the downtown area is comparable to several other downtowns in surrounding communities. Additionally, its a size in which its core area is all within an approximately 10-minute walk - a standard measure for a comfortable walking distance.



Downtown Anoka



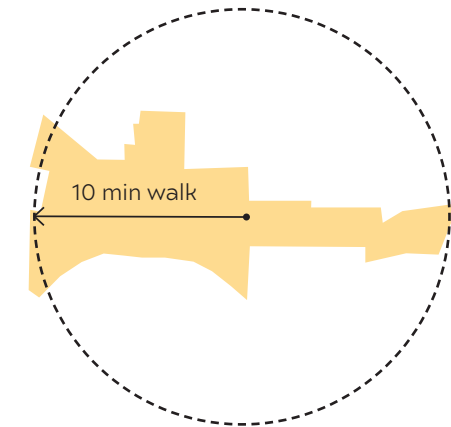
Downtown Monticello



Downtown Elk River



Downtown St. Cloud



DOWNTOWN ELK RIVER

Past and Current Plans

Zoning Code

Details allowable land uses, and site and building design guidance for Downtown, and immediately surrounding areas.

Downtown Traffic and Parking Study (2021)

Analyzed traffic and parking conditions within Downtown; Evaluated the pros and cons of transitioning Jackson Avenue to a one-way.

Highway 10 Corridor Study

The corridor study identifies a series of safety, access, and operational challenges for the Highway.

2040 Comprehensive Plan: Riverfront Planning

Outlines future guidance for Rivers Edge Park, and an ADA accessible seasonal dock to the River; Recommends additional connections to the river front.

Elk River Parks and Recreation Master Plan (2015)

Provides facility and programming improvement recommendations for downtown area trails, river access, and area parks.

Sherburne County Park, Trail, and Active Living Plan

Identifies conditions and needs for improved trail connectivity with Downtown and the Great Northern Trail and Mississippi River Trail; Downtown is within Sherburne County's Priority network corridor for improvements

2040 Comprehensive Plan Guidance

To guide city economic development actions, and promote cohesive developable areas, the city's 2040 Comprehensive Plan ascribed a series of character areas to the downtown. Guidance, including area-specific city policies and strategies, seek to promote job creation, enhance delivery of goods and services, and raise quality of life in each area.



Downtown Mixed Use



Uptown Mixed Use



Neighborhood commercial



Highway 169 commercial

Key Takeaways

- Much of the historic development pattern of downtown remains, though is constrained by natural features and regional infrastructure.
- Existing land use and character area guidance is as in alignment with desired uses in downtown, but may benefit from some additional refinement.
- Only about 1/3 of available parking downtown is utilized on an average day, however available parking isn't necessarily perceived as convenient or "near" destinations.
- Converting Jackson Avenue to a one-way street was determined to be more detrimental to downtown than beneficial.
- Continued expansion of public space and access to the river is critical to maximizing downtown's potential
- New trail connections can connect downtown with the Great Northern Trail, Lion John Weicht Park, Bailey Point Nature Preserve, and other regional destinations

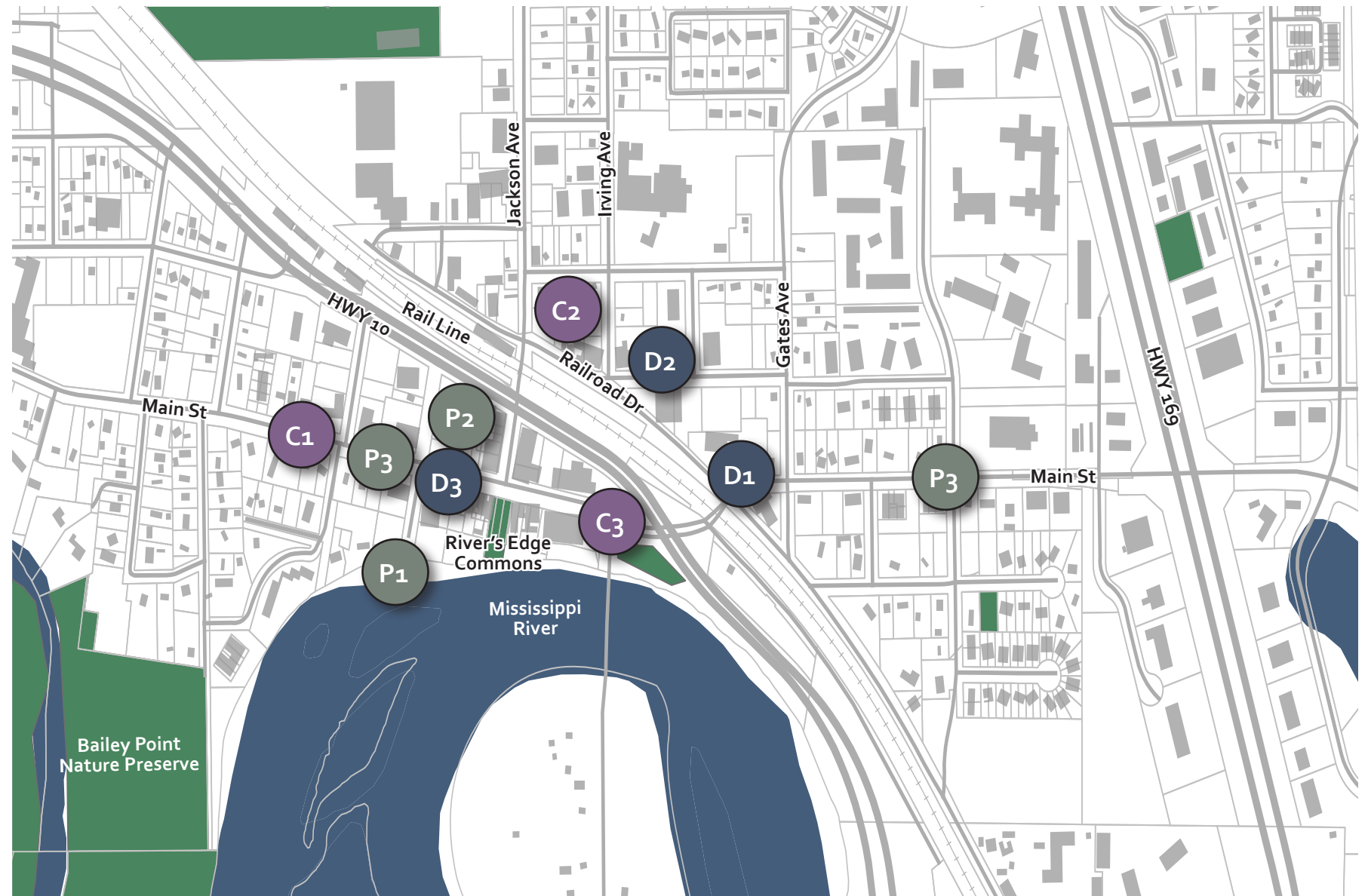
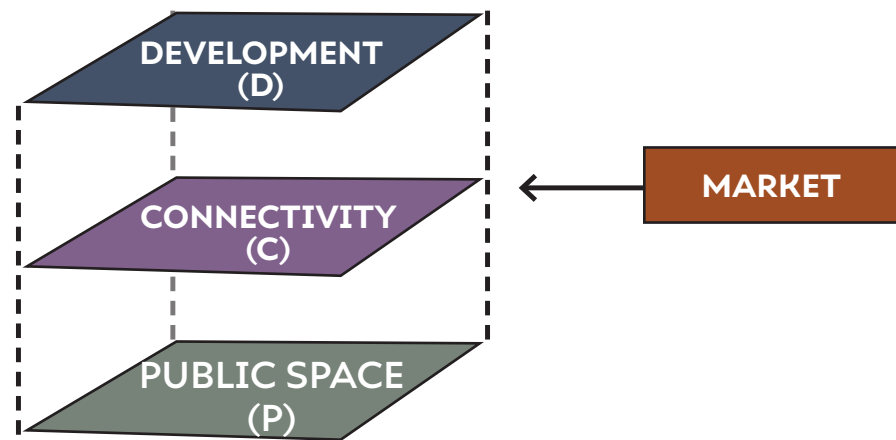


2 ANALYSIS

SYSTEMS OF ANALYSIS

The analysis of Downtown Elk River includes a review of Public Space, Connectivity, and Development. Understanding issues and opportunities through this holistic lens allows for targeted improvements, while recognizing their interrelated role each plays towards creating strong places.

The Plan is also supported by a market analysis. Findings from the market analysis further refined the analysis to identify and explore area recommendations.



KEY FINDINGS

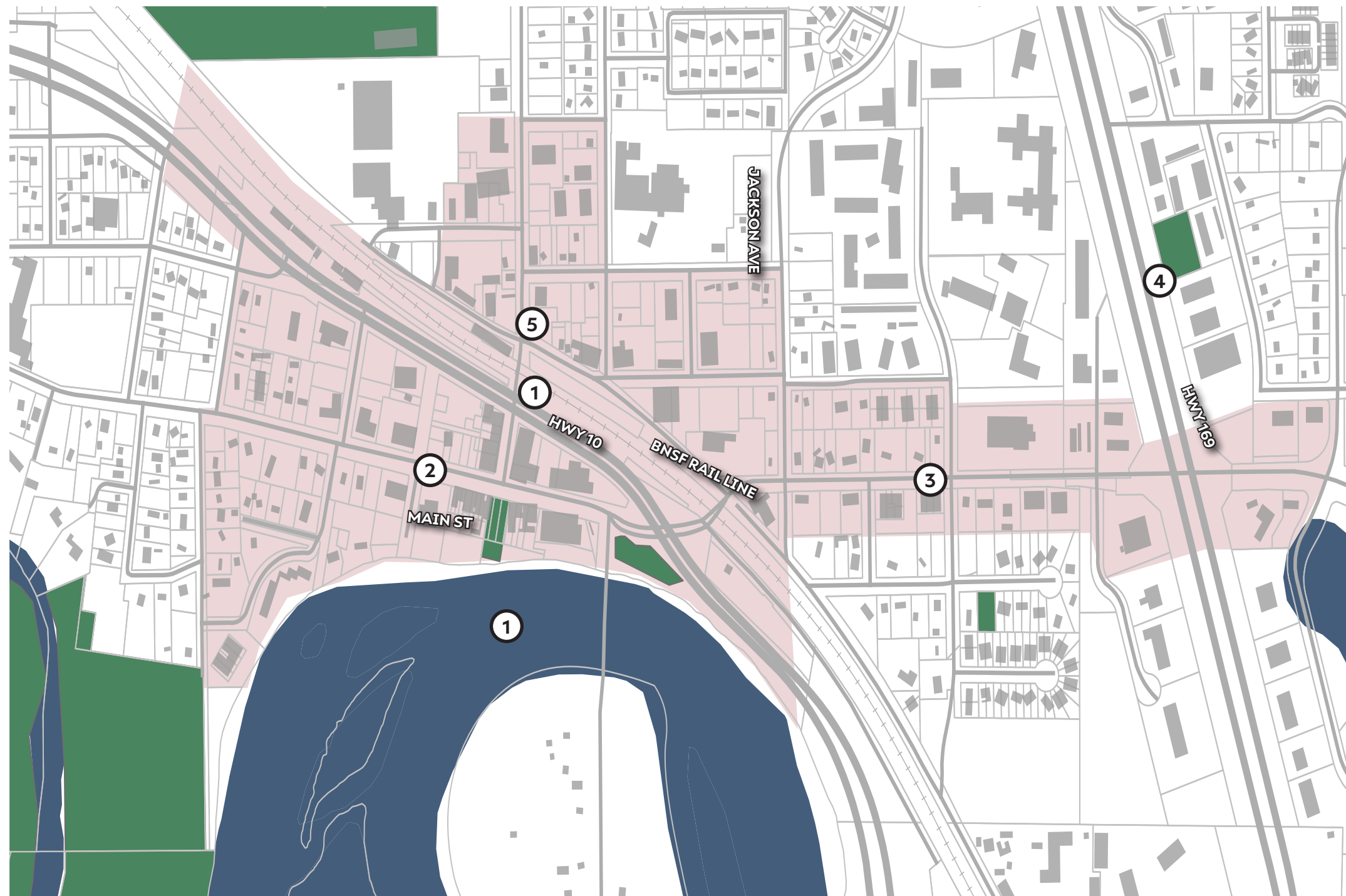
- D1 - Defined Development Character Areas
- D2 - Railroad Dr Redevelopment Opportunities
- D3 - Parking Lot Site, Church Site, Funeral Home Site

- C1 - Main St Design, Crossings of Hwy 10 & BNSF Rail Line
- C2 - Multimodal Connectivity
- C3 - Main St / Parrish Ave Intersection

- P1 - Leverage the River
- P2 - More and more frequent events
- P3 - Enhanced Main St pedestrian experience

DEVELOPMENT

Existing Conditions



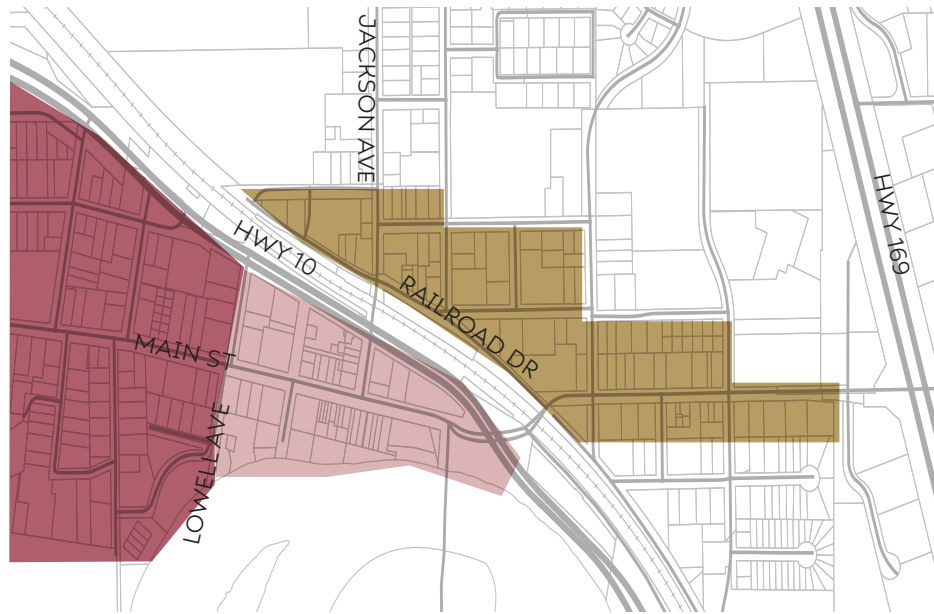
KEY FINDINGS

- ① Downtown is wedged between the Mississippi River, Highway 10, and the BNSF rail.
- ② The City grew up along the river, therefore the older building stock extends west from Downtown.
- ③ As Main Street extends east towards Hwy 169 it lacks a consistent development pattern.
- ④ Most of the City's commercial development is along HWY 169 and not Hwy 10.
- ⑤ Though the rail line predates most of the city's development, the block pattern on the north side of the rail line appears to have been disrupted by the rail alignment.

 Study Area

DEVELOPMENT

Key Findings



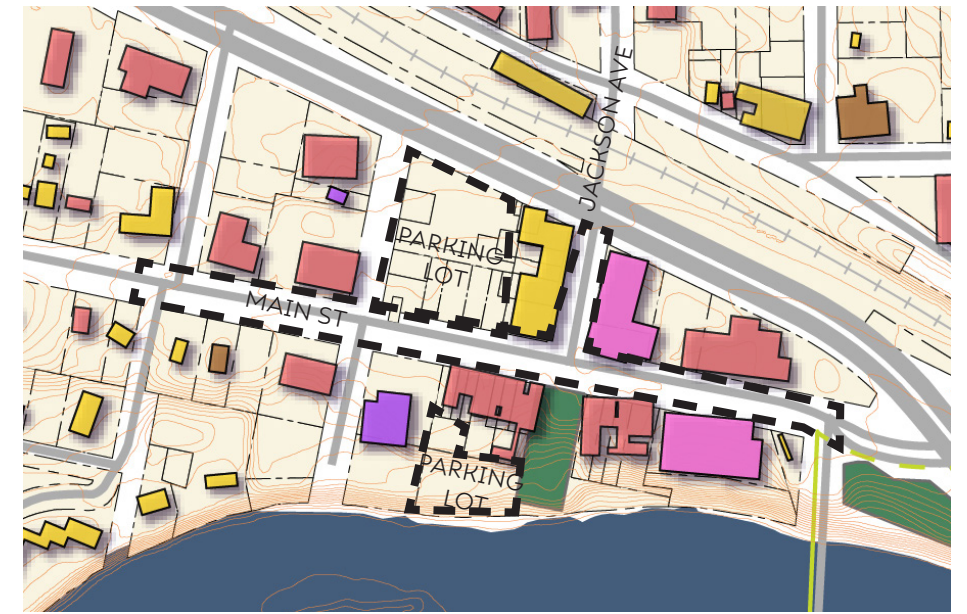
Development Districts Character Areas

- Though the rail line and Highway 10 disrupt the physical fabric of the community, they create the opportunity to clearly define different character areas in the downtown area.
- Main Street, between Lowell Avenue and Highway 10 is generally considered the core of downtown - consisting of mercantile, commercial, and mixed use buildings. The area is a center of the community - a place where a mix of uses come together.
- West of Lowell, the character changes to urban neighborhood - where housing is the dominant land use. The width of Main Street remains relatively wide, which is inconsistent with the residential character astride it.
- Uptown, the area north of Railroad Drive, has never evolved into a place with a consistent character. The uses are not related to each other and the street grid and block pattern have abrupt edges along the rail line.



Railroad Drive Opportunities

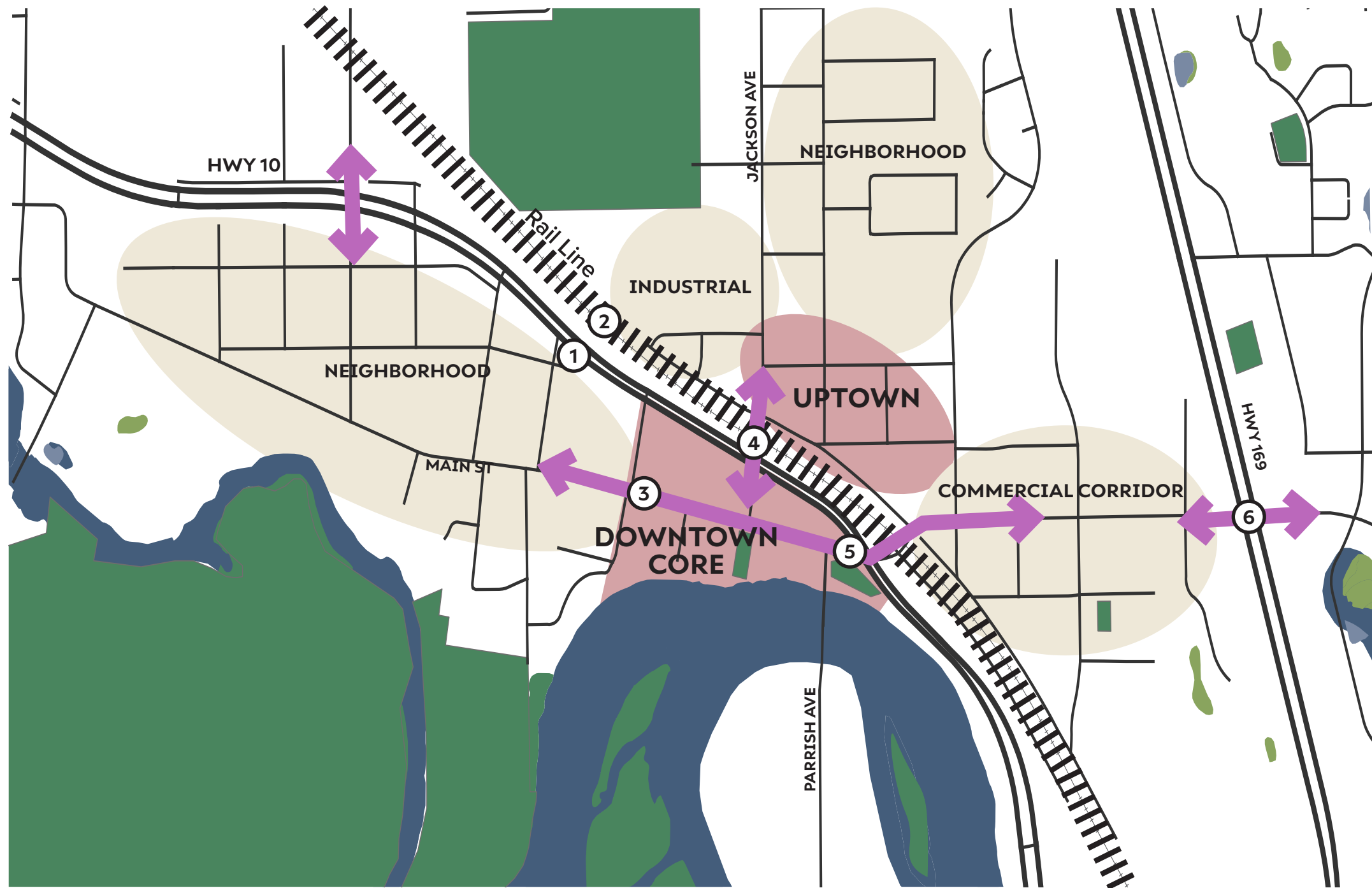
- The Main Street portion of downtown is relatively well built out, therefore there are only a few opportunities for redevelopment. Railroad Drive, on the other hand, presents significant opportunities for redevelopment.
- The properties are underperforming, in that land values exceed the building values - suggesting they are susceptible to change in the near term.
- Given the inability to add housing into the immediate Downtown, the Railroad Drive properties are well positioned for multi-family housing.



Key Downtown Sites

- The City owns several key properties Downtown - including the public parking lot, riverfront properties as well as the street ROW. The City should use these properties to advance the goals of the Master Plan
- The church and funeral home are legal non-conforming uses in the Downtown. They are permitted to operate however they are limited in their ability to expand in that location.

CONNECTIVITY

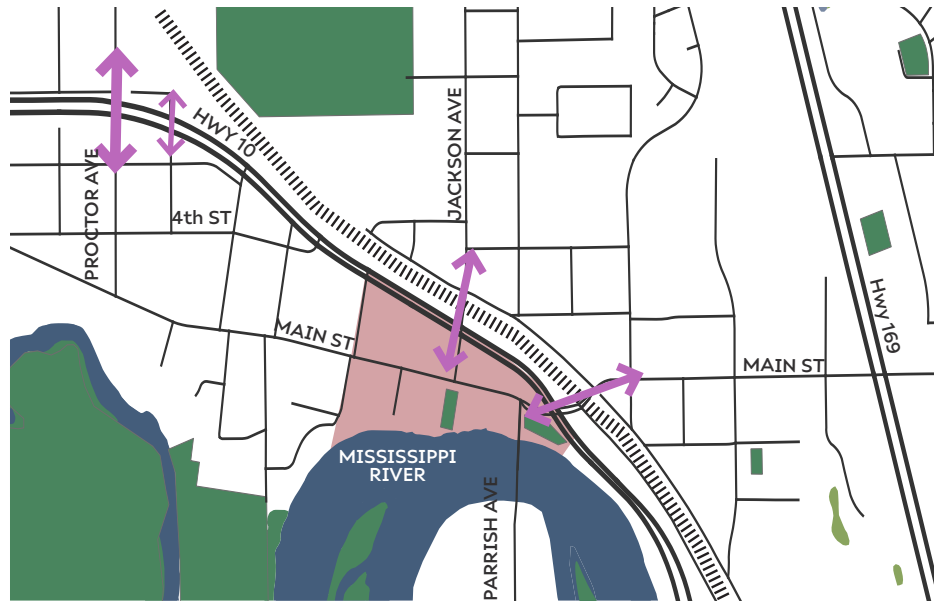


KEY FINDINGS

- ① Hwy 10 bisects the study area, but also offers visibility and regional access
- ② The rail line bisects the center of the downtown area, and is a major barrier
- ③ Main Street is the primary east/west connector in downtown, and the anchor to the local network
- ④ Jackson Ave is the only north/south connection over Hwy 10 and rail line
- ⑤ The Main St/Parrish Ave/Hwy 10/Rail Line intersections are very difficult to navigate by car, foot, and bike
- ⑥ Improvements to Hwy 169 should alleviate area traffic, while offering another means of getting to downtown

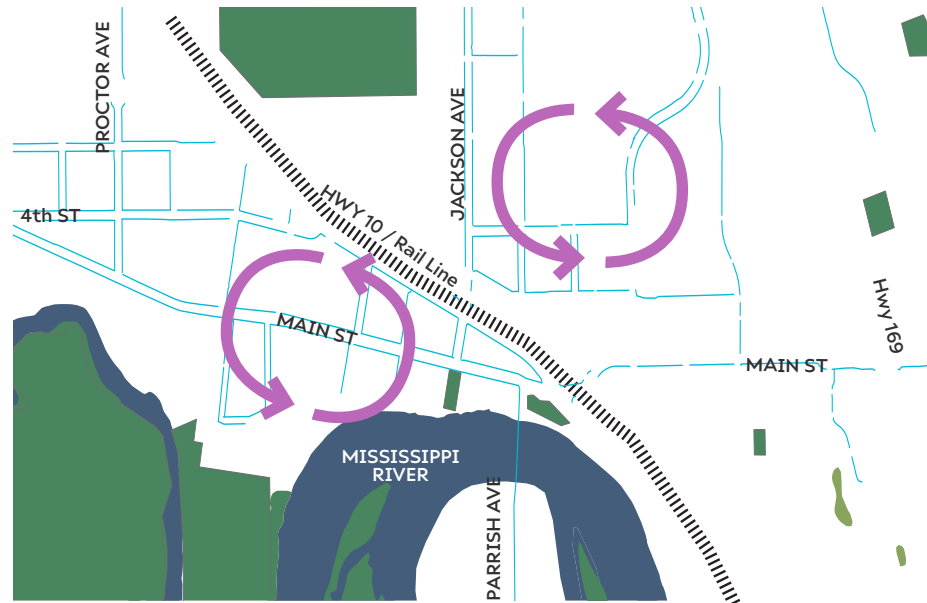
CONNECTIVITY

Key Findings



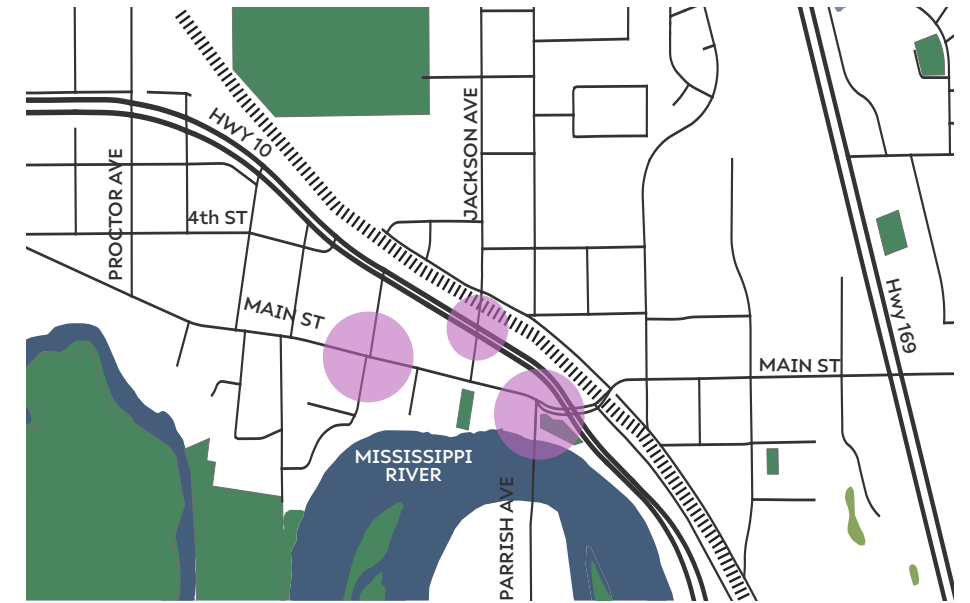
A Disconnected Downtown

- The Downtown Core area is severely isolated due to Highway 10 and the rail line to the north, and the Mississippi River to the south. Getting into and out of the area requires “funneling” onto Jackson Ave, Main St east, and Parrish Ave - all of which have difficult crossings.
- Crossing Highway 10 and the rail line is difficult, uncomfortable if walking or biking and limited in location - the only places to cross within downtown are along Main St or Jackson St. The presence of both make circulation around the area very difficult, particularly during events, trains, or summer weekend traffic.
- The generous roadway widths, angled parking, and modest streetscape along Main Street, don’t promote an attractive retail and pedestrian experience. Reconsidering the roadway design towards one with more space for pedestrians, could better balance the many competing needs within the downtown core.



More Places for Walking and Biking

- Walking within the downtown is generally concentrated within two, circular networks - one north of Highway 10 and one south. While the sidewalk systems is relatively well built in each, pedestrian connections between downtown and uptown are poor.
- There are currently no designated areas to bike into and around the downtown area, and wide vehicle lanes within downtown dissuades people from biking. Connecting downtown to area trails, particularly the Great Northern Trail, could bring another user group into downtown while promoting bike tourism.



Vehicle Spot Improvements

- Vehicle circulation into and within downtown is often very uncomfortable. This is especially true during large events, the many times per day that trains pass, and during summer weekend traffic.
- Spot improvements at the Main Street / Parrish Avenue intersection could help with better circulation at the already location, while offering places to loop back into downtown.
- Similarly, Lowell Avenue at the west end of downtown could be redesigned to offer a loop back opportunity into downtown.
- Spot improvements at Jackson Avenue and Highway 10 crossing could help with vehicle access and safer pedestrian crossings.

PUBLIC SPACE



KEY FINDINGS

- ① Main Street and Jackson Avenue are major public space amenities, but primarily serve vehicles
- ② The city-owned parking lot is a major event space for activities like the farmers market
- ③ Rivers Edge Commons is the premier public space downtown, but its programming is limited to concerts and performances
- ④ The riverfront has the potential to be a major downtown amenity and public space, but is largely disconnected from downtown.
- ⑤ Surrounding public space destinations like Bailey Point Nature Preserve are not well connected to downtown for people walking and biking

PUBLIC SPACE

Key Findings



Leverage the River

- The river is one of downtown's strongest assets, yet it is only experienced via River's Edge Commons and seasonal dock. Enhancing public space along the river and providing additional access and views, is a priority of this plan.
- Improvements along the river and connective public space, could better connect people to each other, the natural environment, and offering new recreational and event opportunities.



More and More Frequent Events

- The community expressed a strong desire for more downtown events, particularly for families. Such events bring the community together to celebrate and socialize downtown.
- The provision of public events will need to balance available city resources, and consider space and facility needs for these activities. If aligned with supportive public spaces downtown, additional events could bolster the role of downtown in civic life, while reconnecting people to downtown as a identity of the city.



Main Street and Jackson Avenue are the Key

- Main Street is the only east-west through downtown. It is also the primary connector and public space within downtown.
- Rethinking the design of Main Street and Jackson Avenue in the downtown core, would offer more space for pedestrian activity, eating and shopping, and events, all while improving multimodal connectivity in downtown.

MARKET SUMMARY

Demographics

	Downtown	Elk River	Sherburne County	Twin Cities Metro Area	Minnesota
Population	262	27,393	102,251	3,790,663	5,826,205
Population Growth, 2010-2024	0.4%	19.2%	15.5%	13.7%	9.8%
Avg. Ann Growth Rate	0.03%	1.4%	1.1%	1.0%	0.7%
Number of Households	134	9,725	36,136	1,479,626	2,309,848
Avg. HH Size	1.84	2.70	2.76	2.52	2.46
Housing Units	145	10,097	37,912	1,557,248	2,556,750
% Renter	55%	20%	16%	30%	27%
Median Household Income	\$43,414	\$100,945	\$101,911	\$96,864	\$86,801
Median Per Capita Income	\$30,479	\$42,879	\$43,615	\$50,717	\$46,714
Median Age	42.7	38.2	37.6	38.1	39

- 262 people live in the Downtown study area, making up just under one percent of the total population of 27,393. Between 2010 and 2024, population growth in Elk River outpaced county- and region-wide growth. The median per capita and household incomes are lower in Downtown than region wide due to the high concentration of affordable housing in Downtown Elk River.

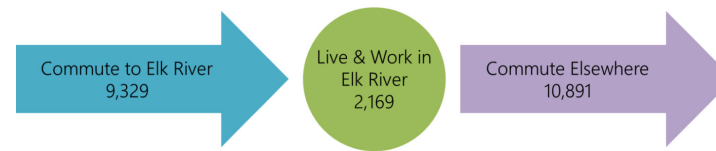
Income Distribution in Downtown and Elk River



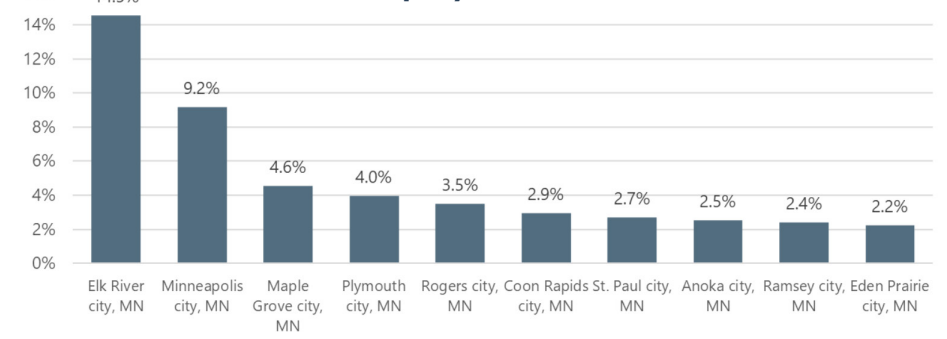
- There is a higher concentration of lower-income households in the Downtown study area, largely due to the fact that nearly a third of multifamily housing units in this area are regulated affordable housing. Along with Jackson Place, the 23-unit Elk River Senior Apartments are affordable. However, the high concentration of upper-income households citywide suggests that there could be an opportunity to add higher-end housing and retail to Downtown.

Employment and Commuting

- Retail trade, manufacturing, health care and social assistance, and educational services dominate the Elk River employment sector.
- Between 2012 and 2021, construction, manufacturing, accommodation and food services, and retail trade saw the biggest job gains among employment sectors.
- Over that period, the city gained 722 new jobs, a 6.7% increase in overall employment. Manufacturing and retail are the biggest employment sectors in Elk River, employing over 1,800 workers each and accounting for nearly two thirds of total employment citywide.
- Manufacturing sector employment in Elk River grew by 45% between 2012 and 2021 while retail grew by 23% percent.



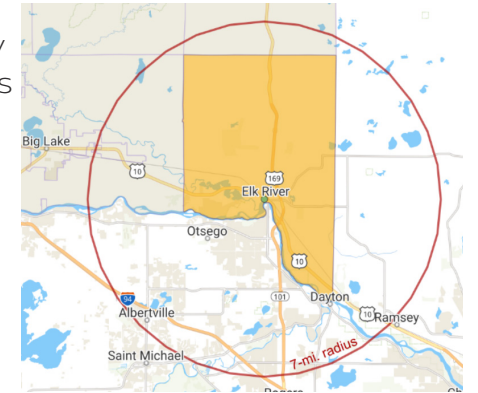
Location of Employment for Residents of Elk River



- Elk River has a total of 11,498 jobs, 81% of which are filled by workers from outside of the city limits (including unincorporated areas). Nearly 11,000 workers who live in Elk River commute to other communities for work. Just 2,169 (17 percent) of workers who live in Elk River work within city limits.
- Among workers who live in Elk River, 14.5% work in the city and nine percent work in Minneapolis, while 52% of Elk River workers commute to other locations, including unincorporated areas.

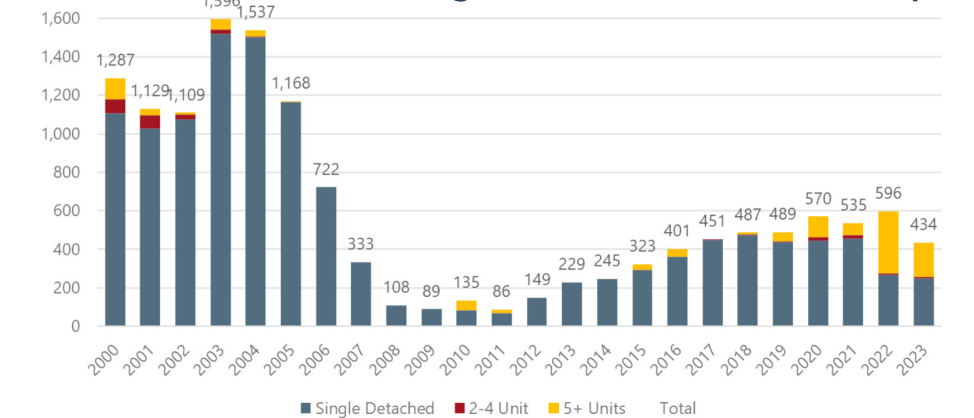
Local Market Conditions

- Residents of Elk River and neighboring cities typically cross municipal boundaries to meet their daily needs. To understand the overall market dynamics in Elk River, the Plan analyzed a seven-mile radius from Downtown Elk River, which includes parts of Sherburne, Anoka, and Wright counties. Within this area, there are 8.3 million square feet of industrial space across 255 properties, with an additional nine proposed properties expected to add two million square feet of industrial space to the market. Multifamily



and retail are also among the most common use types by square feet, though there are more office properties than multifamily in this area. The relatively small office spaces support nonprofit offices, service providers like insurance agents, and mental health care providers.

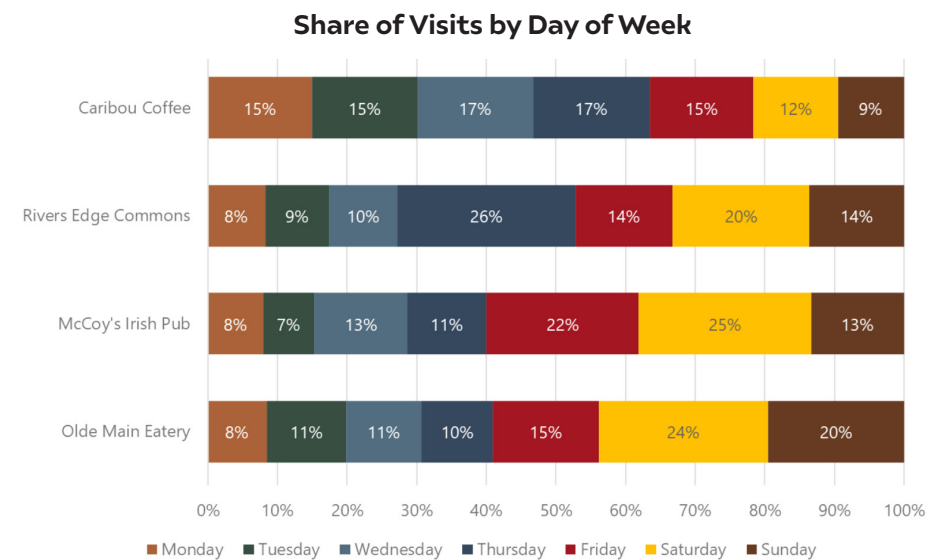
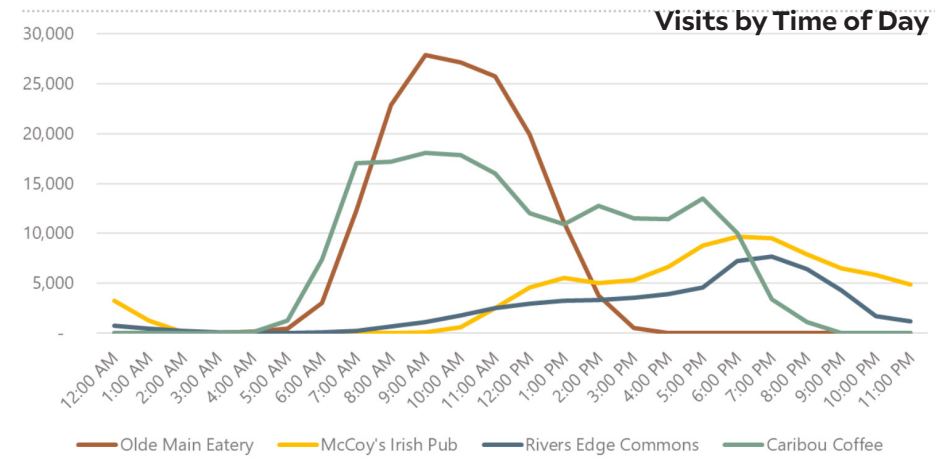
Housing Permits in Sherburne County



- Permitting has not returned to pre-Great Recession levels. While between 2000 and 2006, Sherburne County permitted an average of over 1,200 units per year, between 2017 and 2023 the county permitted an average of around 500 units per year. In 2022, 596 units were permitted, over half of which were in structures with five or more units. It does not appear that the COVID-19 pandemic has had a significant negative impact on permitting.

MARKET SUMMARY

Visitor Dynamics



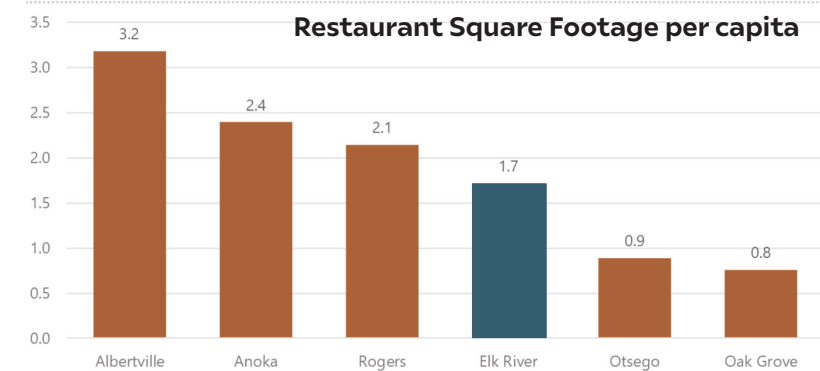
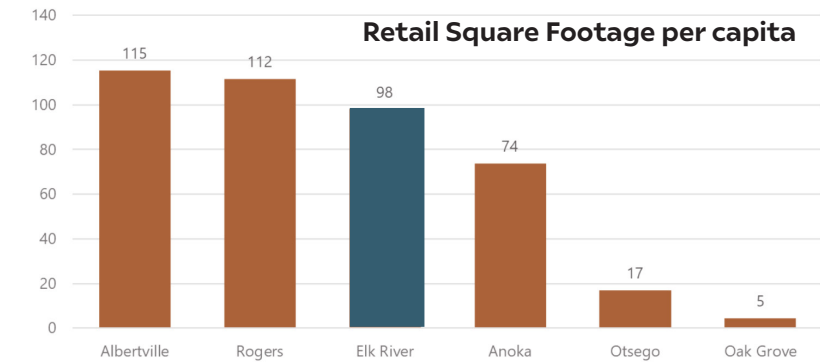
- Visits to Olde Main Eatery peak between 9 and 11 AM, while peak hours at Caribou Coffee are longer, lasting generally between 7 AM and 5 PM. Most visits to the Rivers Edge Commons and McCoy's Irish Pub are in the evening hours.
- Visits to Olde Main Eatery and McCoy's Irish Pub are concentrated on weekends, while Caribou Coffee attracts more weekday traffic. This is likely due to the fact that Caribou Coffee caters to drive-through customers on their way to work. Over a quarter of visits to Rivers Edge Commons are on Thursdays, when summer programming brings residents to the waterfront for concerts and festivals.

Retail Gap Analysis

Supply and demand for Selected Retail Categories within a Five Minute Drive of Downtown Elk River

	5-Minute Drive		
	Demand	Supply	Unmet Demand
Full-Service Restaurants	\$5,020,535	\$2,279,045	\$2,741,490
Beer, Wine, & Liquor Stores	\$1,249,762		\$1,249,762
Sporting Goods, Hobby, & Musical Instrument Stores	\$1,020,322		\$1,020,322
Other Miscellaneous Store Retailers	\$866,695		\$866,695
Furniture Stores	\$847,627		\$847,627
Home Furnishings Stores	\$746,922		\$746,922
Bars/Drinking Places (Alcoholic Beverages)	\$738,332		\$738,332
Limited-Service Eating Places	\$3,624,942	\$3,266,433	\$358,509
Office Supplies, Stationery, & Gift Stores	\$311,033		\$311,033
Shoe Stores	\$308,173		\$308,173
Electronics & Appliance Stores	\$1,466,148	\$1,271,890	\$194,258
Used Merchandise Stores	\$207,068	\$372,488	(\$165,420)
Jewelry, Luggage, & Leather Goods Stores	\$267,834	\$545,829	(\$277,995)
Florists And Miscellaneous Store Retailers	\$134,906	\$1,044,984	(\$910,078)
Book, Periodical, & Music Stores	\$163,044	\$1,645,774	(\$1,482,730)
Clothing Stores	\$1,689,441	\$3,305,221	(\$1,615,780)
Health & Personal Care Stores	\$4,360,267	\$8,473,176	(\$4,112,909)
Other General Merchandise Stores	\$6,715,224	\$11,155,648	(\$4,440,424)
Grocery Stores	\$8,949,073	\$27,873,802	(\$18,924,729)

- The category with the highest unmet demand within a five-minute drive of Downtown Elk River is full-service restaurants. This aligns with information provided by shareholders, who specifically stated the need for family dining as well as dining options open late enough for visitors to summer programming at the river. There is also an unmet demand for full-service and limited-service restaurants within a 20-minute drive of Downtown.



- Although Elk River has a high concentration of retail and service sector employment, it does not have the same concentration of eating and drinking establishments as peer cities. In Albertville, there are three eating and drinking establishments for every thousand residents, while in Elk River there are fewer than two.

KEY TAKEAWAYS

The Center of The City.

The role of Downtown is not necessarily to be the City's economic engine or a competitor to big-box anchored shopping centers, but rather to be a point of pride for the community and an amenity for residents, employees, and businesses. A healthy and thriving downtown communicates that the community is valued and helps to attract residents and businesses.

A Strong Foundation

Elk River is a small but growing city with an active Downtown and supportive business community. From an occupancy and placemaking perspective, Elk River's Downtown is relatively healthy, especially for a town of its size.

Creative Opportunities

Post-COVID work-from-home trends in the office sector are causing problems for central downtown office and retail vitality. Those same trends, however, present an opportunity for outlying and suburban communities that combine high quality of residential life with a well-rounded offering of amenities and services. Downtowns like Elk River's stand to benefit from the increased daytime "body count" to support walkable retail, dining, and small office spaces, especially with complementary residential options.

Leverage Continued Growth

Elk River is growing rapidly, with an average growth rate of 1.4 percent annually between 2010 and 2024. Although residential permitting is below pre-Great Recession levels, this level of growth will continue to fuel a need for housing, requiring a combination of single-family homes and apartments. Downtown could be a key location for new apartments, but more everyday amenities will likely be needed to support Downtown residents. This could include small-format grocery or convenience stores, pharmacies, health clinics, and other necessities.

Introduce More Uses

Downtowns are constantly improving and changing to meet resident needs. Adding more housing and mixed-use development proximate to Downtown and improving the connection between Downtown and the riverfront would help draw more visitors and support local businesses.

Events and Business Hour Alignment

Most visits to Downtown Elk River are evenings during the weekends, as well as Thursday nights in the summer when there is programming, including the Farmers' Market and concerts at the riverfront. However, many of the stores, restaurants, and offices Downtown are not open into the evening hours. While there are limited spaces currently available for new family-friendly dining and entertainment, there could be an opportunity for the City to allow temporary or mobile vendors, like food carts.

Leverage the River for Unique Amenities

Downtown currently lacks a suitable site to build a competitive hotel. However, if riverfront properties currently occupied by the church and funeral home could be assembled at some point in the future, a boutique hotel as part of a mixed-use development could fill a high value niche in a market with little variety in current supply.



3 PLAN PROCESS

THE PROCESS

Project Goals

The Downtown Elk River Master Plan establishes a refined vision and set of implementation steps for Downtown and the immediately surrounding area. This vision builds on the City's 2040 Comprehensive Plan, while reflecting a deeper analysis of downtown's issues and opportunities.

The Plan is guided by a set of Goals to promote a place that is unique to Elk River—one that affirms the historic center of economic and social activity that Downtown played for the City, while affirming a need to continue evolving into a modern center.

It is envisioned that much of the existing fabric of Downtown will generally remain in place - the current roadway network, the types of land uses allowed, and the majority of the area's public spaces. However, a series of targeted interventions are suggested that build on this foundation towards a more robust destination and year-round place of social and economic activity.

A Market-Supported Vision

The future vision of downtown should be aspirational, while supported by current and anticipated market realities. Implementation steps should balance city resources with long-term returns on public investments.

A Public Realm That Supports Businesses and Events

Downtown's public realm is the lifeblood of gathering and activity. Improvements to the Public Realm must continue to support current downtown events and businesses, who rely on the public realm for their success. In turn, improving these spaces must set the stage for strong events and business growth.

Balance Evolution with Predictability

Continued business success in downtown will require a level of predictability in the pace and scale of change. Future improvements downtown should be communicated proactively and regularly with downtown business owners. In doing so, local businesses can become active partners and champions of change.

A Place People Can Comfortably Get To, And Get Around

Getting to downtown is critical considering the constraints of adjacent roadways, rail, and the river. Equally as important are clear ways to move around downtown once you've arrived. Downtown must therefore provide comfortable means of driving, walking, and biking to and around downtown for it to continue being successful.

Appropriately-Scaled Development

The scale of future development in and around downtown must complement existing development, offer transitions in height and scale, and support the city's long-range vision.

Events and Businesses that Bring People Downtown

The busiest days in downtown are driven by events. The continuation and growth of downtown events should be paired with destination businesses that encourage people to visit the area more often, and extend their stay while downtown.

ENGAGEMENT AND STEERING COMMITTEE

Project engagement was conducted using a two-phased approach. This provided for initial, broad input gathering from the public in Phase 1, followed by a feedback loop in Phase 2 to confirm findings and share recommendations.

PHASE 1

- Project Website with project details and information
- Online public preference survey (362 respondents, see Appendix A for results summary)
- Downtown Farmers Market pop-up
- Elk RiverFest pop-up
- Street decals about the project throughout downtown

PHASE 2

- A project overview video that summarized key plan elements
- Public Open House at City Hall that presented key recommendations and outcomes of the plan.

The project was guided by a steering committee comprised of city staff, downtown business owners, and local stakeholders. The steering committee met monthly to review public feedback, evaluate opportunities, craft recommendations, and establish an implementation plan.

WHAT WE HEARD

- The public deeply values downtown, particularly its businesses and events.
- Free or low-cost activities downtown draw the most people, particularly where there are things to do for young families and kids - something that is missing elsewhere in the city.
- Downtown should continue to attract more businesses, more places to gather, and better views and access to the River.
- Parking, or at least the perception of available parking, is a concern for the public.
- Within downtown itself, people did not want to see more housing, particularly if it comes at the expense of more stores and restaurants.



Downtown plan engagement at the Farmers Market (left), and Elk Riverfest (right).



Street decals about the project downtown

CONCEPT EVALUATION

Development

Downtown

The steering committee explored and evaluated redevelopment concepts on two, key sites in downtown - the city-owned parking lot north of Main St, and the Elk River Lutheran Church property, should redevelopment ever occur on the site. The church is expected to continue being a valuable member of downtown, and as such, this plan does not anticipate a specific timeline for that property.

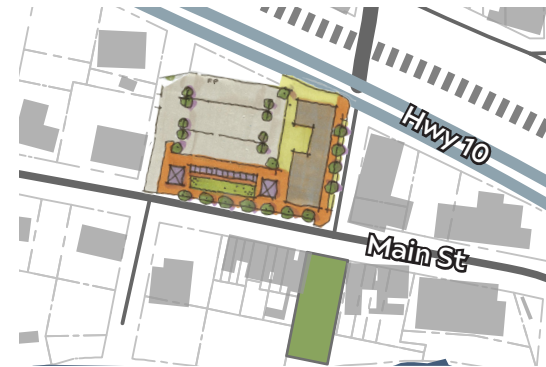
Concepts explored on each site included:

- What an appropriate balance of development, public space, and parking may be on the city-owned parking lot.
- The amount and type of new public space that should occur along Main Street, what public improvements should be done to the Riverfront, and how might these spaces relate to each other.
- How much development can be along Main St and what uses should it include.
- Near and medium-term frontage or public realm improvement opportunities for the Church property, even while they remain at that site.

Downtown Development Guidance

- Any development on the city-owned parking lot should prioritize active ground floor uses along Main Street, and continue the character of downtown.
- A small green or public space on the parking lot site was desired, as a compliment to Rivers Edge Commons, and to continue hosting the Farmers Market
- Development on the city-owned parking lot must balance available parking throughout downtown.
- Any future development south of Main Street should ideally have a presence on Main Street, without turning its back to the river.
- The riverfront experience should be improved, but must be mindful of access and program needs of the Rivers Edge Commons, the visibility of new amenities to promote use and safety, and have clear physical and visual site lines to Main Street.

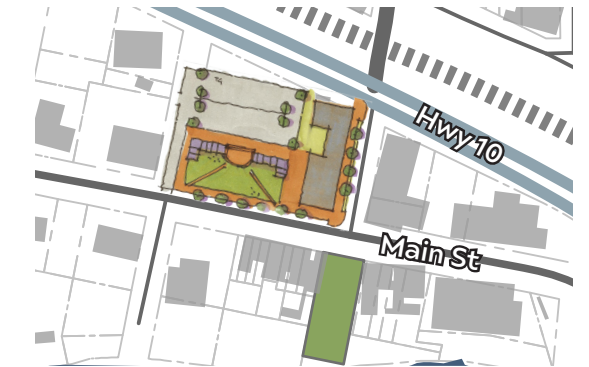
DOWNTOWN DEVELOPMENT CONCEPTS | City-Owned Parking Lot Development and Public Space



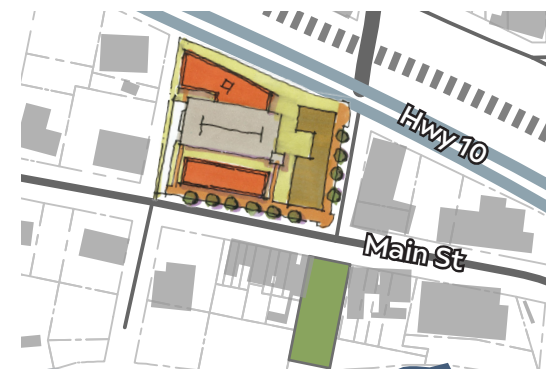
“Front Porch”: Small green space with parking area



“Marketplace”: year-round space for Farmers Market, pop-up events



“Town Green”: Larger outdoor space for events and gathering



“Double Liner”: A central parking ramp with adjacent development

DOWNTOWN DEVELOPMENT CONCEPTS | Riverfront Development and Public Space



“Riverfront Concessions”: Modest development with a destination riverfront



“Welcoming to the River”: Large development oriented to the river, with modest park space

CONCEPT EVALUATION

Development

Uptown

The steering committee also explored and evaluated a series of redevelopment blocks north of Highway 10 and the BNSF rail line - termed "Uptown" during the planning process. Recognizing the comparatively larger area of potential redevelopment sites, this area was looked at holistically as compared to the site-specific approach to Downtown.

In Uptown, the development concept explored:

- What an appropriate balance of residential and commercial uses should be, and where each should be located
- The types, intensity, and general arrangement of residential uses throughout Uptown
- The viability of a grocery or convenience store, and a centralized small commercial "node" at the railroad drive roundabout.

Uptown Development Guidance

- Redevelopment in Uptown should be primarily residential, and allow for a diversity of housing types, scales, ownership, and price points.
- Denser development should be located along Railroad drive, and at major intersections.
- As feasible, a small or medium grocery or convenience store was seen as a good fit for the Uptown area.
- Closing Railroad Drive at Main Street could promote safer circulation at the rail road crossing area, but would require shifting access to Gates Ave.

UPTOWN DEVELOPMENT CONCEPT



Mix of housing types, price points, and ownership, with modest targeted commercial uses.

CONCEPT EVALUATION

CONNECTIVITY

Rebalancing Street Design

The steering committee considered rethinking streets, and the balance of how the public right of way is utilized for vehicles, people, business activities, and public realm enhancements.

For Main Street and Jackson Avenue downtown this included:

- What an appropriate balance of streets, parking, and space for pedestrians may be.
- How to promote more space for pedestrian and economic activity, like outdoor places to walk, eat, shop, and gather.
- For Jackson Avenue, how to improve business access along the block, which is currently ramped on the east-side of the street, with a contiguous railing.

For Main Street east of Highway 10 this included:

- With a limited available right of way, what improvements might be viable, particularly those that improve and create pedestrian space on the north side of the street and a more welcoming environment into the downtown study area.

Connectivity Guidance

- Within Downtown, the usage of the ROW should be rebalanced towards serving pedestrians, events, and street-level economic activity.
- On Main Street and Jackson Avenue there should be more trees and places to sit, while providing space for outdoor dining and selling of goods.
- On Jackson Avenue, improvements should seek out opportunities to allow for mid-block access along the western ramp.
- Main Street "east" should bury utilities, add more space for people to walk, and change the lighting to a more visually attractive style.

MAIN STREET DOWNTOWN CONCEPT (80' Right of Way)



Very large space for cars (60'), limited sidewalk space (10' each side)



Parallel parking on both sides, allowing 30' to be reallocated to sidewalks

JACKSON AVE CONCEPT (80' Right of Way)

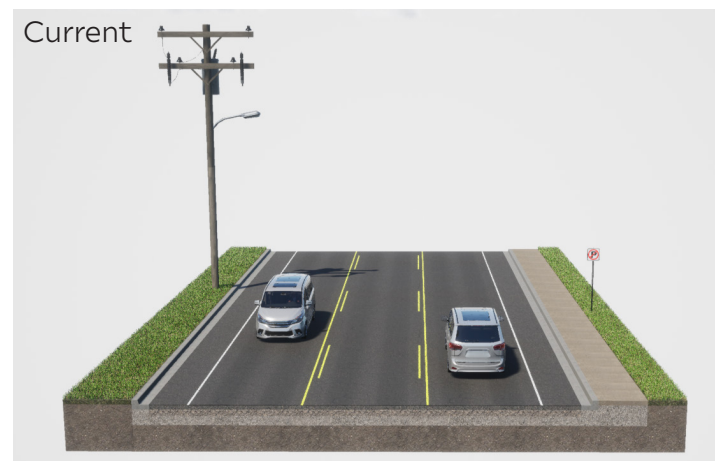


Very large space for cars (56'), limited sidewalk space (12' each side)

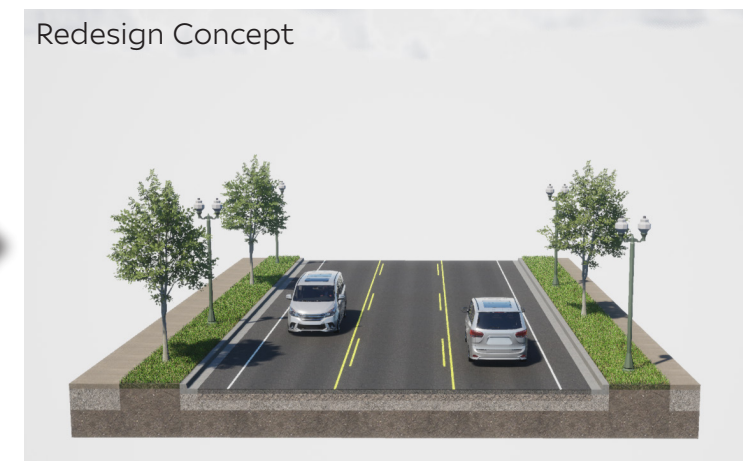
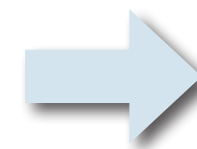


Parallel parking on both sides, allowing 16' to be reallocated to sidewalks

MAIN STREET EAST (65' Right of Way)



Appropriate space for cars, with a single sidewalk and utilitarian utility poles and lighting



Reduce travel and turn lane widths, allowing 8' to be reallocated for another sidewalk and boulevard trees

CONCEPT EVALUATION

Public Space

More Places to Gather and Play

Public space improvements were considered along Main Street, within the city-owned parking lot, and the Riverfront. These spaces were recognized for their continued importance of hosting events, attracting people downtown, and supporting a strong sense of place to support downtown businesses.

Public space considerations explored:

- The size, location, and design of any new public space in the city-owned parking lot
- Opportunities for improving the Riverfront's access, visibility, and design, including trails, event space, natural areas, and navigating grade down to the river.
- The relation of future public spaces downtown to Rivers Edge Commons.
- How sidewalks and the street could better support area businesses and events.
- The recognition that new public spaces in Uptown will likely come about through partnership with private developers.

Public Space Guidance

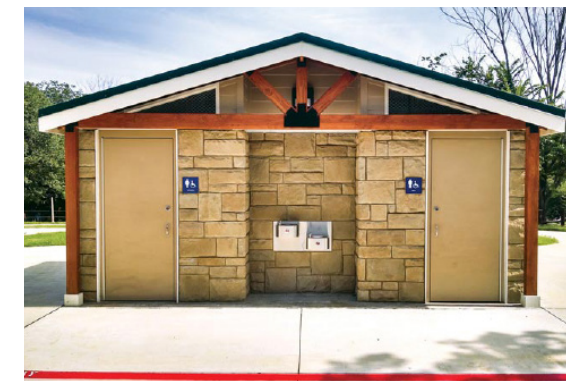
- The Farmers Market should continue to be hosted either in the city parking lot, or along Main Street. While a modest host structures could be of benefit, public space design and programming flexibility is important.
- Any new public space on Main Street in the city-owned parking lot should be modestly sized, while accomodating new development and public parking.
- While the riverfront could host some additional programming, most of this activity should remain along Main Street and Rivers Edge Commons.
- Any gazebo, bathrooms, or other structures along the riverfront should be designed with clear site lines to Main Street to promote use and safety.
- New public spaces in Uptown will be opportunity-driven, and in partnership with future private development.

MAIN STREET PUBLIC SPACE



Exploration of a large public green space (above), smaller covered market (top right), and covered streetscape features (bottom right)

RIVERFRONT IMPROVEMENTS

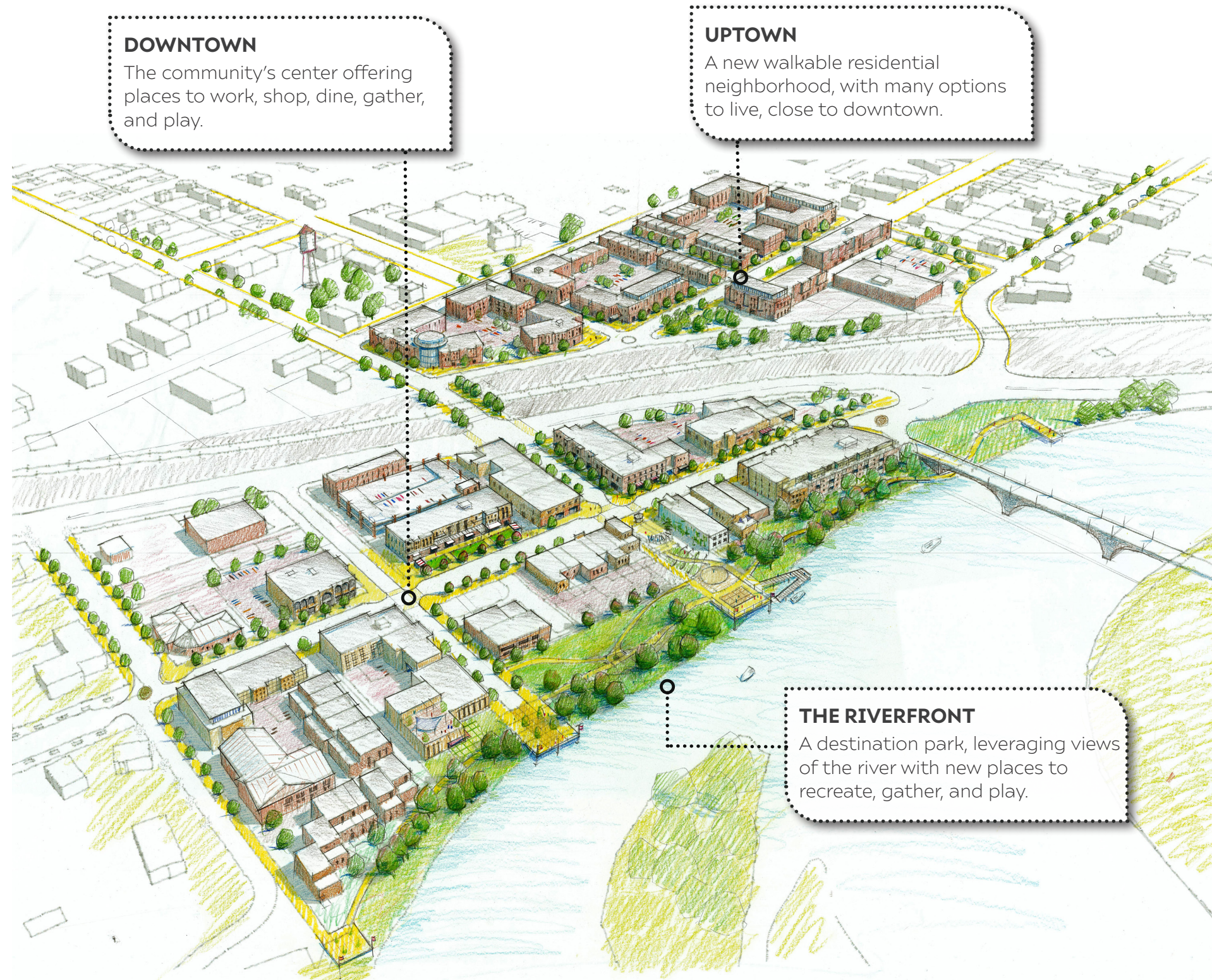


Riverfront improvements like trails (left), development-driven trails (middle), or pop-up vendor space (right)



4 RECOMMENDATIONS

THE PLAN



DOWNTOWN

Downtown Elk River has a strong foundation in which to build on. In leveraging its many assets - the business community, Rivers Edge Commons, year-round activities, and highway visibility - downtown will further assert itself as the destination center of the city.

Improvements to the street and sidewalk will provide a public realm that both compliments downtown businesses, while creating flexible space for events to naturally expand into. Targeted and strategic redevelopment will expand existing street activity, while offering more places to work and live. Similarly, as improvements to the river are incrementally accomplished, new development will be drawn to the views and access of the river. Elsewhere, events and activities will continue to grow, offering families, residents, and visitors more things to do together throughout the year.

Access into and around downtown will be enhanced by carefully implemented improvements along Main Street and Jackson Avenue. Strategically located gateway projects will provide people "loop backs" through downtown, addressing the challenges of a linear downtown, hemmed in by the highway and river.



Downtown will continue to evolve into the center of the city's identity, offering year-round activities, commerce, and connections with friends and neighbors.

DOWNTOWN

City-Owned Block

The City owns a 2 acre site in the middle of downtown. The site currently hosts a 160 space surface lot, and a small 4,000 sq ft turf green space.

Future development of the site must balance the creation of an active retail frontage along main street, the provision of a publicly-accessible public space, and preservation of at least a portion of the site for public parking. An east/west oriented parcel (PID 75-00405-0240) in the center of the site could provide a convenient delineation between the development and parking areas, while maintaining vehicle access to existing development on the west side of Jackson Ave.

As the city prepares for a future development RFP, it should remain flexible on the exact development and public space arrangement. Some options shown here illustrate how development and public space programming could be arranged.

Key Outcomes

- New publicly accessible park along Main St, that can accommodate the farmers market and other events
- Active ground floor with direct access onto Main Street
- Pedestrian-scale design and frontage along Main Street
- Above-floor office or residential uses
- Maintain ability for future structured, shared parking ramp
- Explore the viability of a linear development along Highway 10
- A publicly-accessible restroom for the public space

DEVELOPMENT VISION

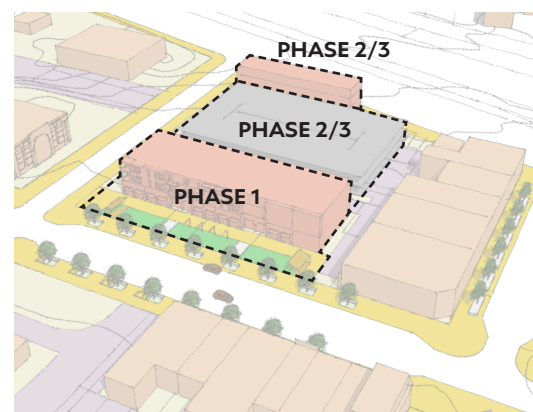


POTENTIAL DEVELOPMENT OUTCOMES

- 8,000-12,000 sq ft ground floor retail space
 - 20-25 new housing units or 16,000 - 24,000 sq ft new office space
 - 6,000 - 10,000 sq ft public space to gather
 - 80-160 parking spaces (pending future structured parking)
- 1 Publicly-accessible green space along Main Street
 - 2 Mixed-use building, with active ground floor and office or residential above ground
 - 3 Surface, or future structured parking
 - 4 Space for future "linear" development
 - 5 Maintain access to existing building, with rear alley

SITE PHASING

The initial development may not be able to deliver structured public parking or linear development, so the city should anticipate multi-phased site improvements. Development on Main Street should ensure feasibility of future site improvements.



DEVELOPMENT ALTERNATIVES

The city should maintain an openness to working with a private developer on a feasible development plan that meets city goals. Recognizing this, other development alternatives were explored that could also meet the needs and expectations of the community.



DOWNTOWN

Elk River Lutheran Church Site

Elk River Lutheran Church is a strong downtown partner, and is an amenity for the community. The City and downtown stakeholders should continue to partner with the church towards opportunities for continued local partnerships

If the church ever desires to relocate, the site could play an important role in the continued evolution of downtown business and public space improvements.

A mixed use project that offers commercial uses along main street, residential, office, or event uses in the rest of the building, and extends park space along the River, could be a unique site that leverages both the River and Main Street.

Key Outcomes

- Establish a shared parking agreement on the back surface lot, for public use
- Coordinate on planned frontage and site improvements along Main Street, to reduce vehicle access points onto Main Street, create places to sit, and provide vegetation or a small greenspace
- Continue to work with the church as a strong partner and amenity within downtown

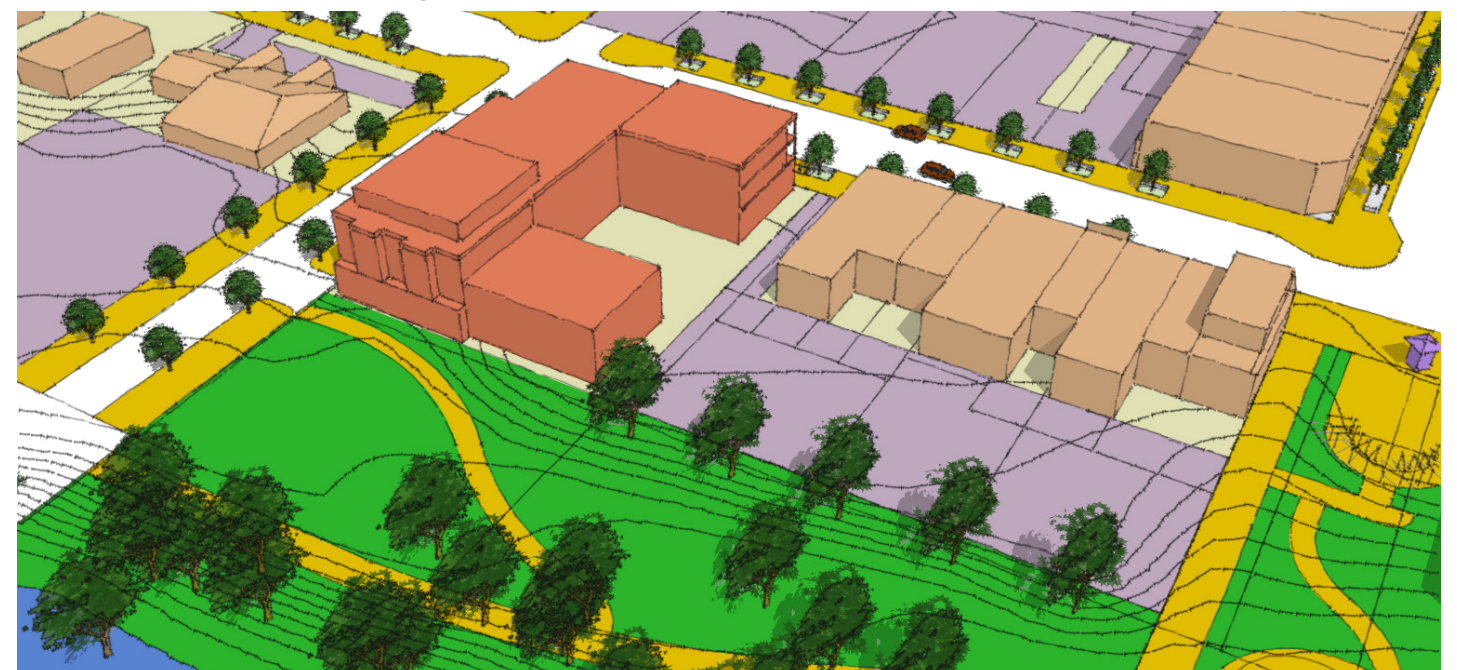
Existing



Key Outcomes

- Active ground floor with direct access onto Main Street
- Pedestrian-scale design and frontage along Main Street
- Above-floor office or residential
- Underground or podium parking, to reduce parking footprint
- Convert the lower surface parking lot to extended green riverfront park space

Potential Future Redevelopment



DOWNTOWN

King to Lowell

On the southwest edge of downtown, the “King to Lowell” block offers long-term, opportunity-driven redevelopment potential to extend the active frontage of downtown while further activating the river.

The block is anchored by a funeral home that sits on the east half of the block. If it were to relocate in the future, that site could become a significant redevelopment opportunity in that the property spans Main Street to the River.

A redevelopment could include a large mixed use project with active street frontage, or a modest boutique hotel. Adding a hotel downtown would further establish downtown as a destination.

Elsewhere on the block, larger commercial or residential development should line Main Street, whereas town homes or small apartment housing could line the riverfront to the south. Redevelopment of the block should include extending the riverfront park west to at least Lowell Ave / Angel St.

Structured or interior surface parking would likely be required, but could service the needs of the entire block if designed well.



- 1 Mixed-use buildings along Main St and Lowell Ave, with active ground floor on Main St, and office or residential above ground
- 2 Boutique hotel with views to the River
- 3 Expanded public park
- 4 Housing fronting the riverfront park

Key Outcomes

- Medium-to-high density housing, with active ground floor uses along Main Street, along the northern half of the block.
- Townhomes or similar owner-occupied housing along the southwest quadrant of the block, leveraging views of the river.
- A modest or boutique hotel along King Avenue NW, connecting Main Street to the Riverfront, leveraging views and access of the river.
- As development along the riverfront occurs, portions of land along the river is transitioned to public riverfront park space, with trails connecting Rivers Edge Commons to Bailey Point Nature Preserve



Modest townhomes, tri-plexes, or similar scale housing could be located along the riverfront



Stillwater’s Hotel Crosby. Credit: Ironmark Building Co.

DOWNTOWN

Main Street and Jackson Avenue Redesign

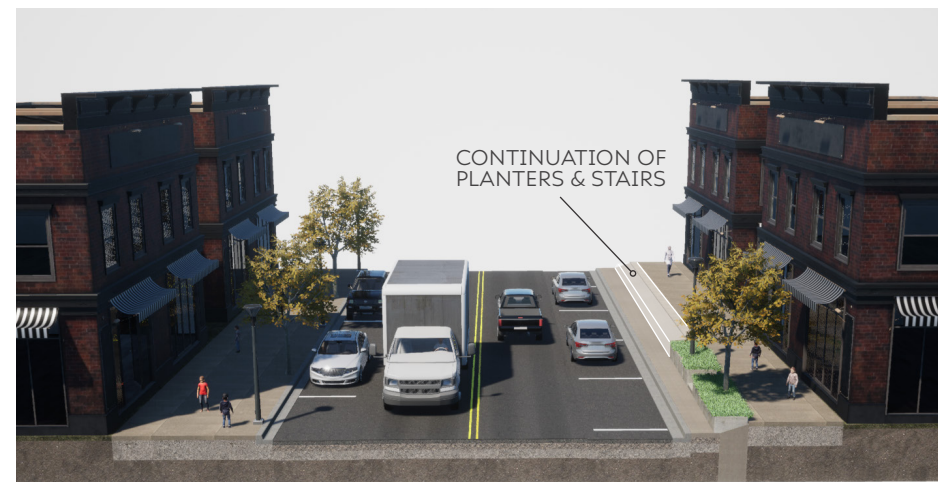
The city is planning to reconstruct Main Street and Jackson Avenue in the coming years, presenting an opportunity to rethink the layout and function of the street.

The design “balance” of the street today has about 75% of the right of way used for vehicles - traveling, delivering, or parking. The redesign of Main Street and Jackson Ave should instead move each towards a 50/50 balance, in which the right of way supports vehicles and pedestrian activity in equal balance.

By doing so, Main Street has the potential to become a destination street in the city in which daily activities happens.



Jackson Ave Redesign



Main Street Redesign



Wider sidewalk space between buildings and the street, would allow for twice as much room to walk, roll, dine, shop, and gather, than currently exists today.

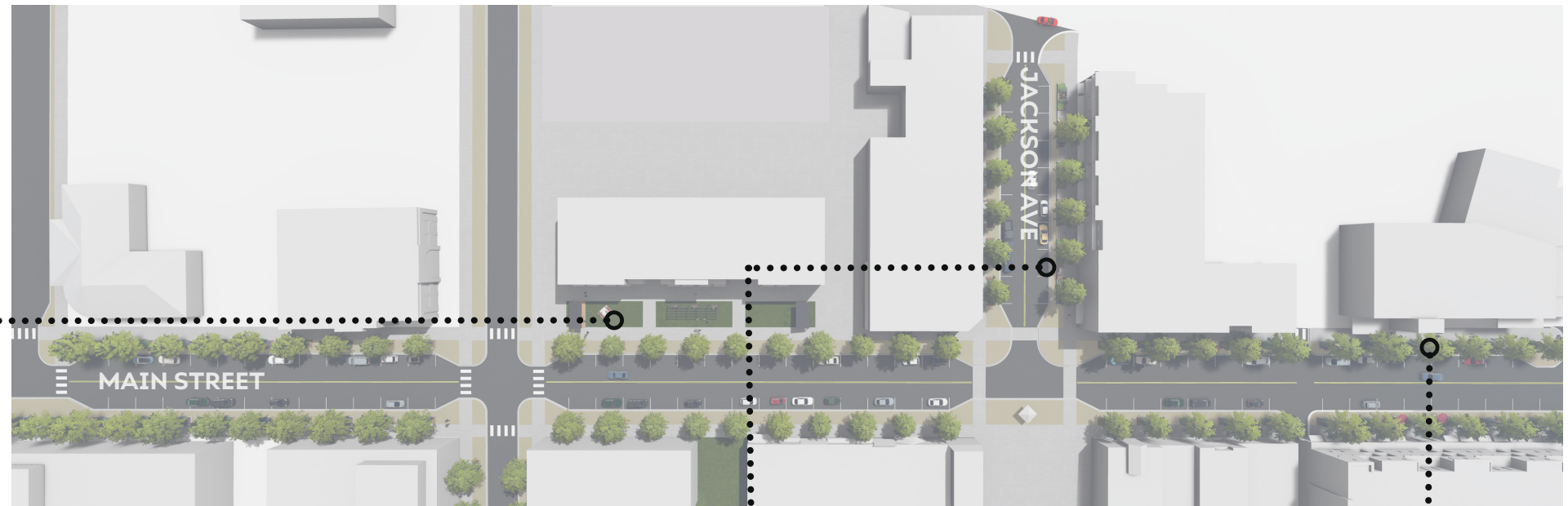
To do this, the plan envisions all angled parking is shifted to parallel parking, and the two travel lanes are reduced to 11 feet. The space “gained” from these changes is enough to double the sidewalk space on both sides of the street, including a new boulevard space with seating and planters for trees.

DOWNTOWN

Main Street and Jackson Avenue Redesign

Key Outcomes

- Main Street and Jackson Avenue are destination streets that balance vehicles and pedestrians, and better serve downtown activities.
- Leveraging a new public space with the King Lot development, events in downtown utilize the expanded sidewalk and boulevard space, while possibly extending into Main Street for larger events.
- Construction occurs through a phased approach, that incrementally closes and constructs streets and sidewalks, and balances project cost, time, and property access.



Flexible Event Space

The additional sidewalk and boulevard space on the north side of Main Street will compliment a future public space at the King Lot development. Events in this public space could spill out onto the sidewalk and boulevard - possibly even the street itself - offering flexible event space that can grow and contract with an event's needs.



New Access to Jackson Ave Shops

By shifting Jackson Avenue's angled parking to parallel, and tightening up travel lanes, a redesigned street will add on-street parking while also increasing the sidewalk space on the east side of the street. In doing so, the current ramped edge could include a landing, periodic stairs to create new points of access to shops, and seating space that "overlooks" the street.



Casual Gathering

The redesign of Main Street would allow for casual outdoor gathering, dining, and even shopping - something downtown lacks today. The new boulevard spaces could act as an extension of businesses, offering additional seating space, areas to advertise and sell goods, or simply to leverage the seating and trees for an attractive "front porch".

DOWNTOWN

Entrances and Gateways

As a narrow, linear downtown wedged between Highway 10 and the Mississippi River, Downtown Elk River lacks routes to “loop back” around to get to a destination. This east end of downtown particularly struggles with this, as missing a destination means having to navigate the Parrish Ave/Hwy 10/Rail Line intersections.

As part of the reconstruction of Main Street, there are opportunities to establish both simple and innovate “loops” at either end of downtown, while also establishing clear gateways.

At Main Street and Lowell Avenue, a mini roundabout would offer this circulation, create a modest welcome feature to downtown, and realign the intersection. To the east, a more complex intersection may warrant an interim solution, while more significant opportunities are explored (see pg X). A U-turn loop back feature could be added just before the parrish intersection, allowing vehicle U-turns while preserving access from Parrish Avenue and into the Bank of Elk River. Within this feature could be a modest art of signage feature, welcoming in visitors. While untraditional, the city of Columbia Heights recently implemented a similar feature.



U-Turn loop back design in Columbia Heights

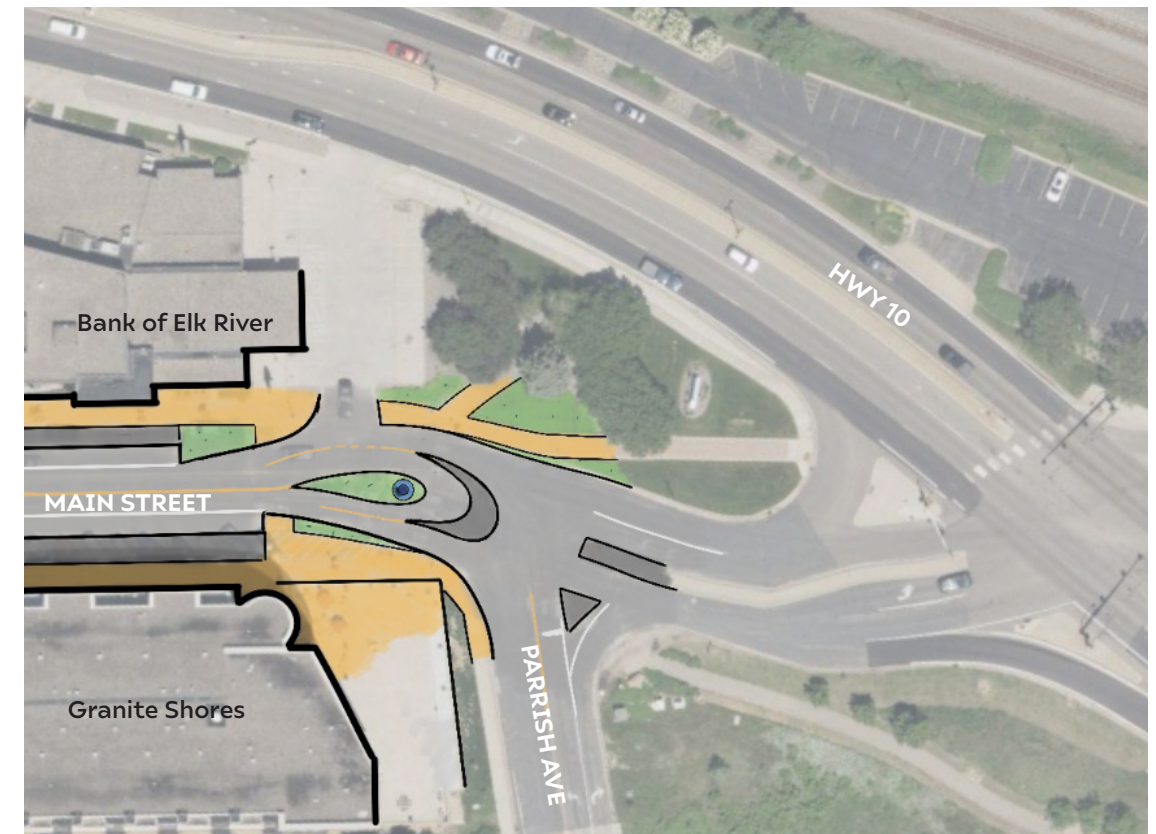
Key Outcomes

- Conduct a design and operation feasibility study for the Main St/Parrish Ave intersection U-Turn design, as part of the Main Street reconstruction project.
- Reconstruct both intersections as part of the Main Street reconstruction project, implementing public art, signage, or other gateway features.

Main St & Lowell Ave Mini Roundabout



Main St & Parrish Ave Interim Design



DOWNTOWN

Downtown, Highway 10, and the Rail Road

The east entrance into downtown is a complex convergence of road, rail, and river. The area is very difficult to cross east-west, particular during rush hour or when there is one of the dozens of trains that pass through the area on a given day.

Improvements to the East Entrance will require significant, and long-term coordination between the City, MnDOT, and BNSF (the rail line owner). Highway 10 is still in a state of good maintenance, and likely won't be due for reconstruction for many years. The City should therefore use this time to plan for and develop design improvements that enhance the entry experience into downtown, while ensuring MnDOT and BNSF needs are continued to be met.

One solution could be to separate Highway 10 and Main Street - an approach called "grade separation". In this scenario, Highway 10 would be elevated over Main Street, while adding a pair of roundabouts to Main Street to allow for easy on/off access from Highway 10, and the ability to "loop" back into downtown. The BNSF rail line would remain in its location, and Main Street would use the grade change to tunnel underneath the rail line, before coming back to grade at the Gates Ave intersection. This would require closing the Railroad Dr access to Main Street.

Roundabouts aren't necessarily the preferred route for pedestrians, however east-west pedestrian safety would be significantly improved as people would no longer need to cross Highway 10 or the rail line. Comfortable sidewalks, pedestrian scale lighting, and even the inclusion of art or other features could make it an attractive pedestrian route.

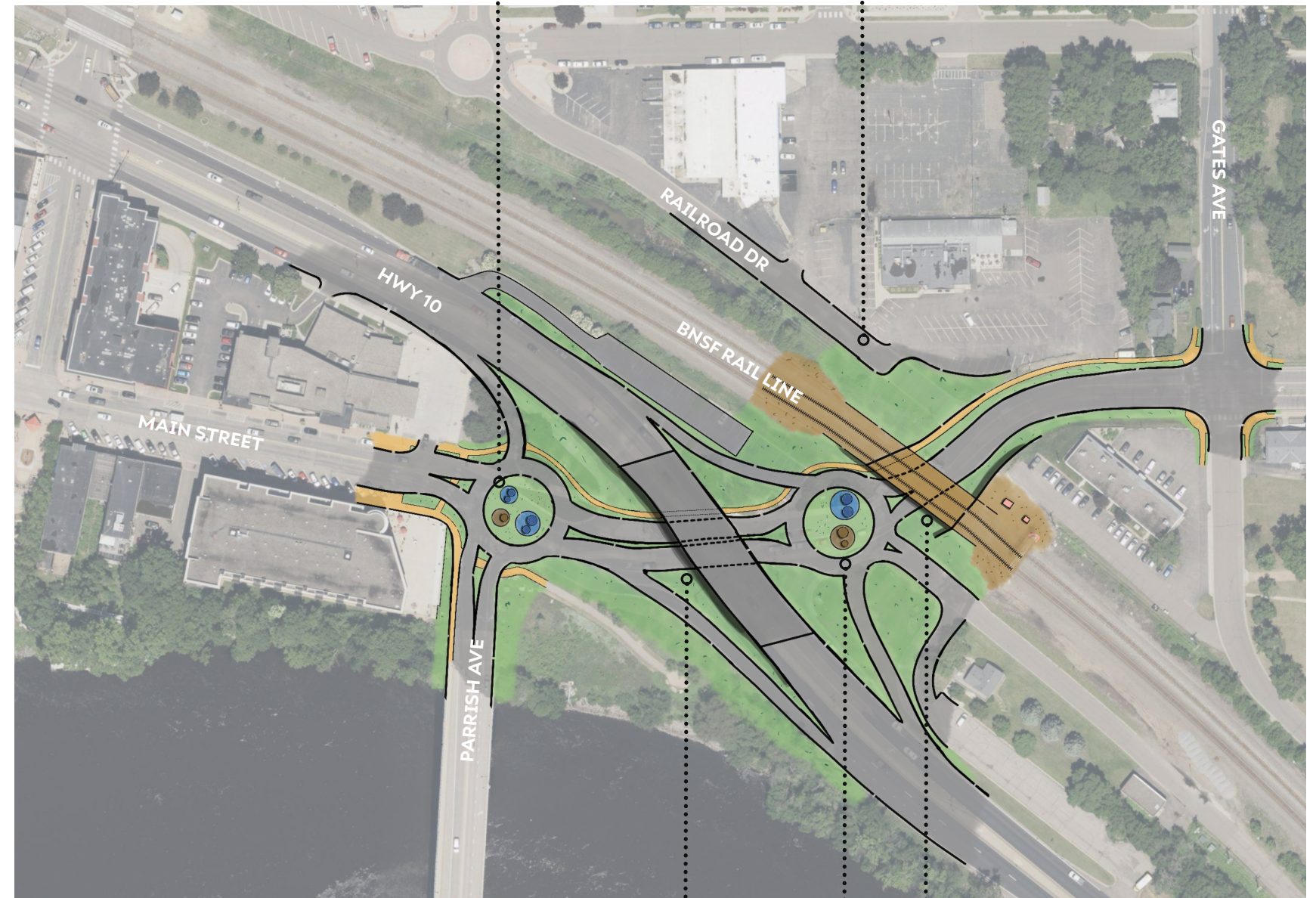
While this is just one concept, the City should continue to coordinate with MnDOT and BNSF, while progressing additional feasibility studies for the area.

Key Outcomes

- Conduct a design and operations feasibility study, to determine the viability, and alternative layouts, for the interchange area.
- Initiate conversations with MnDOT and BNSF, to raise awareness of design opportunities and challenges.
- As feasibility assessments occur, continue working with MnDOT and BNSF on design, funding, and timeline coordination.

A roundabout between Highway 10 and downtown at Parish Avenue allows for circulation and "loop backs" into downtown

Closed Railroad Dr access



Highway 10 is elevated over Main Street

Main St uses existing topography to tunnel under the BNSF rail line, which remains at current grade

A roundabout between Highway 10 and the BNSF rail line allows for circulation and loop backs into downtown

THE RIVERFRONT

The riverfront holds the potential to be an even more significant amenity for downtown, and a stronger part of its identity. Downtown has made several improvements to leverage the water to draw people and activity to downtown, yet could benefit greatly from even more investment. Establishing a signature public riverfront edge could become a defining feature within downtown, and offer a unique amenity in which to draw more activity, people, and private development interest.

Such a space should be a gathering spaces for families, employees on a lunch break, friends meeting for coffee, or shoppers looking for a brief respite. The naturalized edge offers year-round views of the water, while providing flexible programming space that compliments commercial and event activity within the redesigned Main Street and Jackson Avenue.

Expanding publicly-accessible space along the riverfront will be a multi-year, multi-phase approach, with strong partnerships and collaboration with current and future property owners. While the city can take some initial steps today, property owners and other stakeholders will be necessary partners to build this amenity.



Downtown's public riverfront will be a local and regional draw, while offering a unique place to rest, play, and gather that supports Downtown.

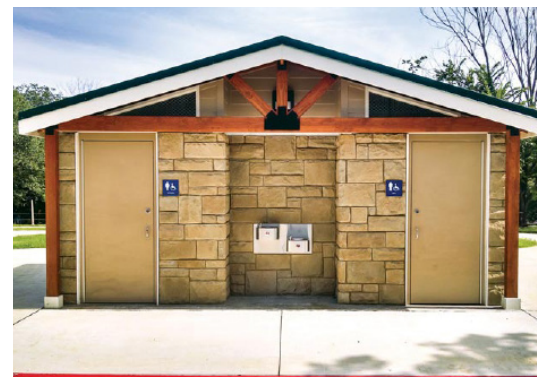
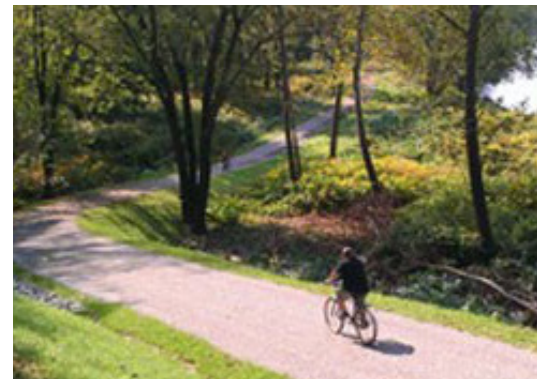
THE RIVERFRONT

Today, the Riverfront plays a limited role within Downtown’s daily life, as many buildings back up to the river, and few provide direct visibility and access

Changing this will require significant adaptations to existing buildings, or more practically, changing the expectations of future development along the river. As such, any future development facing the river should ensure strong visibility of the water, and whenever possible, direct access to future public space.

Within this public space, the city and should implement trails connecting Rivers Edge Commons to Bailey Point, while exploring future feasibility of a suspended or elevated trail between Rivers Edge Commons and the Parrish Bridge (in response to bluff and grade). Supportive elements and programming should accompany this trail, including pedestrian scale lighting, plenty of seating, public art, and strategy places to pause and enjoy nature. The south terminus of King Avenue in particular could be such as place, offering an overlook at the rivers edge, and a modest plaza space for events.

Future improvements should ensure continued drop off access for performers at the Rivers Edge Commons, which could be facilitated with a larger trail from King Avenue. Likewise, any future structures, restroom facilities, or similar programmable space should have clear visibility and sightlines, while ensuring maintenance staff have clear and easy access.



Key Outcomes

- Transition the city-owned parking lot to a riverfront park area, with wide trail access to allow for access to the amphitheater
- Generous trails that connect riverfront park to Bailey Point nature preserve to the west
- “Punch out” spaces such as docks or overlooks that people can look out over the river, particularly at the end of King St
- Small pad spaces for park activation, such as coffee carts or book mobiles
- Places to sit, rest, and play, with clear wayfinding and signage

UPTOWN

Within and around Downtown, the strongest opportunity for new and more places to live is within “Uptown” - the four blocks within Jackson Avenue, 4th Street, Railroad Drive, and Gates Avenue.

While Downtown will continue to offer a mix of places to shop, work, play, and live, Uptown will be defined as a residential neighborhood with a mix of housing options. Larger apartments could be located along the edges, whereas smaller town homes, apartments, or even compact single-family homes, are found in the interior.

As opportunity-driven commercial uses emerges, they will be located along a small activity “node” at the Railroad Drive, Irving Avenue, and 3rd Street intersection. Likewise, a small or modest grocer could be located in the south corner along Main Street, leveraging the proximity of Uptown and Downtown, along with visibility from Highway 10’s more than 30,000 daily travelers.

Along Main Street “East”, and enhanced entry experience is provided with an additional sidewalk, improved streetscape, and signage that clearly orients travelers to downtown.



Uptown will be a new, walkable residential neighborhood, offering more housing choices in close proximity to downtown.

UPTOWN

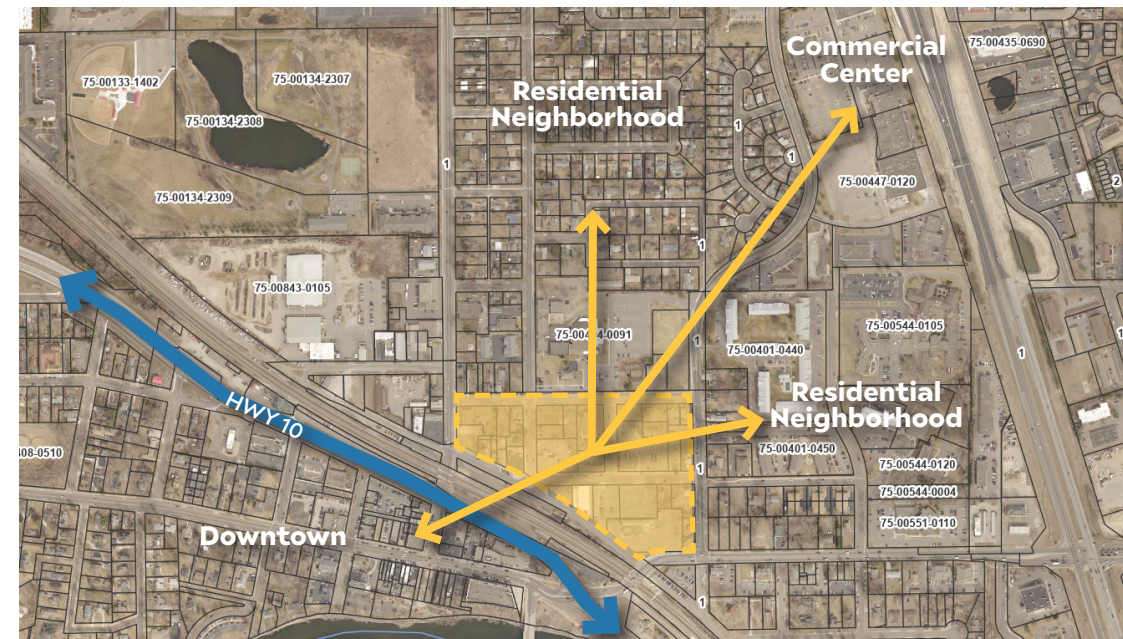
A New Neighborhood



- 1 A variety of housing types and options
- 2 Commercial uses oriented around the Railroad Dr intersection
- 3 Removed Railroad Drive access to Main Street
- 3 A small, modest grocer

Key Outcomes

- A mix of housing types, styles, ownership, and price points, to provide a broad range of housing opportunities for the uptown area.
- A walkable, pedestrian scaled area, that leverages the existing street network and grid.
- Opportunity-driven commercial uses are oriented around the Railroad Drive/Irving Avenue/3rd St intersection, creating a small activity node within the neighborhood..
- A new modest grocery store in the southern area of Uptown, serving residents in Uptown, Downtown, the surrounding neighborhoods, and travelers along Highway 10. Railroad Drive access to Main Street should be closed off at this location.



Uptown is strategically located to support downtown, surrounding commercial areas, and be a complement to surrounding residential neighborhoods. Additionally, any future commercial uses in Uptown, such as a grocer, would benefit from the visibility and access to Highway 10.

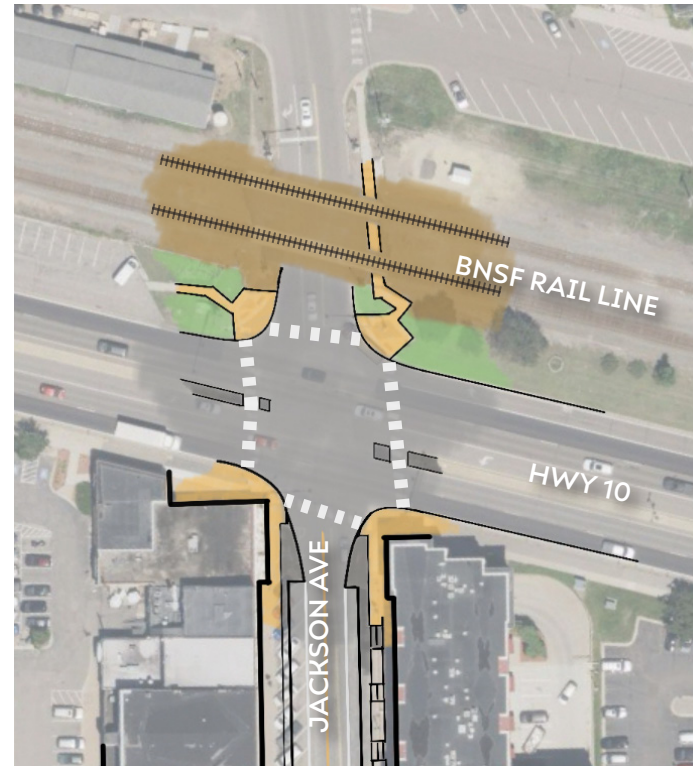
UPTOWN

Jackson Street Crossing & Main Street East

Jackson Avenue Crossing

Improving the Jackson Avenue crossing at Highway 10 will offer better pedestrian safety and stronger connectivity between Downtown and Uptown.

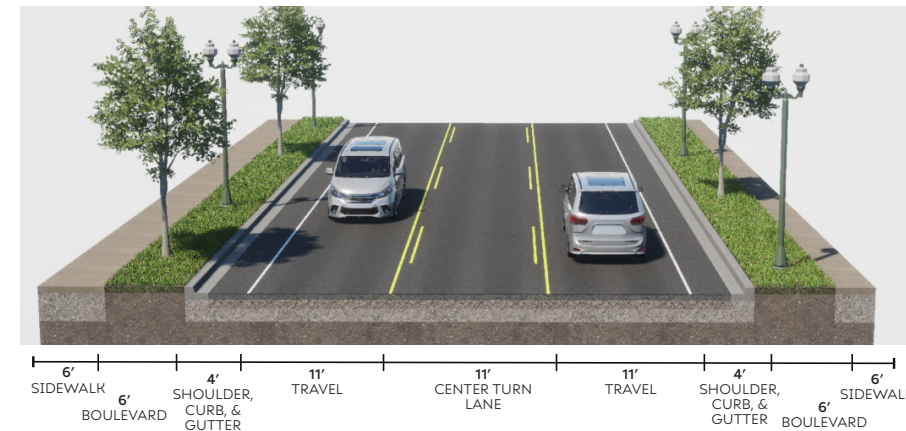
Solutions could include a longer and dedicated pedestrian crossing signal timing, pedestrian refuges within Highway 10 at the center medians, and tightening up the curbs at each intersection. Adding a marked cross walk across Jackson Avenue on the north side, will also help pedestrian visibility.



Main Street East

Main Street East would benefit from minor design improvements. The existing street layout should remain, but should add a new sidewalk along the north side of the street. This would require reducing the current shoulders, and reconstructing and reversing the sidewalk and boulevard on the south side of the street. Additionally, the city should bury the existing utility poles, add new lighting, and add features like banners, trees, signage, and art.

Other improvements could come by working with current and future property owners to consolidate property access onto side streets, where possible. Minimizing driveways and vehicle access onto the street would benefit both vehicles and pedestrian by keeping vehicle access to intersections. This would require working with existing and future property owners to redirect access to side streets, or potentially establish new access points. Shared access and/or parking agreements may also be needed for some properties.



Consolidated Access



Key Outcomes

- As the street pavement becomes due for repair or replacement, design the street with sidewalks on both sides, buried utilities, and more aesthetically attractive lighting.
- Work with current and future owners on mutually beneficial opportunities to consolidate access, facilitating access agreements or new access points, as needed, to promote safety along Main Street East.



5 IMPLEMENTATION

IMPLEMENTING THE PLAN

Clarity and Prioritization

To equip the City and its partners with the long-term guidance to realize the plan's vision, the following implementation plan outlines both action steps necessary for downtown broadly, and those specific to five opportunity area projects.

Downtown Vision Implementation Steps

Implementation action steps are provided for the entire downtown, and are organized around Connectivity, Development, and Parks & Open Space.



IMPLEMENTING THE PLAN

Development

Action	DEVELOPMENT	
	Timing	Lead (L) / Support (S)
1. Develop the City Owned Lot Solicit and pursue development of the city-owned parking lot along Main Street.	Priority	(L) Community Development (S) Engineering, Public Works
2. Continue supporting existing businesses Continue to support existing businesses by establishing programs such as: • Façade improvement program • Business improvement grants • Business spotlight program	Ongoing	(L) Community Development (S) Engineering, Public Works
3. Prepare for private redevelopment • Proactively raze uptown to allow for denser residential and commercial mixed use • Establish general development partnership processes and outcomes expectations, for redevelopment within downtown and uptown • Case-by-case evaluation of city site acquisition, preparation, and sale	Ongoing	(L) Community Development (S) Engineering, Public Works, Private Property, Owners, Private Development
4. Market & advertise downtown In partnership with DEBA and area Chambers, continue to market and champion downtown for business, activities, and investment.	Ongoing	(L) Community Development, DEBA (S) Elk River Area Commerce
5. Attract Specialty Users Begin attracting users downtown such as a grocer and boutique hotel (see appendix 2)	Priority / Ongoing	(L) _____ (S) DEBA

Opportunity Area Project Implementation Steps

Implementation action steps specific to each Catalytic Project are also provided, and are structured as single-page project sheets for ease of use.



1. City owned lot
2. King to Lowell
3. Riverfront
4. Main & Jackson
5. Uptown



OPPORTUNITY AREA

City-Owned Lot

Priority: 0-1 Years
Near-Term: 2-3 Years
Mid-Term: 3-5 Years
Long-Term: 5+ Years
Origin: Policy / Opportunity-Driven

PRIORITY			
Complete a site survey and environmental review of the city-owned property.	Develop preliminary infrastructure designs and details for public infrastructure into and around the site. These plans should clearly delineate pedestrian crossing locations and the redesign Main St.	Complete preliminary site environmental reviews to understand site conditions, and identify anticipated remediation needs.	Using the Downtown Master Plan, establish clear development outcomes that the City and area stakeholders expect from a future development on the site.
PRIORITY		NEAR-TERM	
Solicit a developer for the riverfront property redevelopment project through a competitive RFP process. The developer should expect to partner with the City on the project.	Establish necessary agreements during the development negotiation process, for shared access and use of the remaining parking area on the site.	As needed, partner with the developer on financial subsidies or other strategic partnership needs, as identified during development negotiations.	Complete all required design approvals, site rezoning, project permitting, and begin construction.

KEY COMPONENTS

- New publicly accessible park along Main St, that can accommodate the farmers market and other events
- Active ground floor with direct access onto Main Street
- Pedestrian-scale design and frontage along Main Street
- Above-floor office or residential uses
- Maintain ability for future structured, shared parking ramp
- Explore the viability of a linear development along Highway 10

IMPLEMENTING THE PLAN

Development

Priority= 0 -2 Years
 Near Term= 2-5 Years
 Med. Term= 5-10 Years
 Long-Term= 10+ Years
 Ongoing= Periodically / Opportunity-Driven



DEVELOPMENT		
Action	Timing	Lead (L) / Support (S)
1. Develop the City Owned Lot Solicit and pursue development of the city-owned parking lot along Main Street	Priority	(L) Community Development, HRA, EDA (S) Engineering, Public Works
2. Continue Supporting Existing Businesses Continue to support existing businesses by establishing programs such as: <ul style="list-style-type: none"> Façade improvement program Business improvement grants Business spotlight program 	Ongoing	(L) Community Development, HRA, EDA (S) Engineering, Public Works
3. Prepare for private redevelopment <ul style="list-style-type: none"> Proactively rezone Uptown to allow for denser residential and commercial mixed use Establish general development partnership processes and outcomes expectations, for redevelopment within Downtown and Uptown Case-by-case evaluation of city site acquisition, preparation, and sale 	Ongoing	(L) Community Development, HRA, EDA (S) Engineering, Public Works, Private Property Owners, Private Developers
4. Market & Advertise Downtown In partnership with DERBA and area Chambers, continue to market and champion downtown for businesses, activities, and investment	Ongoing	L: Community Development, DERBA (S) Elk River Area Chamber of Commerce
5. Attract Specialty Users Begin attracting uses downtown such as a grocer and boutique hotel (see Appendix B)	Priority / Ongoing	(L) Community Development, HRA, EDA (S) DERBA

IMPLEMENTING THE PLAN

Connectivity

Priority= 0 -2 Years
 Near Term= 2-5 Years
 Med. Term= 5-10 Years
 Long-Term= 10+ Years
 Ongoing= Periodically / Opportunity-Driven



CONNECTIVITY		
Action	Timing	Lead (L) / Support (S)
1. Reconstruct Main St & Jackson Ave Reconstruct Main Street and Jackson Avenue with wider, more comfortable space for people, events, and business activity	Priority	(L) Engineering, Public Works (S) Community Development
2. Improve Main St East Walkability Reconstruct Main Street East with a new sidewalk on the north side and work with property owners to consolidate access	Med. Term	(L) Engineering, Public Works (S) Community Development
3. Provide New Riverfront Trails Construct new trails along the riverfront, connecting Downtown to Bailey Point Nature Preserve	Ongoing	(L) Engineering, Parks & Rec (S) Public Works, Community Development
4. Continue Exploring Crossing Opportunities Continue to explore intersection and crossing improvement opportunities along the Main St / Hwy 10 / Rail Line intersection	Ongoing	(L) Engineering (S) MnDOT, BNSF, Sherburne County
5. Implement Strategic Parking Agreements Work with Elk River Lutheran Church to establish a parking agreement for public use of their parking lot.	Priority	(L) Community Development, Elk River Lutheran Church

IMPLEMENTING THE PLAN

Public & Open Space

Priority= 0 -2 Years
 Near Term= 2-5 Years
 Med. Term= 5-10 Years
 Long-Term= 10+ Years
 Ongoing= Periodically / Opportunity-Driven



PUBLIC & OPEN SPACE		
Action	Timing	Lead (L) / Support (S)
1. Create a New Public Park In partnership with new development, establish a modest new public space along Main Street in the city-owned parking lot	Priority	(L) Community Development, Parks (S) Engineering, Public Works
2. Enhance Downtown Streetscape Provide high-quality streetscape and public realm amenities along Main Street & Jackson Avenue, as part of the reconstruction project	Priority	(L) Engineering, Public Works
3. Enhance Main Street East Provide enhanced streetscape and signage amenities along Main Street East, as part the reconstruction project	Ongoing	(L) Engineering, Public Works (S) Community Development
4. Host More & More Frequent Events Host more events throughout the year - both along Main Street and the River	Near Term	(L) Parks & Rec (S) DERBA
5. Expand the Riverfront Extend public riverfront park area along the River, as redevelopment occurs	Ongoing	(L) Parks & Rec, Engineering, Community Development (S) Public Works, Property Owners

OPPORTUNITY AREA

City-Owned Lot

Priority= 0 -2 Years
 Near Term= 2-5 Years
 Med. Term= 5-10 Years
 Long-Term= 10+ Years
 Ongoing= Periodically / Opportunity-Driven



Lead Implementer	City Community Development, City Public Works, HRA, EDA
Partners	Private Developer
Timeline	Planning and design should begin over next 1-5 years; Implementation beginning by 2027
Funding Source(s)	Private development

KEY COMPONENTS

- New publicly accessible park along Main St, that can accommodate the farmers market and other events
- Active ground floor with direct access onto Main Street
- Pedestrian-scale design and frontage along Main Street
- Above-floor office or residential uses
- Maintain ability for future structured, shared parking ramp
- Explore the viability of a linear development along Highway 10

PRIORITY

Complete a site survey and environmental review of the city-owned property.

Develop preliminary infrastructure designs and details for public infrastructure into and around the site.

These plans should clearly delineate pedestrian crossing locations and the redesign Main St.

Complete preliminary site environmental reviews to understand site conditions, and identify anticipated remediation needs

Using the Downtown Plan, establish clear development outcomes that the City and area stakeholders expect from a future development on the site.

PRIORITY

Solicit a developer for the city-owned property redevelopment site through a competitive RFP process.

The developer should expect to partner with the City on the project.

Establish necessary agreements during the development negotiation process, for shared access and use of the remaining parking area on the site

As needed, partner with the developer on financial subsidies or other strategic partnership needs, as identified during development negotiations.

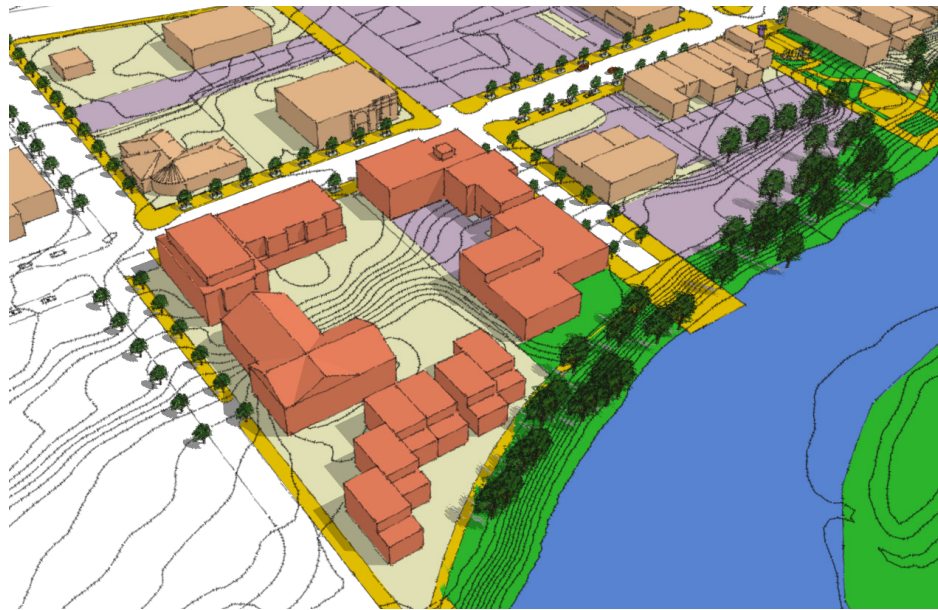
Complete all required design approvals, site rezoning, project permitting, and begin construction

NEAR-TERM

OPPORTUNITY AREA

King to Lowell

Priority= 0 -2 Years
 Near Term= 2-5 Years
 Med. Term= 5-10 Years
 Long-Term= 10+ Years
 Ongoing= Periodically / Opportunity-Driven



Lead Implementer	City Community Development, City Public Works
Partners	Private Developer, HRA, EDA
Timeline	Planning and design should begin over next 1-5 years; Implementation beginning by 2027

KEY COMPONENTS

- New mixed use and single unit residential development
- Active ground floor with direct access onto Main Street
- Pedestrian-scale design and frontage along Main Street
- Above-floor office or residential uses
- Attract a hotel to east side of the block
- Negotiated new public park extension along the riverfront

ONGOING

Continue outreach and communication with current site owners, to track with and evaluate redevelopment opportunities, and timing.

Consider on a case-by-case basis the mutual benefits of city acquisition of sites, to facilitate assembling redevelopment sites.

Begin networking with and positioning downtown for a hotel by building key relationships and attending industry events.

NEAR TERM

Prepare for future park extension on the south end of the block by preparing a conceptual park site plan, to use in owner and developer discussions.

NEAR TERM

Develop preliminary infrastructure designs and details for public infrastructure into and around the site.

These plans should clearly delineate pedestrian crossing locations and the redesign Main St.

Hotel Attraction

Attracting a hotel in this area could be somewhat challenging for Elk River, as it is not yet a tourism destination, and few small boutique hotels have been built in the area in recent years. However, as the City implements improvements that address connectivity challenges and emphasize Downtown's connection to the river, the prospects for a new hotel development will improve. Strategies the City should consider to attract hotel development include:

- Identification of key sites, preferably along the river, that are suitable for hotel development and zoned appropriately to allow for an urban, boutique-style hotel. This can include sites suitable for both ground-up development and adaptive reuse.
- Increasing visits to Downtown Elk River through branding, high-quality placemaking, and events that draw visitors from throughout the region and provide a reason to stay overnight.
- Financial incentives such as tax abatements, infrastructure improvements, and/or shared planning costs.
- Building and maintaining relationships with developers and key property owners to identify potential opportunities for partnerships.

OPPORTUNITY AREA

Riverfront

Priority= 0 -2 Years
 Near Term= 2-5 Years
 Med. Term= 5-10 Years
 Long-Term= 10+ Years
 Ongoing= Periodically / Opportunity-Driven



Lead Implementer	City Community Development, City Public Works, City Parks
Partners	Private property owners
Timeline	Near-Term / Ongoing

NEAR-TERM

Prepare for future park extension on the south end of the block by preparing a conceptual park site plan, to use in owner and developer discussions.

Transition the south end of King Ave to a park space, in preparation for future park expansions and improvements.

MED-TERM

Transition the lower city-owned parking lot to park space, expanding Rivers Common Edge park and implementing initial trail and programming features.

ONGOING

Continue coordinating with property owners on future opportunities to continue expanding park land and programming features.

As completed, install trails along Angel Ave to Bailey Point Nature Preserve

Improvement Phasing

Realizing the vision for the Riverfront will require strategic phasing, and strong partnerships with current and future property owners. The plan recommends making improvements within city-owned property first, while seeking out opportunity-driven improvements for privately held property.

KEY COMPONENTS

- Convert the lower city parking lot to park
- Incrementally transition properties along the riverfront, starting with the lower city-owned parking lot, and King Ave terminus
- Extend trails west as park land is extended, eventually connecting to Bailey Point Nature Preserve.
- Add elements into the park like seating, lighting, gazebos, docks over the water, and space for pop-up programming



OPPORTUNITY AREA

Main Street & Jackson Avenue

Priority= 0 -2 Years
 Near Term= 2-5 Years
 Med. Term= 5-10 Years
 Long-Term= 10+ Years
 Ongoing= Periodically / Opportunity-Driven



Lead Implementer	City Community Development, City Public Works, Construction Contractor
Partners	DERBA, Private Property Owners
Timeline	Planning and design should begin over next 1-5 years; Implementation beginning by 2027

KEY COMPONENTS

- Reconstruct Main St and Jackson Ave as envisioned by the Downtown Plan.
- Prior to construction bidding, develop a desired construction phasing plan with DERBA that incrementally closes and reconstructs each street, and identify support programs during construction.
- During construction, ensure clear and regular communication with area property and business owners.

PRIORITY

Progress Main St and Jackson Ave into detailed design, cost estimating, and funding.

Work with DERBA to establish a construction phasing plan, identifying key performance outcomes and property/business owner support programs.

Ongoing

As part of project bidding, request options for phased construction scenarios, to weigh options and mitigate access issues.

During construction, ensure weekly contractor and business owner progress meetings, and the installation of clear signage that accurately directs to business access.

Construction Phasing

To balance construction cost, time, and business access, the reconstruction project should utilize a phased construction plan, such as block-by-block phasing. The final phasing plan should be developed in partnership with DERBA to promote project transparency and business readiness.



Example Approach to Project Phasing

- Phase 1
- Phase 2
- Phase 3

Property and Business Support

During project bid preparation, develop a list of support options for area property and business owners. These could include:

- Weekly progress report meetings
- Contractor incentives for time and preserving clear access
- Bi-weekly window and facade cleaning, using local cleaning business(es)
- Artistic access and wayfinding signage, using local artists
- Hosted events during construction to promote activities downtown

OPPORTUNITY AREA

Uptown

Priority= 0 -2 Years
 Near Term= 2-5 Years
 Med. Term= 5-10 Years
 Long-Term= 10+ Years
 Ongoing= Periodically / Opportunity-Driven



Lead Implementer	City Community Development, Private Property Owners
Partners	Public Works, HRA, EDA
Timeline	Opportunity driven as current owners decide to willingly sell and/or redevelop

KEY COMPONENTS

- Redevelopment of existing blocks that brings a greater variety of housing options, and commercial uses at the Railroad Dr intersection
- A new small or medium grocer located along Main St East
- Improvements to Main St East to enhance mobility, safety, and area aesthetics.
- Improvements to the Jackson Ave / Hwy 10 intersection to enhance pedestrian crossing safety.

PRIORITY

- Rezone properties are needed to support desired redevelopment outcomes, and prepare clear expectations for future developers.
- Begin networking with and positioning uptown for a grocer. by building key relationships and attending industry events.

ONGOING

- Consider on a case-by-case basis the benefits of city acquisition of sites, to facilitate assembling redevelopment sites.
- Continue outreach and communication with current site owners, to track with and evaluate redevelopment opportunities, and timing.

ONGOING

- Design, fund, and reconstruct the identified portions of Main Street East, adding an additional sidewalk and enhanced streetscape features
- Partner with MnDOT to design, fund, and reconstruction the Jackson Ave/Hwy 10 intersection, with pedestrian crossing improvements.

Grocer Attraction

Attracting a grocery store to the Uptown neighborhood will be challenging, especially because there are only few small-format grocery chains operating in the US. The most likely type of grocery store to locate in this area may be a cooperative. Strategies that cities can use to attract grocery stores include:

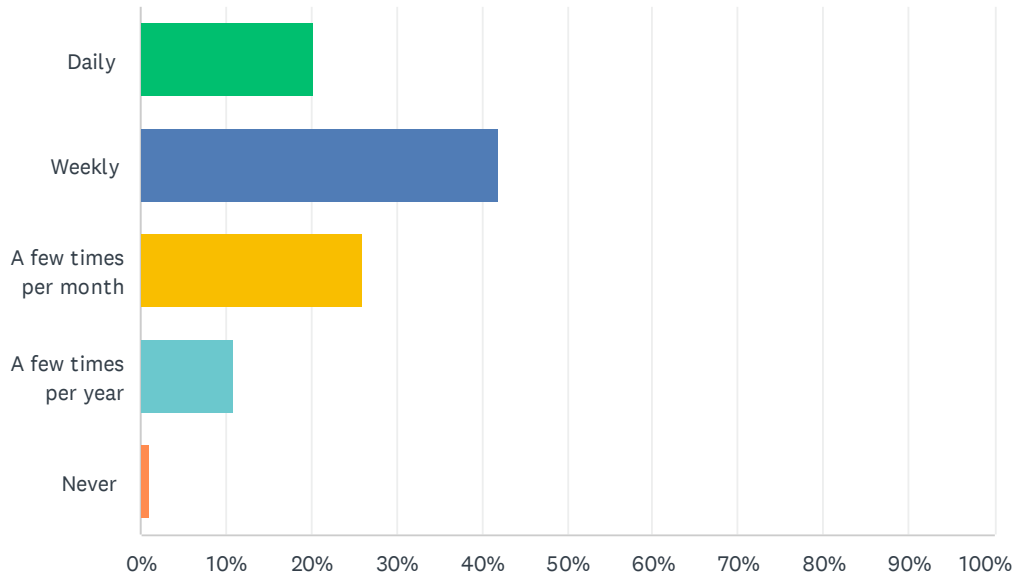
- Financial incentives like land, infrastructure improvements, and/or tax credits
- Regulatory incentives like zoning changes or modifications, design concessions, and expedited permitting
- Alerting the local tenant broker community to a potential grocery location in Uptown
- Attending local ICSC events to network and spread the word about the opportunity

6 APPENDIX

APPENDIX A

Q1 How often do you visit Downtown?

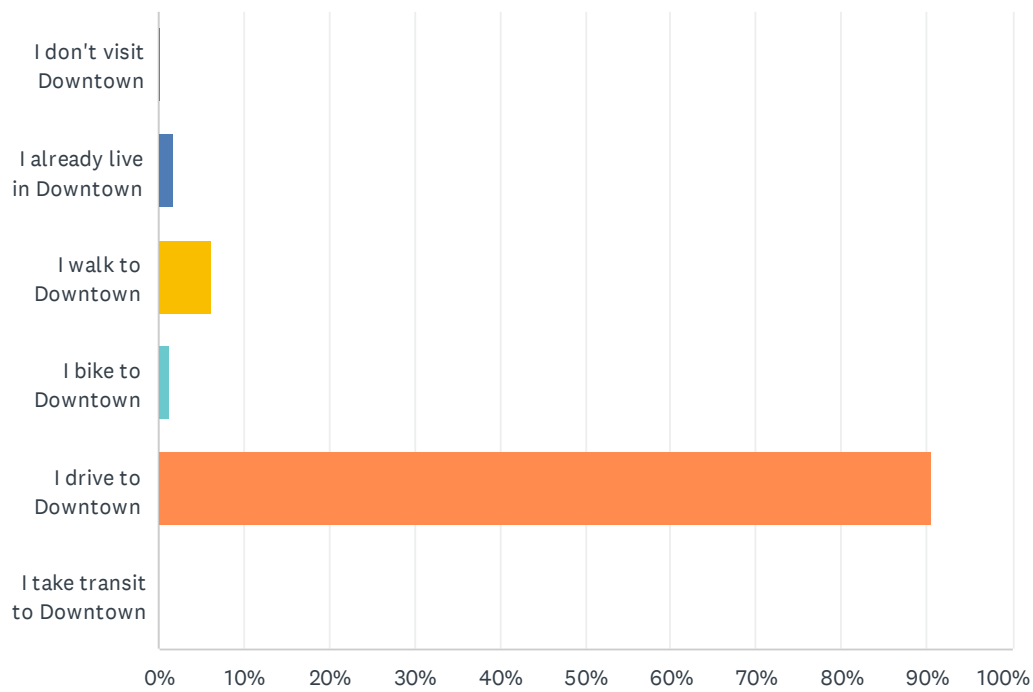
Answered: 362 Skipped: 0



ANSWER CHOICES	RESPONSES	
Daily	20.17%	73
Weekly	41.99%	152
A few times per month	25.97%	94
A few times per year	10.77%	39
Never	1.10%	4
TOTAL		362

Q2 When you visit Downtown, how do you usually get there?

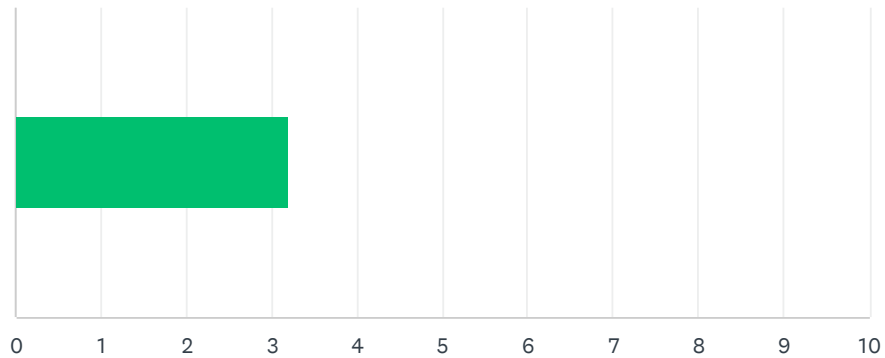
Answered: 362 Skipped: 0



ANSWER CHOICES	RESPONSES	
I don't visit Downtown	0.28%	1
I already live in Downtown	1.66%	6
I walk to Downtown	6.08%	22
I bike to Downtown	1.38%	5
I drive to Downtown	90.61%	328
I take transit to Downtown	0.00%	0
TOTAL		362

Q3 How would you rate the ease of getting to Downtown area by each of the following travel modes? Use the slider to indicate between "Very difficult" and "Very easy" for each mode of travel. Walking

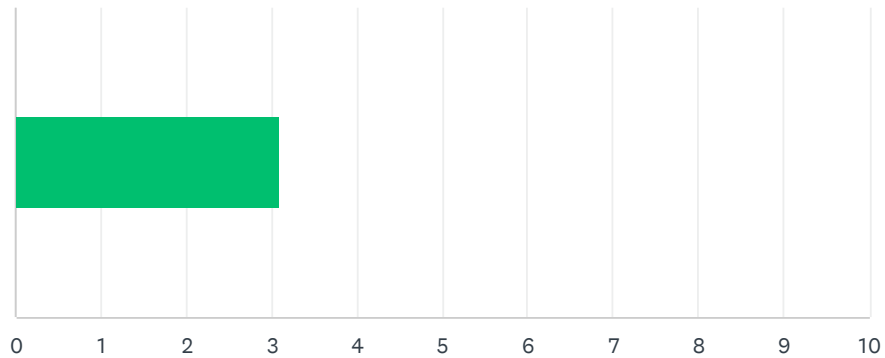
Answered: 362 Skipped: 0



ANSWER CHOICES	AVERAGE NUMBER	TOTAL NUMBER	RESPONSES
	3	1,157	362
Total Respondents: 362			

Q4 How would you rate the ease of getting to Downtown area by each of the following travel modes? Use the slider to indicate between "Very difficult" and "Very easy" for each mode of travel. Biking

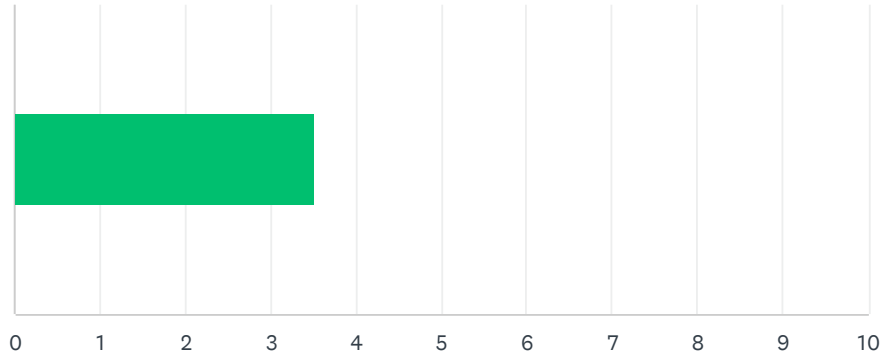
Answered: 362 Skipped: 0



ANSWER CHOICES	AVERAGE NUMBER	TOTAL NUMBER	RESPONSES
	3	1,123	362
Total Respondents: 362			

Q5 How would you rate the ease of getting to Downtown area by each of the following travel modes? Use the slider to indicate between "Very difficult" and "Very easy" for each mode of travel. Driving

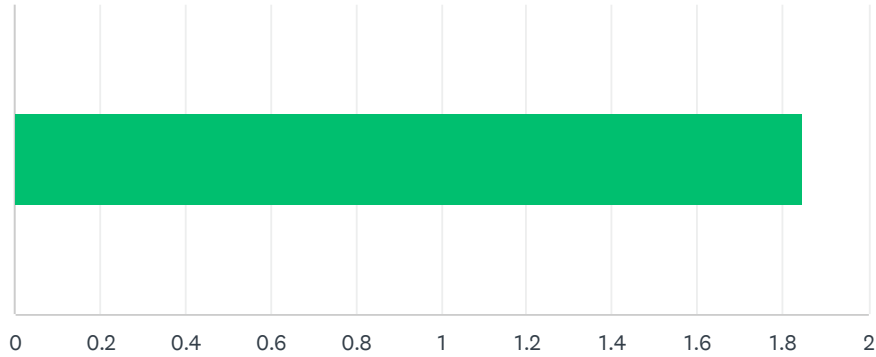
Answered: 362 Skipped: 0



ANSWER CHOICES	AVERAGE NUMBER	TOTAL NUMBER	RESPONSES
	4	1,277	362
Total Respondents: 362			

Q6 How would you rate the ease of getting to Downtown area by each of the following travel modes? Use the slider to indicate between "Very difficult" and "Very easy" for each mode of travel. Transit

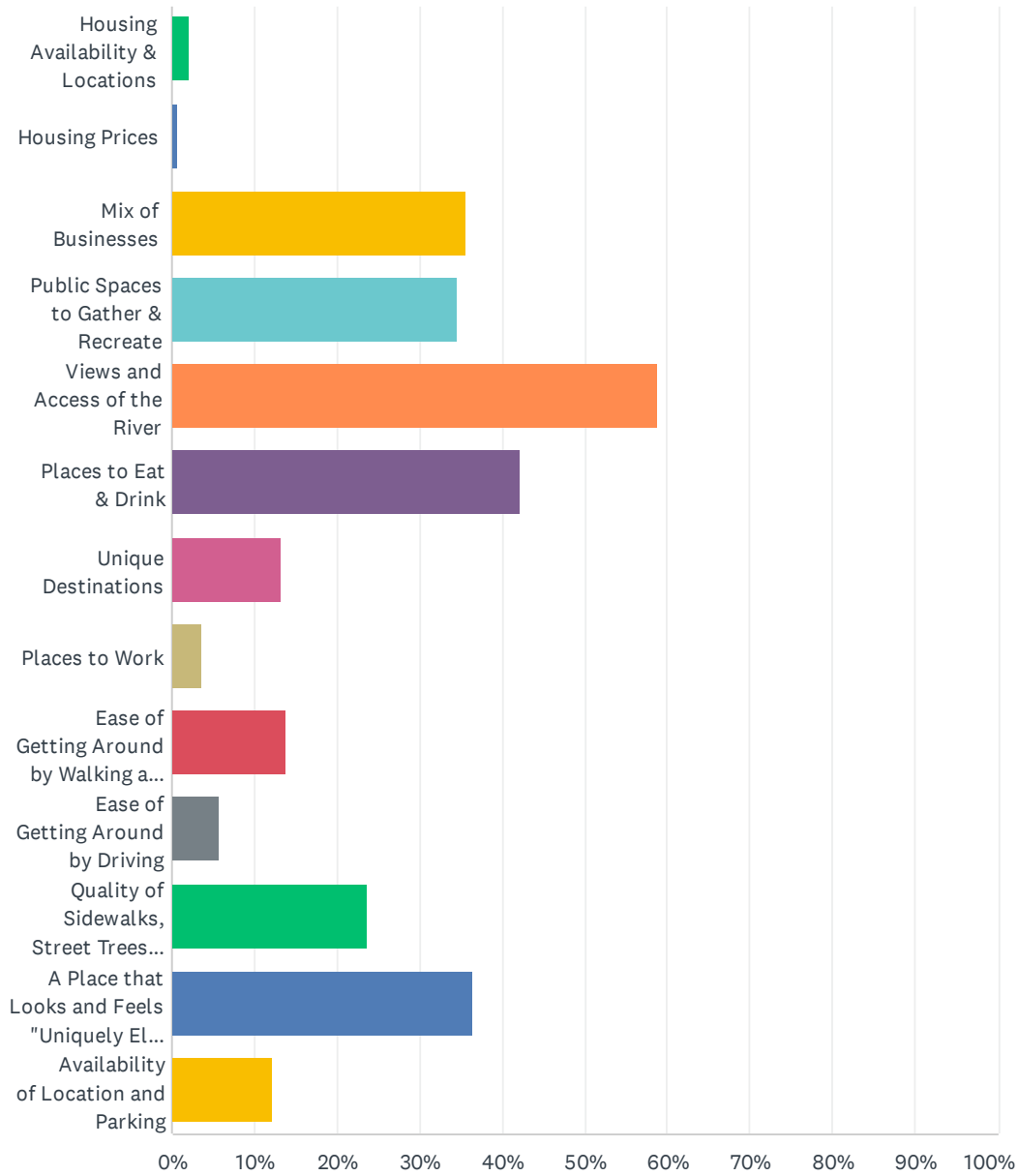
Answered: 362 Skipped: 0



ANSWER CHOICES	AVERAGE NUMBER	TOTAL NUMBER	RESPONSES
	2	669	362
Total Respondents: 362			

Q7 What are some strengths of Downtown? Select at least one (1) and up to three (3) strengths.

Answered: 362 Skipped: 0

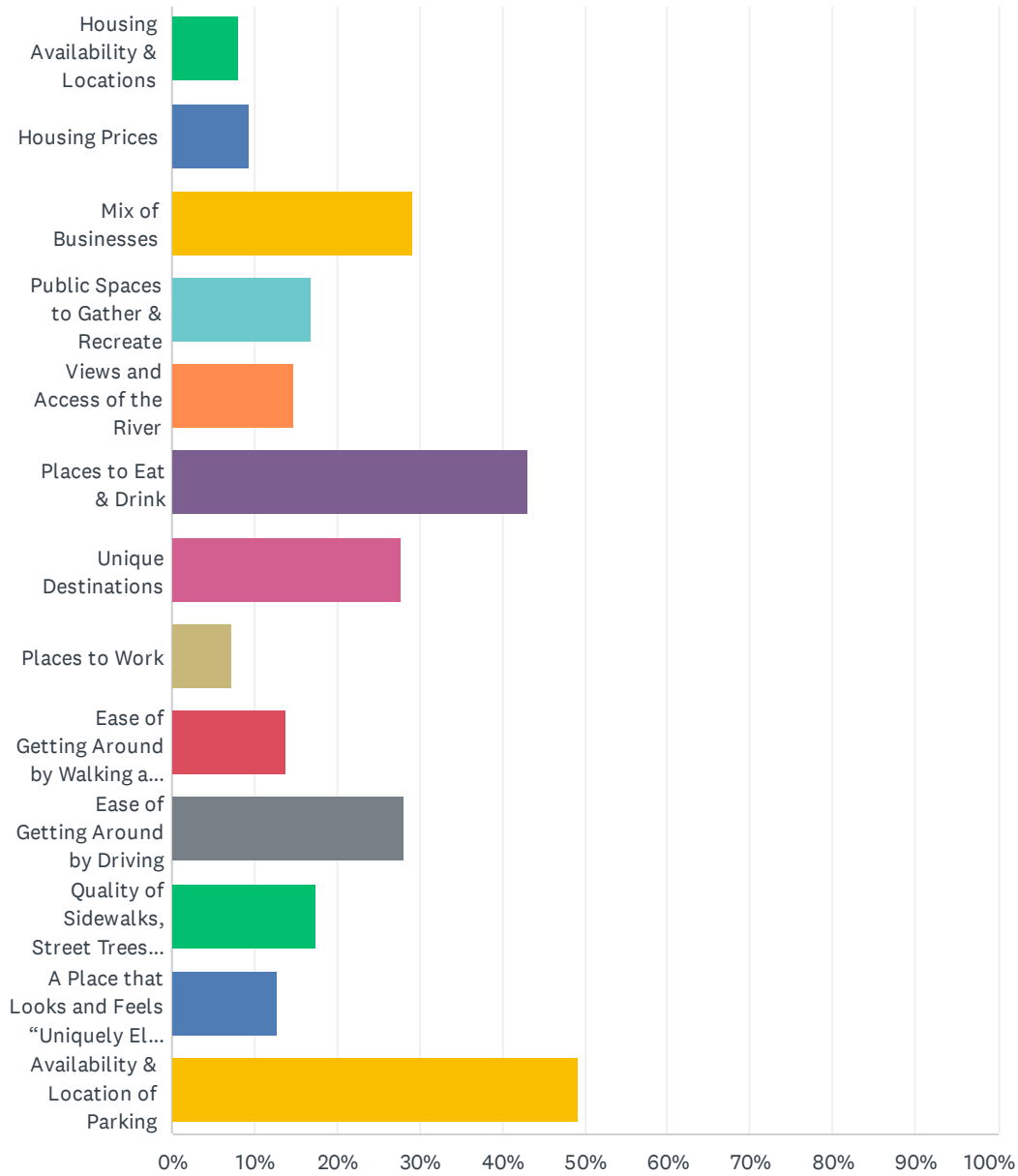


Elk River Downtown Area Plan - Community Survey

ANSWER CHOICES	RESPONSES	
Housing Availability & Locations	2.21%	8
Housing Prices	0.55%	2
Mix of Businesses	35.64%	129
Public Spaces to Gather & Recreate	34.53%	125
Views and Access of the River	58.84%	213
Places to Eat & Drink	42.27%	153
Unique Destinations	13.26%	48
Places to Work	3.59%	13
Ease of Getting Around by Walking and Biking	13.81%	50
Ease of Getting Around by Driving	5.80%	21
Quality of Sidewalks, Street Trees, and Overall Aesthetics	23.76%	86
A Place that Looks and Feels "Uniquely Elk River"	36.46%	132
Availability of Location and Parking	12.15%	44
Total Respondents: 362		

Q8 What some weaknesses of Downtown? Select at least one (1) and up to three (3) weaknesses.

Answered: 362 Skipped: 0

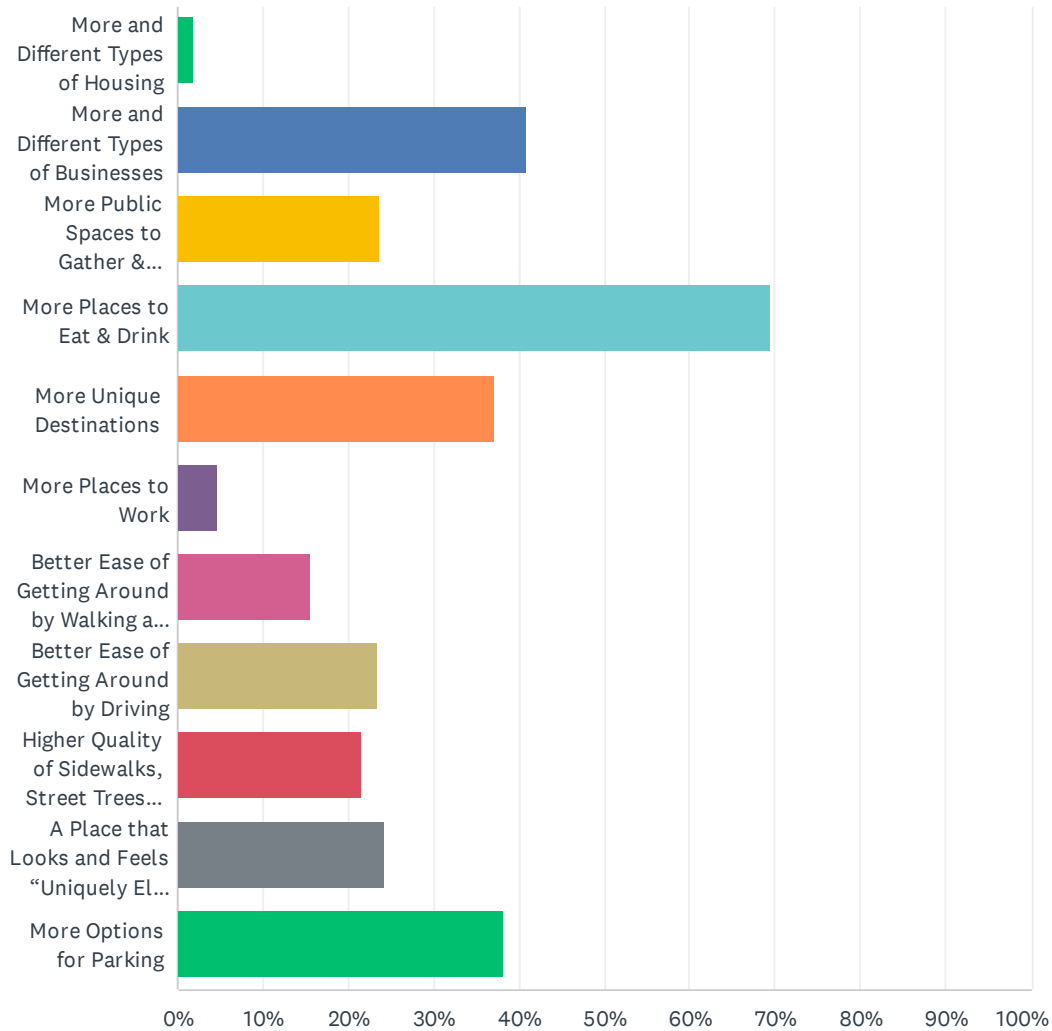


Elk River Downtown Area Plan - Community Survey

ANSWER CHOICES	RESPONSES	
Housing Availability & Locations	8.01%	29
Housing Prices	9.39%	34
Mix of Businesses	29.28%	106
Public Spaces to Gather & Recreate	16.85%	61
Views and Access of the River	14.64%	53
Places to Eat & Drink	43.09%	156
Unique Destinations	27.62%	100
Places to Work	7.18%	26
Ease of Getting Around by Walking and Biking	13.81%	50
Ease of Getting Around by Driving	28.18%	102
Quality of Sidewalks, Street Trees, and Overall Aesthetics	17.40%	63
A Place that Looks and Feels "Uniquely Elk River"	12.71%	46
Availability & Location of Parking	49.17%	178
Total Respondents: 362		

Q9 Thinking of the future, what would you most like to see in Downtown? List at least one (1) and up to three (3) things you'd like to see.

Answered: 362 Skipped: 0

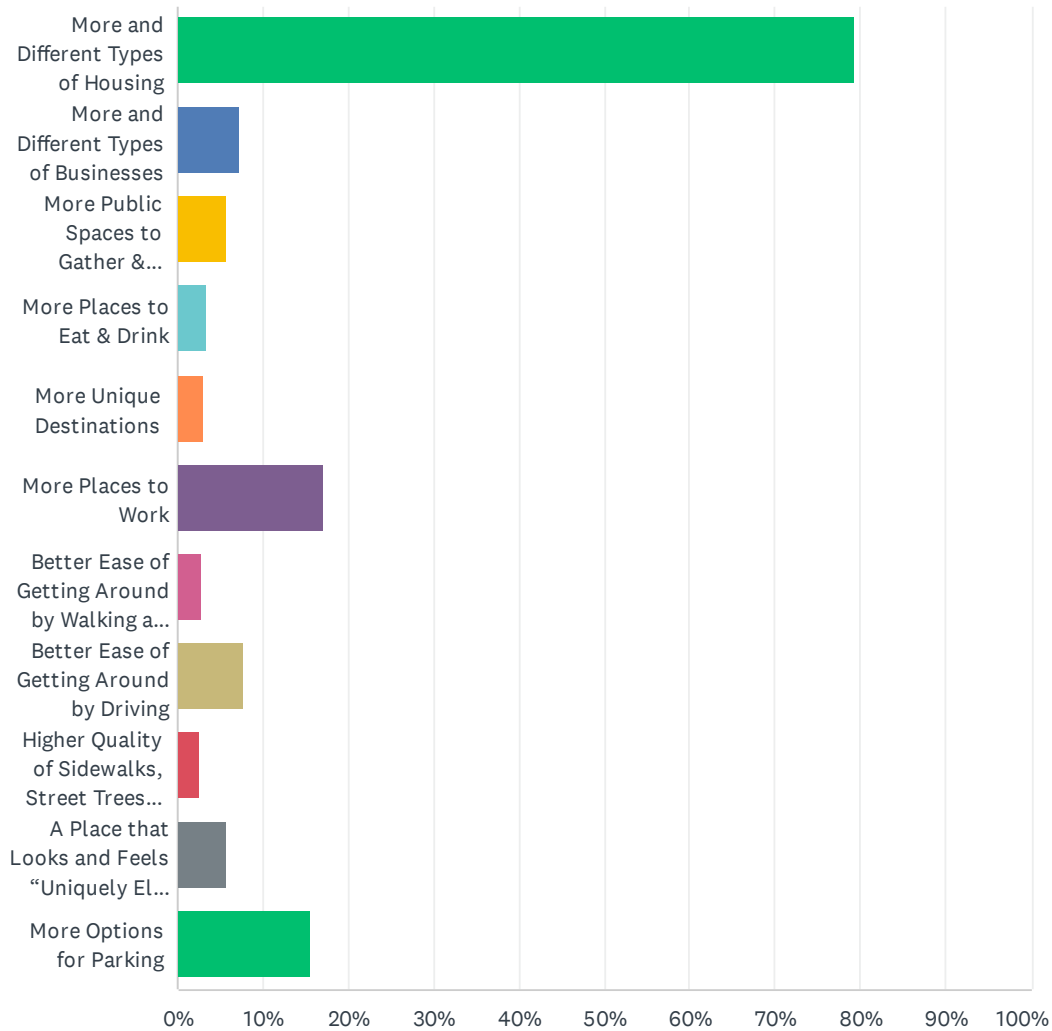


Elk River Downtown Area Plan - Community Survey

ANSWER CHOICES	RESPONSES	
More and Different Types of Housing	1.93%	7
More and Different Types of Businesses	40.88%	148
More Public Spaces to Gather & Recreate	23.76%	86
More Places to Eat & Drink	69.61%	252
More Unique Destinations	37.02%	134
More Places to Work	4.70%	17
Better Ease of Getting Around by Walking and Biking	15.47%	56
Better Ease of Getting Around by Driving	23.48%	85
Higher Quality of Sidewalks, Street Trees, and Overall Aesthetics	21.55%	78
A Place that Looks and Feels "Uniquely Elk River"	24.31%	88
More Options for Parking	38.12%	138
Total Respondents: 362		

**Q10 Thinking of the future, what would you least like to see in Downtown?
List at least one (1) and up to three (3) things you would not like to see.**

Answered: 362 Skipped: 0



Elk River Downtown Area Plan - Community Survey

ANSWER CHOICES	RESPONSES	
More and Different Types of Housing	79.28%	287
More and Different Types of Businesses	7.18%	26
More Public Spaces to Gather & Recreate	5.80%	21
More Places to Eat & Drink	3.31%	12
More Unique Destinations	3.04%	11
More Places to Work	17.13%	62
Better Ease of Getting Around by Walking and Biking	2.76%	10
Better Ease of Getting Around by Driving	7.73%	28
Higher Quality of Sidewalks, Street Trees, and Overall Aesthetics	2.49%	9
A Place that Looks and Feels "Uniquely Elk River"	5.80%	21
More Options for Parking	15.47%	56
Total Respondents: 362		

Q11 Is there anything else you'd like to share about your experience, concerns, or ideas for Downtown?

Answered: 172 Skipped: 190

#	RESPONSES	DATE
1	DO NOT make it a mini rogers.... Rogers sucks. Elk river is nice. Please keep it like that and not develop too much. Thanks. - Out side viewer.	9/18/2024 8:51 PM
2	We hope the historic buildings keep their aesthetic and that more historic buildings aren't torn down! We would also love to see The Pit used for more events throughout the year—ever since Elk River hosted Hockey Day MN in 2014 The Pit has felt neglected and forgotten.	9/15/2024 9:11 PM
3	Easier way to walk or bike across hwy 10	9/15/2024 10:23 AM
4	We should expand past the railroad also	9/12/2024 9:49 PM
5	Need more places to eat/drink that over look the scenic views of the river	9/12/2024 4:28 PM
6	We need way more places to eat around downtown elk river.	9/12/2024 4:21 PM
7	Would love to see a variety of retail and restaurants in the area. Things that bring people to the area.	9/12/2024 1:08 PM
8	Somehow, both sides of Hwy 10 need to be incorporated. Maybe a tunnel could go in under Hwy 10 and the tracks at Jackson and make Jackson pedestrian only. Then, move the parking to the north side of the tracks. That frees up Downtown for more shops. There's plenty of room between Jackson and Irving on Railroad Drive.	9/12/2024 12:29 PM
9	The bridge over the river has terrible access to downtown. It's hard to see what is coming when turning left from the bridge. Traffic thru town and past the bridge goes way too fast. I suggest a roundabout by taking out the bank and creating a better Traffic flow.	9/7/2024 12:25 PM
10	Issues with 3 way stop crossing when walking. Would like to see 4 way stop by dentist of elk river and tattoo places.	9/6/2024 8:47 PM
11	The parking areas along the river have high potential for conversion to public spaces that highlight the beauty of the river and ties in with a trail system. North of this along Main would be highly valuable mixed use residential with river views and street level retail.	9/6/2024 5:26 PM
12	It would be great to have businesses and gathering places that actually have views of the river. A mix of businesses that are destinations - shopping, dining - as opposed to services.	9/6/2024 5:00 PM
13	Driving is an overall nightmare, especially during the morning and evening commute. The stoplights should be replaced with roundabouts and if possible there should be an overhaul and redesign of the streets and parking.	9/6/2024 2:36 PM
14	I do not want unique places to be torn down to have cookie cutter apartment buildings put up. I want the charm of Elk River to stay the way it is. I would like a couple more restaurants and cute shops to move in there. When they took out the little corner store and put in a dojo that was disappointing.	9/6/2024 9:44 AM
15	Don't modernize with cheap new construction buildings. Leave it alone. Fix the river shoreline by adding proper vegetation.	9/6/2024 6:49 AM
16	Intersection at Jackson and Hwy 10 is extremely dangerous, especially driving S on Jackson towards downtown - cars constantly get locked up in the intersection and are unable to go straight through while waiting for turning cars. Take out parking in front of businesses there and create turn lanes!! So many accidents and near misses at that intersection.	9/5/2024 10:29 PM
17	Please provide bathrooms not satellites. Other communities do...What can be done about the blind left turn onto Main Street from Parrish- horrible, get rid of a parking spot to increase safety. Keep having family events downtown- maybe shuttle folks	9/5/2024 10:07 PM

Elk River Downtown Area Plan - Community Survey

18	I like that there was a "more places to work" option. I feel like the only place I can come and stay a while is The Edge, but I honestly don't like Edge so I don't stay. I wish there were more places that weren't just sit for a meal and leave. But I LOVE our local restaurants, too. All of them.	9/5/2024 9:37 PM
19	Get the farmers market out of downtown!	9/5/2024 5:34 PM
20	Getting to Elk River from Otsego is tricky when driving. The stop sign after crossing the bridge and trying to turn into downtown is a challenge. It would be great to see this updated...maybe a round about or something different would be great.	9/5/2024 4:08 PM
21	Would like to see more variety of restaurants, including Asian and vegetarian/ vegan options. Even though parking is usually adequate, during the farmers market parking can be an issue as well as general ability to get around in the area. Consider moving the farmers market to another location that would offer more parking / mobility.	9/5/2024 11:57 AM
22	I would like downtown to be family-oriented and a place that friends and family can go to support local businesses while enjoying the place we call home.	9/5/2024 10:43 AM
23	Public restrooms with running water at the park.	9/5/2024 10:36 AM
24	Add rooftop restaurants with river views, and clean up the shoreline. Elk River should make paths down by the river and clean up some if the fallen trees ect.	9/5/2024 10:05 AM
25	People complain about street parking therefore they avoid visiting the area, but they dont realize parking options are free for blocks. I do not want to see a parking ramp clog up the streets, its a historic astetic and nature centered street, the elite apartments already took over the river and quaint look of the area. We need to be proud of our old main street stores and restaurants instead of hide them or remove them. Let's not turn into a drive thru city like rogers and osseo and ramsey.	9/5/2024 9:45 AM
26	Bulldoze it and take out the stoplights. Elk River is not a destination nor is 2 blocks of shopping and eating a "Downtown"	9/5/2024 8:46 AM
27	Would love a public playground. Possibly in Hankee pit. For the kids who live in the downtown area, there is no walkable public playground or park. The pit sits empty all spring/summer/fall long. I would love the farmers market or music or both be moved to a different night as Thursdays are busy with cabin traffic as is.	9/5/2024 8:42 AM
28	We love downtown and the local shops. Please continue to encourage local business.	9/4/2024 11:33 AM
29	The north side of U.S. 10 corridor from Main Street to Norfolk Ave. REALLY needs a facelift. Go visit Arlington Heights, Illinois charming downtown that developed around the railroad station.	9/3/2024 9:14 AM
30	The Parrish road intersection is terrible. Is there anyway to put a light in? I see near accidents there every day.	9/1/2024 12:35 PM
31	I lived in this town for 17 years and this is the most beautiful part of the town. Don't ruin it.	8/31/2024 1:13 AM
32	Nowhere to eat. General aesthetic is "rundown" or "old"	8/30/2024 6:10 PM
33	Some businesses need to spruce up their exterior - they look like they haven't been updated in years	8/30/2024 5:57 PM
34	We need more inclusivity and diversity! All should feel welcome in downtown Elk River, and be able to enjoy, including dietary restrictions, accessibility, etc.	8/30/2024 12:58 PM
35	I like the idea of closing Jackson and making it a public place. Unfortunately you will sacrifice some driving and parking	8/30/2024 12:21 PM
36	Regular restrooms for Rivers Edge park	8/30/2024 11:35 AM
37	More places to eat and drink would definitely get me and my family to downtown more. It is such a great location but not a lot that gets us there right now and we have to drive based on where we live.	8/30/2024 8:53 AM
38	Unique shops and the views are awesome. They keep the feel of small town and are unique to Elk River The apartments are either too pricey for most people or to much like cheap public housing. It's not a good mix. Theirs nothing in between.	8/30/2024 8:46 AM

Elk River Downtown Area Plan - Community Survey

39	The farmers market needs chairs and tables to visit and recreate, maybe some food trucks	8/29/2024 9:57 PM
40	Needs a legit, easily-accessible, public parking ramp.	8/29/2024 3:38 PM
41	Traffic is heavy and there no where to secure bikes	8/29/2024 12:42 PM
42	Please fix the corner it's hard to see with the parking and traffic coming from all sides	8/28/2024 10:29 PM
43	Interactive businesses/wntertainment (I.e. bowling, game room (Dave and buster like), indoor waterpark), other winter indoor activities.	8/28/2024 8:56 PM
44	I love downtown Elk River, but when events are happening it can be difficult to get around and find a parking spot. Maybe more local shops or a new restaurant.	8/28/2024 7:02 PM
45	We love downtown Elk River & specifically decided to live here because we wanted a small town with an old downtown area. Would love to see more restaurants downtown! We love the addition of the Edge & would love to see more unique local places like that.	8/28/2024 5:17 PM
46	The bridge off parish backs up so badly I would think that would be the first priority before anything else can really be addressed. The bridge traffic backs up being able to access downtown or causes me to drive around it to avoid.	8/28/2024 4:19 PM
47	It would be great to have more food options. The loss of Daddyo's was huge!	8/28/2024 1:08 PM
48	I feel like I'm always heading out to neighboring towns to find the good dining experiences that aren't a chain or fast food. We have a couple great places to eat here, but not enough in my opinion!	8/28/2024 12:59 PM
49	Make the area of Main Street (by the shops) easier to get through by vehicle. The area backs up in rush hour and becomes almost impossible to get through.	8/28/2024 12:06 PM
50	Rivers Edge Commons Park needs to have a public restroom and indoor changing area for the weddings and bands	8/28/2024 12:01 PM
51	We need more modern stores in elk River, like Homegoods / marshals etc	8/28/2024 11:53 AM
52	Need a grade separated pedestrian crossing at adjacent to Main Street over TH10 and RR tracks.	8/28/2024 10:17 AM
53	A high quality sandwich shop with options to order at counter then either sit inside and eat, or take to go would be fabulous!	8/28/2024 10:03 AM
54	Don't lose, keep and maintain the Historic bldgs and vibrant Elk River history.	8/28/2024 9:50 AM
55	Leave it alone, looks great how it is.	8/28/2024 9:49 AM
56	We look for family-friendly and kid-friendly things to do, and fun outdoor spaces	8/28/2024 9:44 AM
57	Would love to see roof top decks on restaurants located on the river. Better views.	8/28/2024 9:33 AM
58	That intersection by 10 and Wright county bridge 42. That needs something different there not sure if a round about would be good or something different. Hopefully when 169 gets done it gets better but I still see it will be busy.	8/28/2024 9:23 AM
59	I'd love for downtown to utilize the riverfront for areas to sit and have a drink with friends to enjoy the beautiful setting	8/28/2024 9:19 AM
60	Please fix the traffic situation with the stoplights on highway 10 and the roads to it.	8/28/2024 9:16 AM
61	Would love to see some existing businesses spruce up external appearance to add to the overall look and feel of downtown. Some businesses look great while others are falling apart.	8/27/2024 9:23 PM
62	The hardest part about getting downtown by bike and walking is the connection to my neighborhood to the trail system. Biking or walking along County 1 or 33 is not safe to get to a trail. There is not enough parking when there are large events. I have not been to the entertainment for many years due to the lack of parking and not enough seating.	8/27/2024 6:13 PM
63	Try to get business to take the vacant areas and more restaurants to make a more reason to go downtown	8/27/2024 2:11 PM
64	I feel like so many businesses are leaving Elk River and very few are looking at Elk River. There is little retail shopping, the variety of sit-down, casual dining establishments is limited. It	8/26/2024 8:22 PM

Elk River Downtown Area Plan - Community Survey

	seems like Elk River is a drive-thru town vs a destination.	
65	Better use of the riverfront for the public	8/26/2024 2:43 PM
66	We moved here after 30 years in St. Cloud. The attraction for us was the downtown. We live downtown so we don't have to park on the streets but watching all the free on street parking fill up in the morning and people walking around town is what attracted us here. Downtown St. Cloud lost a lot of that when they redeveloped that area. Some of our favorite businesses had to close. It's great that your asking for input. Thank you	8/26/2024 10:53 AM
67	Develop a river walk -- the river is Elk River's biggest asset. Also, hold more events (art fests, food fests) to get people to go downtown and feel more like a community.	8/25/2024 4:19 PM
68	You have a unique opportunity of making a cute downtown with shops, restaurants etc. Don't bog down with housing and business. Make it very walkable and a destination for tourists to come visit. Don't have it past the train tracks either that is idiotic. If your intent is to add, maximize the footprint between Jackson and main going towards Handke. I wouldn't even allow tenants that aren't part of the vibe you want. Chiropractors, law offices and insurance agencies should go somewhere else. Think of Nisswa, stillwater and other high traffic little downtowns that attract people. Make it a destination.	8/25/2024 3:05 PM
69	I would love to see more options for sit down restaurants, especially with views of the river. Daddy-os would be a great option for for a fun new restaurant, possible roof top patio. I think capitalizing on the River access will help drive revenue to local businesses. Looking for fun, local food eateries with that small town feel (Nissawa town vibes come to mind) - easy to walk around, shop, and find a place to stop to eat or munch on the food you picked up. The accounting, insurance, attorney, etc businesses - I would like to see them moved to either a second story in downtown proper if that's where they currently are or move all of those types further down main, closer to 169.	8/25/2024 2:39 PM
70	Going in the right direction. However the karate business seems like a wasted opportunity to attract tourism and commerce to the area overall. Love the book store!	8/25/2024 12:50 PM
71	I would like to see more restaurants with outdoor spaces overlooking the river. The intersection main and parish needs to be improved.	8/25/2024 9:34 AM
72	More usable greenspace	8/24/2024 3:02 PM
73	Move the Thursday concerts to lake orono so people that work can find a place to park and sit,people putting up chairs all day while people are at work is BS.	8/23/2024 11:01 PM
74	This city council has 'progressed' and 'developed' a beautiful small town with natural areas, into a concrete, overcrowded jungle. STOP REDEVELOPING and start considering the environmental impact of the continual influx of people and developers.	8/23/2024 4:05 PM
75	It would be wonderful to have more social options like downtown Anoka! It was a huge draw when had the brewery downtown.	8/23/2024 12:32 PM
76	Driving and parking are difficult. It is cramped and the Parrish intersection needs to be fixed. There needs to be traffic control there.	8/23/2024 12:04 PM
77	A long time ago there was a convenience store--Ron's. I really miss that. It would be good to have more everyday grocery options on the western side of town. Also miss the gift shop @ Kemper's.	8/23/2024 11:23 AM
78	Pedestrian and bicycle access from the south is very poor even for people who would otherwise be in walking distance. I live by the train station and to get to downtown I have to walk all the way down Twin Lakes to Main Street even though it's like a mile down the highway.	8/23/2024 11:05 AM
79	I really wish we still had a small grocery/convenience store downtown.	8/23/2024 10:02 AM
80	Thanks	8/23/2024 9:57 AM
81	Please do not add more housing to downtown. It is a very tight location as it is to navigate traffic and it takes away from that small town charm.	8/23/2024 9:32 AM
82	I really enjoy the downtown but too busy with cars at certain times. More parking would ease the situation. I like how the Mississippi River is accessible to visitors and Elk River citizens.	8/23/2024 9:27 AM
83	More shopping and overall better and safer access to downtown	8/23/2024 9:20 AM

Elk River Downtown Area Plan - Community Survey

84	The businesses aren't connected to community and aren't inviting. Insurance agencies, law firms, etc. The bars are insular and unwelcoming if you haven't been going to them for years. We need more businesses like The Edge and Pompeii and Minne Made who want to connect with the public.	8/22/2024 10:48 PM
85	I love downtown. I think keeping it safe but fun is key. I think a park would be nice somewhere as another activity for the kiddos. I think other experiences like the escape room place could also be fun if it was walkable. Maybe an arcade eatery or something.	8/22/2024 1:43 PM
86	Personally moving from Dallas TX to Elk River there is severe lack of food options in the area. Food always brings people together. Beer garden would be lovely.	8/22/2024 11:48 AM
87	Increasing the overall look of downtown. Bring a modern touch. A facade refresh to store fronts, modern artwork and better overall landscape.	8/22/2024 9:12 AM
88	I love the idea of what downtown Elk River is. I think it needs a couple more bars/restaurants and at least one more of those should be open past 11pm. McCoy's is fine, but we would like more options. Farmers market Thursday is one of my favorite days! Maybe more block party type events where you shut down the streets could be fun too!	8/22/2024 8:05 AM
89	There just not much that downtown elk river offers. We find ourselves heading elsewhere to anoka, maple grove, rogers or otsego to the restaurants and shops.	8/21/2024 10:36 PM
90	Bring in businesses. Especially to more than just downtown. Lots of eye soars in the city. I've lived in town for 31 years and very sad.	8/21/2024 10:02 PM
91	Need to utilize the riverfront area better	8/21/2024 9:41 PM
92	More patios for enjoying the River.	8/21/2024 8:14 PM
93	The city needs to add a way for local growers to sell things in town and they also need to stop thinking that the down town area needs to be protected the town town area needs to grow with out city involment	8/21/2024 6:04 PM
94	No section 8 housing More retail More community events / entertainment/ engagement Lights crossing streets	8/21/2024 4:17 PM
95	Modern Restrooms at Rivers Edge Commons Park	8/21/2024 3:51 PM
96	I like a downtown center that can be an entertainment and social district! I love downtown Anoka and want to see Elk River invest in similar ways. Like places to relax and socialize and see the river (either with more accessible shoreline or rooftop patios), hidden unique spaces like Avant Garden Bookstore's patio in Anoka- perfect spot would be if a cafe bought the Elk River Chiropractic building and made a patio in the space behind it that connected to the dock and amphitheater area... also an investment in art! Maybe new murals or sculptures. I'm also eager to see the escape rooms reopen and am excited that the owners might invest in tabletop gaming as well. It would also be cool to see an arcade or play area that is more kid friendly too... and more unique food options like thai or greek. Personally to me there are buildings I wish weren't right in downtown... like the funeral home and the church in the old bank. That space could be developed better.	8/21/2024 3:35 PM
97	More stuff to do.	8/21/2024 2:38 PM
98	The Parrish and Main street "street" needs to be turned into something new. The afternoon traffic is terrible. Build a bridge or something over hwy 10 and the railroad tracks to ease congestion.	8/21/2024 2:28 PM
99	Getting off parish is difficult and the trains back up traffic. Do not need more apartment buildings	8/21/2024 2:11 PM
100	Downtown intersections are not pedestrian friendly. Too much traffic goes through Downtown without Downtown as a destination. Traffic flow should be changed to encourage local traffic only, such as making Jackson one way, changing parish to not be able to go west onto main, lower speed limits in old Downtown, etc.	8/21/2024 1:52 PM
101	Right now getting in and out of "downtown" is cumbersome. So much so I don't stop & linger anymore. It's a place to pass through, sadly. Parking is bad, all businesses have their backs turned to the river. Look at Hudson WI or Stillwater MN as examples to strive for. Those are both river towns that are destinations. Emulate them, they are successfully attracting & keeping people coming there.	8/21/2024 1:48 PM

Elk River Downtown Area Plan - Community Survey

102	Main St and Parrish is an awful intersection. Too small for the amount of traffic it seems. It needs to be improved.	8/21/2024 1:42 PM
103	I feel Elk River, especially the DT area, is dying. Has been for years. If they want to maintain it, I would suggest trying to create it- like Ramsey is. We are not Anoka, we've never had a bustling DT like Anoka. Let's try less to preserve some of what's there to make room for new and attractive businesses/housing/features. You have a river- utilize it more. Spend some \$ and buy up some of the areas between 169/10. I'd rather see \$ going to beautify and create a nice town. I'm sorry, but many of the homes/businesses in that area need to go if possible. It's time to create a city (not a "town"). You can't stop progress, if you try to hold onto the past too much, your city becomes a relic of the past. And not in a good way.	8/21/2024 1:19 PM
104	The traffic and the trains. Between the trains and traffic on Highway 10, often times it backs the traffic up terribly. The intersection by Granite Shores and the Bank of Elk River is so dangerous. It's also too close to Hwy 10. In a perfect world the railroad tracks would move out of downtown.	8/21/2024 1:15 PM
105	Elk River has no unique bars or restaurants. McCoys and The Depot are dives and all the other food options are chain fast food. The city needs to bring in more venues like Big Lake or Zimmerman have to keep residents in town, examples, Willie's on the Water, the Buff, Trails, Ds Smokehouse. Patio seating or venues that host meat raffles, bingo etc. I pay huge taxes in a city I live in and have to leave the city for any form of entertainment.	8/21/2024 1:07 PM
106	Downtown is horrible to get to for people going north from otaego (over the bridge). Something needs to be done with that stoplight situation, especially on Thursdays. I would love to go to the farmer's market but I don't because it is so difficult to get into downtown from any direction.	8/21/2024 12:56 PM
107	I like the farmers market in the summer. A Christmas Market would be really cool.	8/21/2024 12:41 PM
108	Intersection of Hwy10, Main St, and Parish Ave is a disaster.	8/21/2024 12:15 PM
109	Traffic at those lights are horrible. Everyone runs red lights making you worry if you walk or bike or even drive you will get hit. Parking is always full unless you go all the way down past the funeral home and walk down. Love the river and the fountains and the dock there. Love the businesses as well.	8/21/2024 12:06 PM
110	I'd love to see less insurance and bank options and more places to gather or shop at a co-op. A nice dining spot would be really fun as well.	8/20/2024 10:47 AM
111	More Christmas tree lights, more weed and trash control, food truck night, I scream sop, deli and market where mystery place is, American Flag in park, extension of down town through parking lots. Food truck fronts along Main Street parking area, greenery in triangle by Karate on Ten, a few real planted Christmas tree's.	8/20/2024 9:22 AM
112	No pavers, do colored / textured concrete	8/18/2024 8:08 PM
113	We would love a long walking path along the river	8/18/2024 7:23 PM
114	The free public parking lot by the feed store is very unkempt and Un attractive. It does not represent a clean, modern and attractive entrance into our downtown area. It is a bit of an eye sore. The lights down the road by Lake Orono are very pretty. Something like that could look very good there by the public parking too! With some trees and planters with flowers like the downtown area has. That would be a beautiful welcome!	8/18/2024 3:19 PM
115	I would love to see a farm to table restaurant or a fun brewery! I love having options for having a night out! I love edge coffee, old main, and sunshine depot but I would love to see more!	8/18/2024 12:47 PM
116	Bike path too babcock. More lights at babcock, especially the boat launch. A small parking ramp in downtown would be ideal.	8/17/2024 4:27 PM
117	Elk River needs to bring in more retail, dining and boutique businesses. Avoid franchises, and commercial businesses. The city needs its residents to stay, invest and spend money in the city, and attract people passing through to want to stop and spend money at the local shops. Instead as a community we are forced to leave the area to all the surrounding cities for all our basic needs as well as all the additional things we buy. Liquor is the only exception. Elk River apparently has a need for 3+ liquor stores...	8/16/2024 9:20 PM
118	All of the businesses are, not even historic, but just old. As part of younger folk, we would like to see more updated businesses appearances, a business/restaurant chain on this side of hwy	8/16/2024 3:22 PM

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10, or something that people under 40 years-old can enjoy. And traffic is awful, especially the turn onto Parrish.

119	Downtown should have more places to go and enjoy yourself not service places such as karate massage places, or hair places, maybe more unique stores and restaurants.	8/16/2024 11:41 AM
120	I wish the church building on the river was a food & drink, art or some kind of gathering space. It's under utilized as a church. I really wish there was a walking/biking path that connects Bailey point and Babcock Park along the river. This would make the city a destination to enjoy the river. We should embrace the river like Anoka & Stillwater do. I would love to see more boutique shopping, deli, speakeasy, gift shops. Less insurance, investment, office spaces.	8/16/2024 7:44 AM
121	Elk River Downtown has an underutilized resource in the river/views and potential as a unique destination that could serve as a "3rd place" (home, work, 3rd place to go to for entertainment, food/beverages/recreation/socializing). I'd like to see the downtown area as a hub for trails for walking and biking - separating vehicle traffic to make this a safer and more pleasant and appealing destination. See Seattle's waterfront as an example of a destination area with entertainment, dining, shops connected by walking and biking paths. Connecting the wonderful trails (Woodland, along Highway 10, East of 169) perhaps with bike/walking paths going behind the schools on School Street and connecting these trails to downtown as a hub. Budgeting for excellent wayfinding signs would be essential to encourage use. Invite bike/scooter rental companies to locate in the community. A public transit electric tram/bus route to include the Northstar Park and Ride, Walmart, Coburns, Cub, School Street, Aegir's Brewery, Downtown and/or similar higher volume stops. Eliminating vehicle traffic in the core Downtown and rerouting access/parking for those living/delivering and with disabilities. Reorienting businesses to river views and adding 3/4 season patios... Move parking away from downtown and provide bike and pedestrian access that does not require crossing vehicle traffic (bridge/tunnel). Encourage/support additional retail, recreation, dining in the downtown area - places like a late night coffee shop, specialty foods, artist studios, bicycle repair, train store, game night gathering space, smaller specialty shops... The Downtown Plan should create a space that both invites residents to spend more time in the downtown and draws people from across the state for a unique and memorable visit.	8/15/2024 7:23 PM
122	Highway 10 makes downtown uneasy with the amount of traffic. A risk of crossing 10. I'm 50 years old and I don't want to cross it. Also putting the train station outside of Elk River was dumbest thing. Should have put it where the lions and rental place is. Having people to come directly into downtown to spend money and have downtown grow should've been a no brainer.	8/15/2024 3:57 PM
123	Terrible planning in past	8/15/2024 9:53 AM
124	Traffic congestion and extreme lack of parking will continue to keep people avoiding downtown. Sips on Main is a great idea but needs a new location. Hot pavement in August is a horrible idea.	8/15/2024 9:21 AM
125	Farmers market location should be moved- it's too congested and traffic patterns for those who live in that area is horrible.	8/15/2024 9:03 AM
126	Some complain about the cost of the flowers in the summer. I disagree with them...I love the "old town" friendly feel. So many of those things are being lost. Love to see old photos of towns with the holiday decor along main streets. However, I don't live in Elk River do I don't have to pay for it.	8/15/2024 8:04 AM
127	It would be amazing if from Studio 107 to Edge Coffee were lil shops. A safer way to cross Hwy 10. More parking.	8/15/2024 8:04 AM
128	I saw tax payer dollars went to pay for flowers for downtown. That is an abomination! If they businesses want flowers they should pay for it out of their own pocket- NOT TAX DOLLARS!!!	8/15/2024 8:03 AM
129	People use Main Street and Jackson as through streets to get through town rather than TO town. Figure out how to keep that traffic out of downtown and it would add to the appeal tremendously!	8/15/2024 8:03 AM
130	Change the amphitheater area for better seating that is level and has actual seats with backs instead of uneven grass and slabs of rock. The current businesses and restaurants are not anything that I am interested in with the exception of the dental office. Nicer (clean, updated) restaurants is really what we need.	8/15/2024 7:59 AM
131	Need to absolutely fix the intersection of Parrish Ave and Main St. It's absolutely horrible and	8/15/2024 7:49 AM

Elk River Downtown Area Plan - Community Survey

why I avoid downtown Elk River.

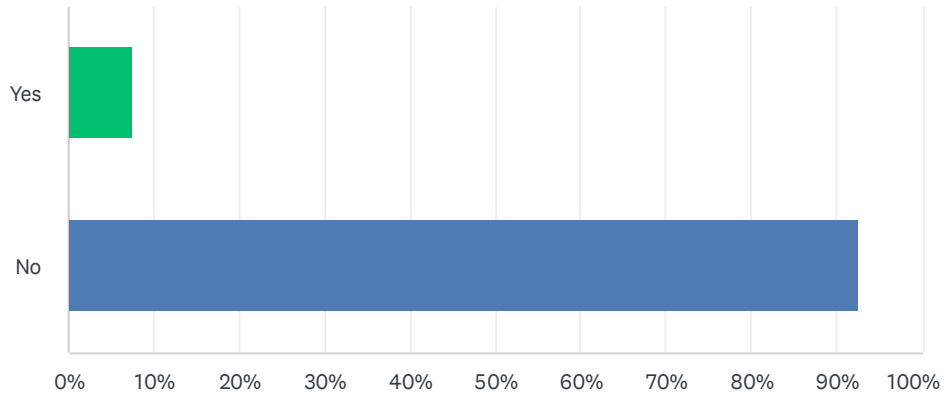
132	Parking is a nightmare. Farmers market is great but basically closes a parking lot and creates problems. Thursday nights traffic is horrible at 5-7pm because of the market and concerts which makes it unappealing to return. Need to face the facts that downtown is really along the river and not on the other side of HWY 10	8/15/2024 7:47 AM
133	Downtown lost its charm years ago.. losing Kempers and meat market was final straw..	8/15/2024 7:36 AM
134	More eating and gathering places.	8/14/2024 10:40 AM
135	As a new small business owner in downtown I would love to see more businesses come in that will attract people to the downtown area (unique shops/restaurants/activities as opposed to private offices). As a business on Jackson I would love to see Jackson turned into a one way in order to make parking available on both sides and ideally the removal of the rail on the east side to switch to something more inviting.	8/14/2024 7:29 AM
136	Cute town. Enjoy what has already been updated. Please. KEEP historic buildings. So many torn down already. 😞	8/13/2024 10:10 AM
137	A trail or walkway that goes along the river for people who like to do walks.	8/13/2024 9:29 AM
138	I like how Downtown gets decorated for the different holidays!	8/13/2024 9:14 AM
139	Don't remove the old part of elk River from this area, if you do there will be nothing original left of this town.	8/13/2024 5:09 AM
140	The parking is terrible.	8/12/2024 10:57 PM
141	It's cute, I love the concerts in the summer and the fountain. More dining options or a business for social gathering would be nice- I'm thinking a smoothie or wine place? Definitely not a chain restaurant- just some place to go.	8/12/2024 10:09 PM
142	I would love to see some fun shops and restaurants	8/12/2024 9:48 PM
143	Love the downtown but parking is the biggest issue. On a busy weekend or farmers market days, parking can be difficult.	8/12/2024 9:46 PM
144	Enforcement of 30mph through town, restaurant in the Daddy Os location with better quality food options like Pour (Otsego), Main Street Farmer (St Michael) or Big Bore (Hanover).	8/12/2024 9:46 PM
145	Riverside dining. Patios riverside	8/12/2024 12:53 PM
146	Transit would be unique	8/10/2024 8:39 PM
147	Elk River should look to make Downtown like the City of Anoka in terms of restaurants, bars, and entertainment. Current offerings in downtown are substandard and many of the business serve no general benefit to downtown visitors.	8/10/2024 5:38 PM
148	Parking is the issue! There should be a parking ramp - having parking across Hwy 10 is ridiculous. If you create parking, businesses and people will come.	8/9/2024 8:53 AM
149	No more salons, bars, or agencies. Bring in businesses that will draw people who spend time in small towns. Used book store, cozy coffee shop, and different types of small town shops are great ideas. Parking street-side is a pain, and with a lot of new spaces to park, it's not really overly necessary, except for handicap access. Also, increasing the draw to the river could be big. There is very little access to the actual edge of the water and no grass area to sit next to the river. Construction sucks all-around, but if there's some way that the construction traffic can be redirected, businesses will do better.	8/9/2024 12:52 AM
150	ER downtown is right on the river- there should be many modern restaurants/bars (with live music) overlooking that view - Rogers R-Social and many bar/restaurants in Anoka are great examples. We drive to those places and pass by ER down town and that's a shame - I'm glad this survey is addressing the issue.	8/8/2024 10:01 PM
151	Parking is an issue	8/7/2024 7:24 AM
152	NA	8/6/2024 9:41 AM
153	Public restroom is a necessity with the amount of downtown events and activities. Still hoping for one in the old meat market location.	8/6/2024 9:01 AM

Elk River Downtown Area Plan - Community Survey

154	na	8/6/2024 8:43 AM
155	A public restroom would be a great asset to downtown. A higher end restaurant/cocktail bar is a must. No more cobblestone, maybe stamped concrete. Reconfiguration of Main/Parrish/Hwy 10. Consider one way on Jackson. Thank you!	8/6/2024 8:19 AM
156	I believe they have turned away a lot of businesses and proposals, including one from Tippy Chicken. The "old school" businesses have kept new businesses from coming in. We have lost a lot of opportunities to Otsego as a result. They need to make Jackson Street a one way. Make parking available on the other side, take down the railing and even out the sidewalks so those businesses can survive. We need more shopping and dining downtown, not another Karate studio or bank, neither of those bring foot traffic into downtown. The small town mentality needs to change and those decision makers need to see that bringing in more shopping and dining, only benefits them in the long run as well. I don't want to see us lose more businesses to neighboring cities.	8/5/2024 9:20 PM
157	I love downtown but walking and biking across highway 10 is an absolute no! More restaurants and unique shops would be perfect as this area could easily be a great destination town!!	8/5/2024 9:08 PM
158	More retail businesses would be wonderful. A parking ramp would be nice as well.	8/5/2024 9:01 PM
159	Would love to see a round about or three way stop at main start and Parrish. That is a terrible intersection.	8/5/2024 8:14 PM
160	Continuing to have a place that feels warm, welcoming, uplifting, a place people say they want to go to, more activities that can bring people to our downtown, making our downtown a hallmark village.	8/5/2024 7:55 PM
161	No one parks across highway 10, it's not safe walking across the highway.	8/5/2024 6:56 PM
162	The ease of access to businesses on Jackson Ave need improvement	8/5/2024 5:44 PM
163	I love living in the downtown area. But losing the drug store was really difficult. We NEED someplace to buy a pop, a newspaper, a dozen eggs, etc. We could really use a market of some sort. I think it would complete the feel of this area.	8/4/2024 1:44 PM
164	I would love to see a convenience store or grocery store that sells basic household essentials, fresh produce, and other items that you normally have to drive to get.	8/2/2024 1:15 PM
165	The way the rail interacts with the rest of the infrastructure causes lots of problems. This causes downtown to feel disjointed from the rest of elk River. Living downtown we drive into the nearby neighborhood just to avoid the lots on 10 which are controlled by the rail since the cycle times can get excessive at times. It would be expensive but grade separating the rail would eliminate most of the issues	8/2/2024 1:03 PM
166	It would be nice to have more of a walking path near the river. The rivers edge park offers a nice view, but that's really the only place you can enjoy the river.	7/30/2024 10:35 AM
167	I like that the downtown has a high enough volume of businesses and events (farmers market) to make it fun to walk around. I do think pedestrians are difficult to see with cars parked directly next to crosswalks and there are "dead zones" like by the new bookstore that are harder to get to and feel isolated which makes them less appealing to walk to (the area around them is only pavement with a road next to it). I think having refuge islands would help. Maybe more green space in the median? If there was an easier way to get to downtown from the Hwy 10 side that would improve life.	7/29/2024 11:23 AM
168	Main Street and Parish intersection needs to be safer/ easier to navigate.	7/27/2024 3:34 PM
169	Keep the historic buildings, get the downtown original buildings or blocks on the National Historic Register.	7/27/2024 1:33 PM
170	Don't need any more housing built	7/27/2024 12:45 PM
171	I'm looking forward to some of the riverfront businesses turning over (ex the funeral home..) and being developed into entertainment spaces/ restaurants that utilize the riverfront view.	7/25/2024 8:05 PM
172	Italian restaurant would be nice. Something other than bar food. Erase the stop lights on highway ten.	7/25/2024 4:44 PM

Q12 Do you currently live in Downtown?

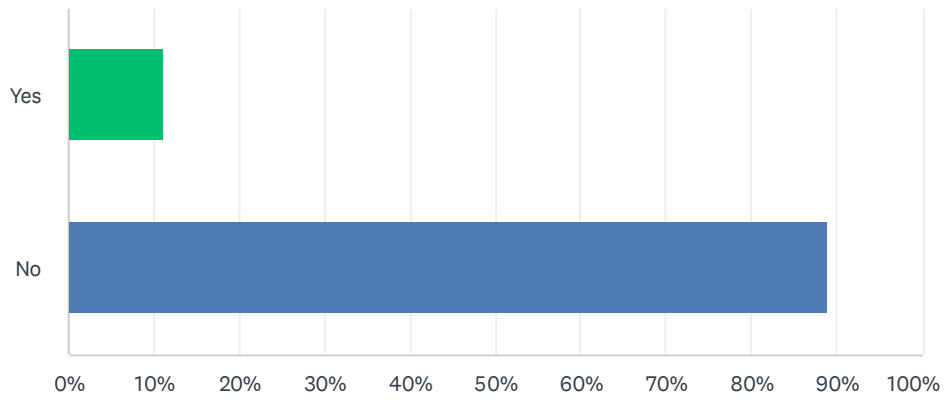
Answered: 361 Skipped: 1



ANSWER CHOICES	RESPONSES	
Yes	7.48%	27
No	92.52%	334
TOTAL		361

Q13 Do you currently work in Downtown?

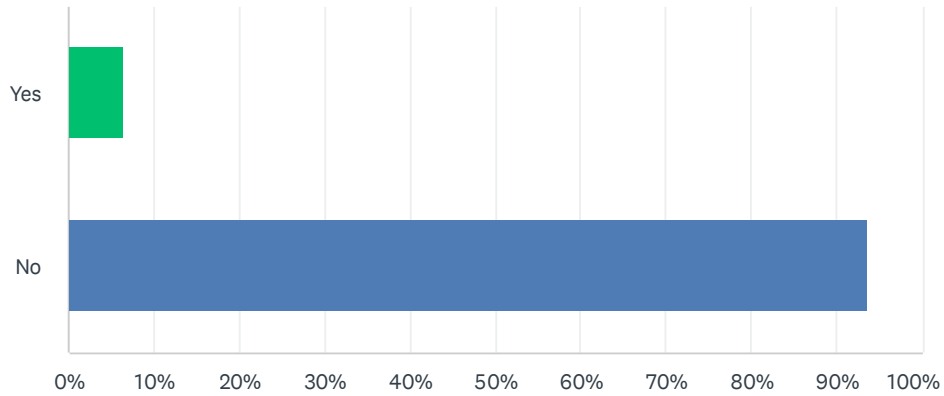
Answered: 361 Skipped: 1



ANSWER CHOICES	RESPONSES	
Yes	11.08%	40
No	88.92%	321
TOTAL		361

Q14 Do you own a business or property in Downtown?

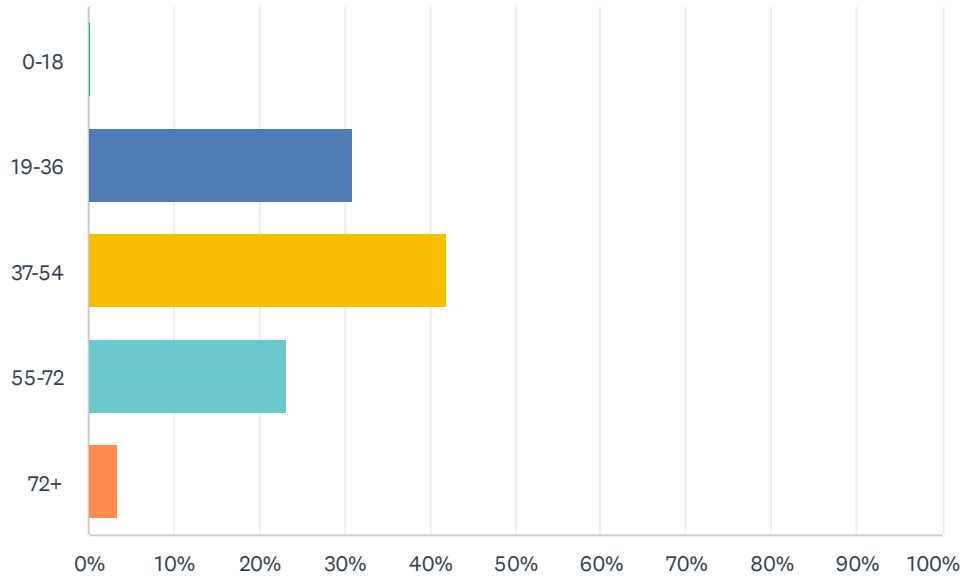
Answered: 360 Skipped: 2



ANSWER CHOICES	RESPONSES	
Yes	6.39%	23
No	93.61%	337
TOTAL		360

Q15 What is your age (optional)?

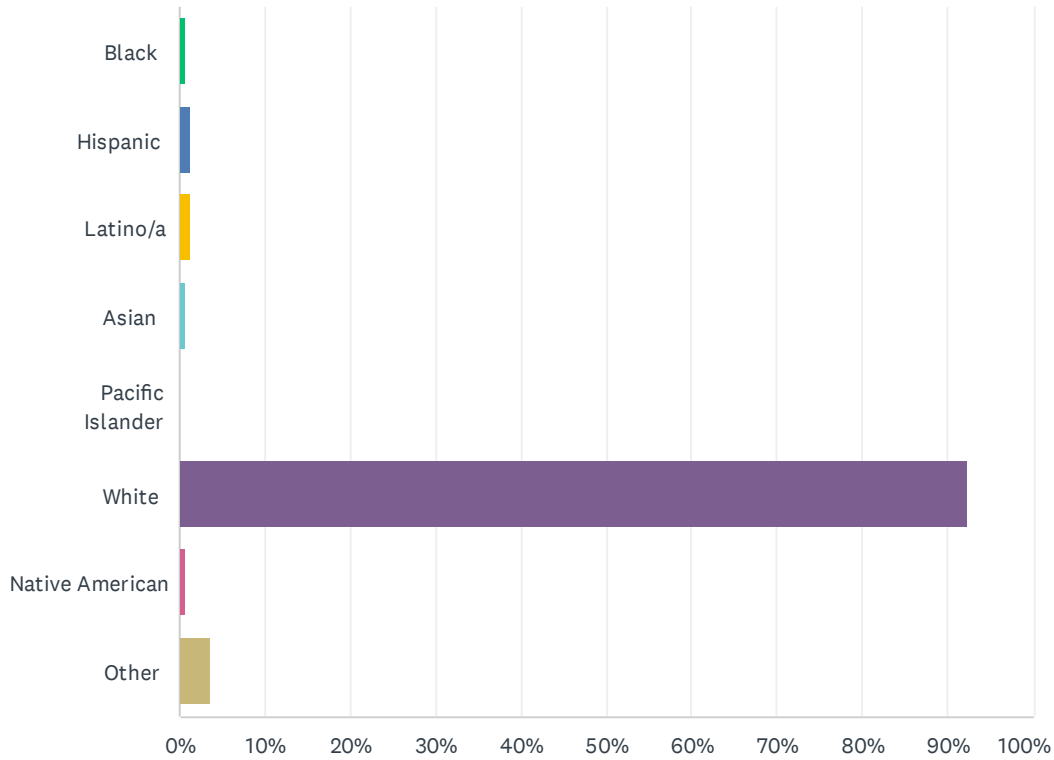
Answered: 347 Skipped: 15



ANSWER CHOICES	RESPONSES
0-18	0.29% 1
19-36	30.84% 107
37-54	42.07% 146
55-72	23.34% 81
72+	3.46% 12
TOTAL	347

Q16 How do you identify (optional)?

Answered: 335 Skipped: 27



ANSWER CHOICES	RESPONSES
Black	0.60% 2
Hispanic	1.19% 4
Latino/a	1.19% 4
Asian	0.60% 2
Pacific Islander	0.00% 0
White	92.24% 309
Native American	0.60% 2
Other	3.58% 12
TOTAL	335

Elk River Downtown

Recommendations & Implementation Strategies

Case Studies

Downtown Stillwater – Stillwater, MN

Categories: Hotel, riverfront revitalization, placemaking

Major Outcomes: New construction, including multiple hotels, spurred by the creation of a new bridge and conversion of the old bridge into part of a pedestrian and bike network.

Description:

Stillwater is a small tourism-driven town in the bluff country of eastern Minnesota. Prior to the closure of the 87-year-old lift bridge in 2017, 18,000 cars crossed the aging bridge daily, bringing visitors to the city's shopping and dining establishments downtown. The bridge closure came on the heels of the opening of the St. Croix Crossing south of downtown. In 2018, two new hotels opened in downtown Stillwater and a third hotel broke ground on an expansion. The new hotels include Hotel Crosby (55 rooms) and the Lora hotel (40 rooms).

In addition to new hotel construction, owners of retail stores downtown embarked on façade improvements of 19th century buildings and in 2019 Wolf Development broke ground on the adaptive reuse of the town's historic armory, which was converted into apartments and commercial space (Fieldhouse 34). These investments reflected the strong economic climate as well as the increased ease of pedestrian access to downtown due to the bridge closure.

Figure 1. Stillwater's Historic Lift Bridge, Now Part of St. Croix Crossing Loop Trail



Source: Twin Cities Outdoors.

Figure 2. The Lora Hotel



Source: Lora Hotel.

Figure 3. Interior of Fieldhouse 34 in the Historic Armory Building



Source: Historic Fieldhouse 34.

Figure 4. Rooftop Patio Overlooking the River at Brick and Bourbon in Downtown Stillwater



Source: Discover Stillwater.

Artists on Main Street – Willmar, MN

Categories: Placemaking, Events

Major Outcomes: Increased visits to Downtown throughout the year; community-building

Description:

Artists on Main Street is a program operated by local organizations, Rethos and Springboard for the Arts. The organizations work with cities to bring art and art-related events downtown, focusing on small and rural cities. Member cities receive funding and resources for the creation of art-based physical, economic, and social solutions for downtown.

One of the current participating cities is Willmar, MN, located in Kandiyohi County southwest of St. Cloud. The City's approach to using the resources provided by the program involves having "small and many" events to attract a wide variety of residents downtown throughout the year with a goal of creating community. In its first year, Willmar received \$20,000 from the program, which funded 13 projects. These included murals, seating areas, art workshops, and a community picnic. In its second year, the City aimed to utilize the program to bolster public outreach for its Comprehensive Plan update. City Planner Sarah Swedburg emphasized that in bringing people downtown for events throughout the year, it built downtown's identity as a place to meet friends and take part in community.

Figure 5. "Wilmar...What Comes to Your Mind" Art Event in 2019



Source: Rethos.

Figure 6. Residents Helping to Paint the Collaborative Downtown Mural Medallion in 2019



Source: Rethos.

Grain District Downtown Redevelopment Plan – Bondurant, IA

Categories: Hotel, Placemaking

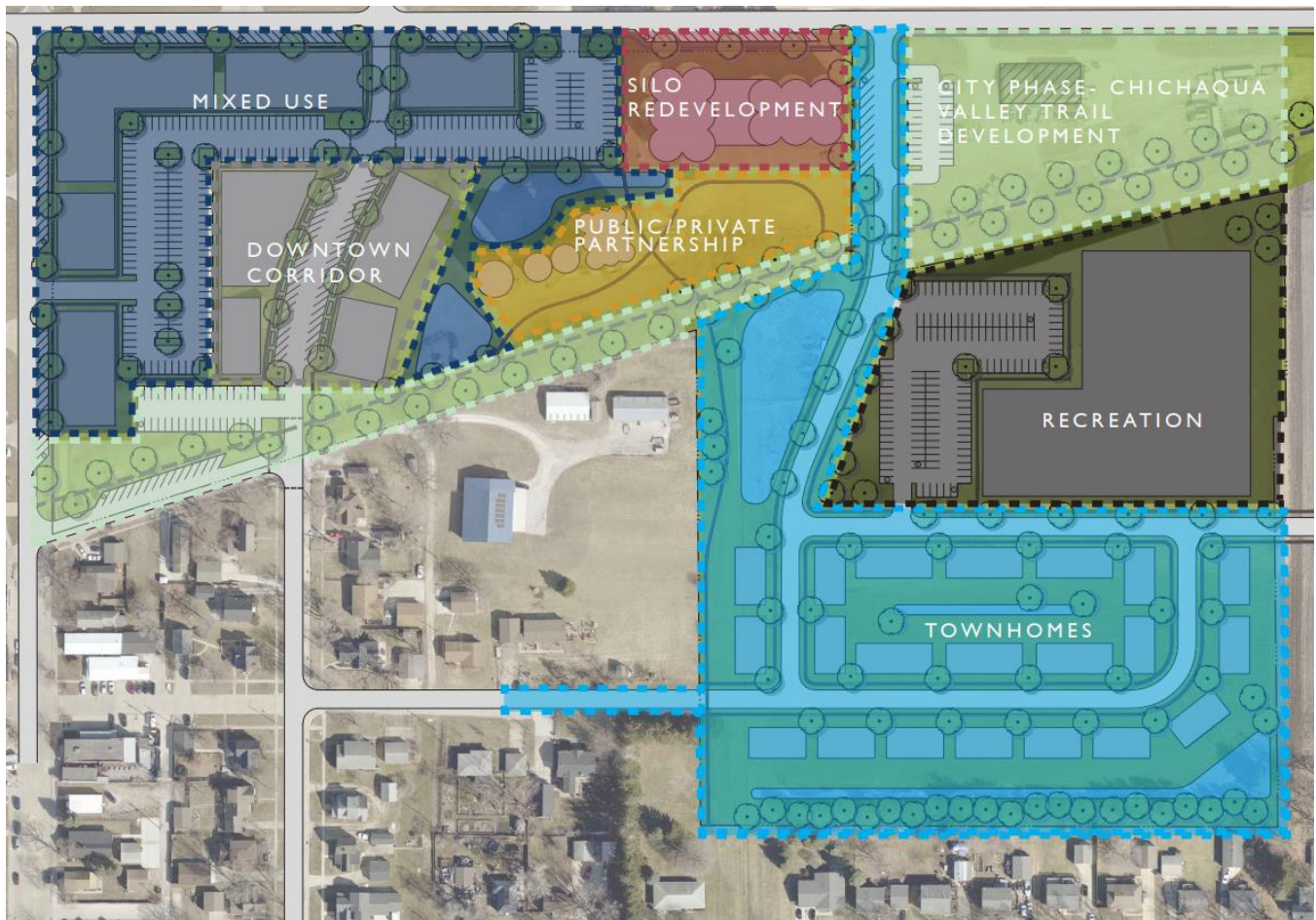
Major Outcomes: Public-private partnership for redevelopment and adaptive reuse of a historic grain silo; public investment in infrastructure, placemaking, and events.

Description:

Bondurant, a city of just over 7,000 residents located outside of Des Moines, adopted a plan in 2024 to redevelop the Grain District, an area adjacent to its downtown. The district was previously home to the Landus Cooperative and features a 140-foot tall grain silo. In 2024, the City released a plan for the grain district that includes townhomes, mixed-use (residential over retail), a park with a sculpture garden and stormwater retention ponds, a recreation center, and a repurposed railroad trail. The local buyer of the Grain District site, The 101 LLC, approached the City with the idea of sharing the cost of the plan adoption and creation process.

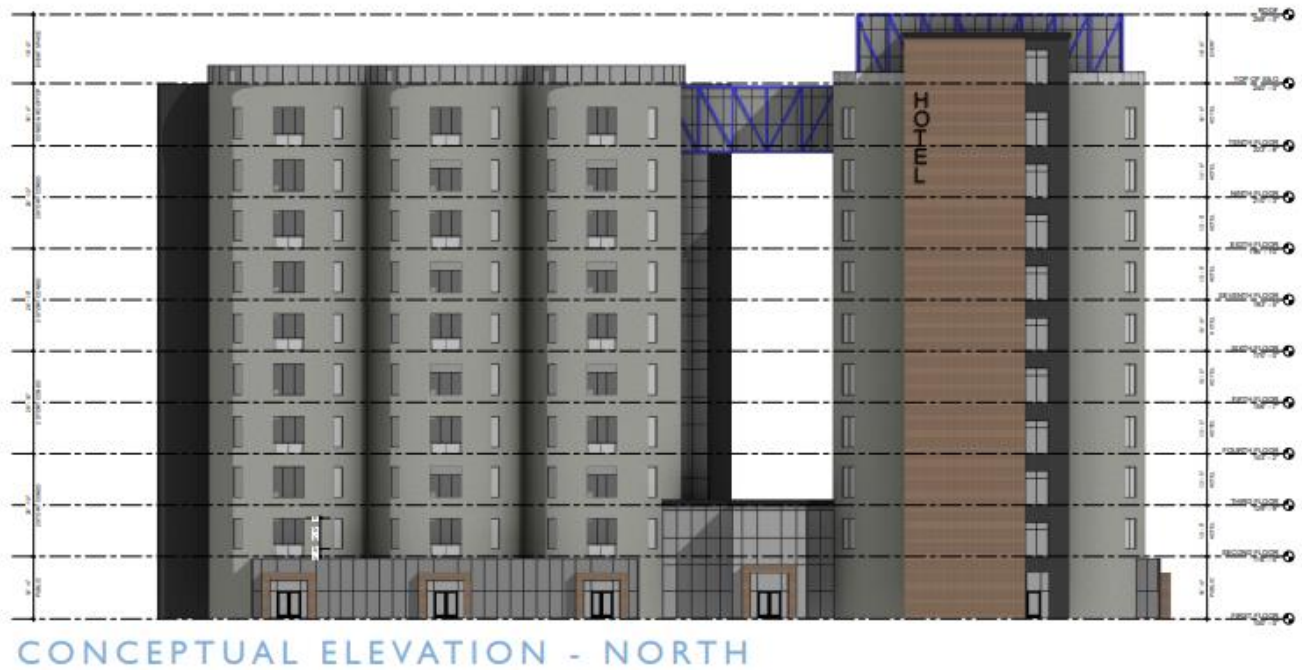
The centerpiece of the plan is the redevelopment of the grain silo into a mixed-use building with a hotel, condos, and retail space. Planned public investments to support the development of the site include an extension of the Chichaqua Valley Trail, realignment and extension of Lincoln Street NE, construction of a public gathering space and an arboretum, implementation of public art and creative placemaking, repurposing of a city Emergency Services facility into a public Parks and Recreation Center, and new public parking. Following adoption of the plan, the City entered into a Memorandum of Understanding (MOU) with the developer. In addition to the infrastructure investments planned by the City, the project is eligible for the City's tax abatement program, and TIF funds could be allocated to the silo redevelopment if it includes hotel and condo uses.

Figure 7. Adopted Plan for the Grain District in Bondurant



Source: City of Bondurant.

Figure 8. Conceptual Plan for Grain Silo Redevelopment



Source: City of Bondurant.

Independence Landing – Independence, OR

Categories: Hotel, Riverfront Revitalization, Events

Major Outcomes: New housing, hotel, and park adjacent to the historic downtown

Description:

Independence Landing is a suburb of Salem, Oregon that has in recent years branded itself as a gateway to the Willamette Valley wine country. The City had spent approximately 15 years prior to forming a development partnership revitalizing its downtown through investments in streetscape and a new park along the river featuring an outdoor amphitheater similar to Elk River's. In 2009, the City's 2020 Vision Plan identified an 18-acre site on the river adjacent to downtown as a key development opportunity. The City used a State transportation grant to fund concept plans for the site, which it ultimately purchased in 2015. In 2017, the City entered into a development agreement with Tokola Properties, which built a park, apartments, townhomes, and the first hotel in the city on the riverfront.

Figure 9. Independence Hotel



Source: Tokola Properties.

Figure 10. Independence Landing



Source: Tokola Properties.

Downtown Walla Walla – Walla Walla, WA

Categories: Hotel, Placemaking

Major Outcomes: Repositioning of Walla Walla as a “boutique destination,” new hotel development, pedestrian plaza.

Description:

The city of Walla Walla in southeastern Washington has a population of just over 30,000 residents that since the 1970s has been working to establish itself as a wine destination. The Walla Walla Main Street Foundation (now the Downtown Walla Walla Foundation) was formed in the 1980s, focusing initially on downtown events like the Italian Heritage Festa and Main Street Fair. In 1987, the foundation partnered with banks on a façade improvement program to revitalize downtown’s aging buildings. In 1991, the City approved a Local Improvement District to fund downtown improvements. In 2003, the City embarked on creating a 20-year downtown Master Plan to guide the future of the district. The Master Plan process created a community vision for the area that the City used as a basis for its urban design framework. The plan identified key public amenities and planned for wayfinding and circulation. Between 2000 and 2014, the City’s downtown won a number of accolades, including Washington Main Street’s Excellence on Main award and the American Planning Association’s Top Ten Great Neighborhood award.

In 2019, the City gained its newest hotel, the FINCH, on the former site of a Red Lion. The 80-room hotel showcases regional amenities by featuring local art and furnishings sourced from the Northwest, as well as through the use of tour guides to suggest activities to guests. Columbia Hospitality, which manages the FINCH, told the *Whitman Wire* that Walla Walla stands out for its “vibrant, unique, and authentic community.”

In 2023, the City made a temporary street closure into a permanent gathering space called Walawala Plaza.

Figure 11. Walawala Plaza in Downtown Walla Walla



Source: Oregon Wine Press.

Figure 12. Downtown Walla Walla



Source: Visit Walla Walla.

Figure 13. The FINCH Hotel in Walla Walla



Source: Visit Walla Walla.

City-Owned Lot: Key Considerations

City Investment

In order to spur development of the City-owned lot, the City will likely need to subsidize the project. Because of current challenges in the development market nationwide, the quicker the City wants to see the lot developed, the more money it will likely need to put into it. Subsidies that can help attract development include:

- Free or reduced-price land transfer
- Equity in the project
- Infrastructure development
- Tax subsidies or deferrals
- Reduced development or impact fees
- Preliminary environmental or other due diligence studies

Partnership Structures & Development Agreement

The Steering Committee has identified redevelopment of the city-owned lot as a top priority for Downtown Elk River. To facilitate the development of this site, the city should conduct developer outreach, embark on a formal RFQ/RFP process to select a developer, and negotiate a development agreement that ensures development on the site is in alignment with the community vision. Key considerations for this agreement include:

- **Land:** the City should determine whether it intends to retain ownership of all or part of the land. Typically, developers prefer to own the development site outright. The City could determine alternative ownership structures, such as a 99-year land lease, but these are complicated arrangements that may not be appropriate for this site. The City could alternatively retain ownership of any parts of the site that are intended for public benefit or that will be developed at a future time.
- **Public Benefit:** Through the development agreement, the City could choose to require the developer to provide some public benefit in exchange for public subsidy (as discussed above). This could include park space, parking, or some other amenity that would benefit the community. However, the City should ensure that the public benefit is not so costly that the agreement falls apart.
- **Timing:** If the City chooses to give the land for free to the developer, it should consider including a provision within the development agreement that causes the land to revert to City ownership if development is not completed within a specified time. This will ensure that the City does not give up control of a key downtown site if the developer is unable to deliver.

Key Actions for Downtown Plan Implementation

Hotel Attraction

Attracting a hotel could be somewhat challenging for Elk River, as it is not yet a tourism destination and few small boutique hotels have been built in the area in recent years. However, as the City implements improvements that address connectivity challenges and emphasize Downtown's connection to the river, the prospects for a new hotel development will improve. Strategies the City should consider to attract hotel development include:

- Financial incentives such as tax abatements, infrastructure improvements, and/or shared planning costs.
- Increasing visits to Downtown Elk River through branding, high-quality placemaking, and events that draw visitors from throughout the region and provide a reason to stay overnight.
- Identification of key sites, preferably along the river, that are suitable for hotel development and zoned appropriately to allow for an urban, boutique-style hotel. This can include sites suitable for both ground-up development and adaptive reuse.
- Building and maintaining relationships with developers and key property owners to identify potential opportunities for partnerships.

Grocery Attraction

Attracting a grocery store to the Uptown neighborhood will be challenging, especially because there are few small-format grocery chains operating in the US. The most likely type of grocery store to locate in this area will be a cooperative. Strategies that cities can use to attract grocery stores include:

- Financial incentives like land, infrastructure improvements, and/or tax credits
- Regulatory incentives like zoning changes or modifications, design concessions, and expedited permitting
- Alerting the local tenant broker community to a potential grocery location in Uptown
- Attending local ICSC events to network and spread the word about the opportunity

Housing

The current development environment nationwide is challenging due to high interest rates as well as rising material and labor costs. Attracting significant housing development may be somewhat challenging in the near term, but the City can use this opportunity to prepare for longer-term investment, especially in the Uptown neighborhood. Actions the City can take to attract housing development include:

- Zoning the Uptown area for a variety of housing types, including townhomes, garden-style apartments, and mixed-use development.
- Ensuring that neighborhood commercial uses, potentially including a grocery store, are allowed and that parking requirements are appropriate for the scale of development.
- Conducting an environmental assessment of the site and potentially mitigating any significant environmental hazards on site that would limit housing development.
- Building relationships with developers who are active in the region and working with them to better understand the opportunities and challenges in this area.
- Providing funding for infrastructure development that will benefit the general public, such as new sidewalks.